



Coachella Civic Center, Hearing Room
53-462 Enterprise Way, Coachella, California
(760) 398-3502 ♦ www.coachella.org

AGENDA

OF A REGULAR MEETING
OF THE
CITY OF COACHELLA
PLANNING COMMISSION

July 06, 2022
6:00 PM

PURSUANT ASSEMBLY BILL 361, ALONG WITH THE GOVERNOR'S STATE OF EMERGENCY DECLARATION ISSUED ON MARCH 4, 2020, THIS MEETING MAY BE CONDUCTED VIA TELECONFERENCE.

If you would like to attend the meeting via zoom, here is the link:

<https://us02web.zoom.us/j/84544257915?pwd=VTdHWitpYVdOUk1NQW8vZ1pqUm0zQT09>

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Public comments may be received via email, telephonically, or via zoom with a limit of 250 words, or three minutes:

In real time:

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In writing:

Written comments may be submitted to the commission electronically via email to gperez@coachella.org. Transmittal prior to the start of the meeting is required. All written comments received will be forwarded to the commission and entered into the record.

IF YOU WISH, YOU MAY LEAVE A MESSAGE AT (760) 398-3102, EXTENSION 122, BEFORE 4:00 P.M. ON THE DAY OF THE MEETING.

CALL TO ORDER:

PLEDGE OF ALLEGIANCE:

ROLL CALL:

APPROVAL OF AGENDA:

“At this time the Commission may announce any items being pulled from the agenda or continued to another date or request the moving of an item on the agenda.”

APPROVAL OF THE MINUTES:

1. Planning Commission Meeting Minutes - June 15, 2022

WRITTEN COMMUNICATIONS:

PUBLIC COMMENTS (NON-AGENDA ITEMS):

“The public may address the Commission on any item of interest to the public that is not on the agenda, but is within the subject matter jurisdiction thereof. Please limit your comments to three (3) minutes.”

REPORTS AND REQUESTS:

NON-HEARING ITEMS:

2. Coachella Sunline Transportation Hub (Architectural Review No 21-13)

The Sunline Transit Hub will serve as a transit center for Sunline Transit Agency services (Line 111, Line 91, Line 92, Line 95) and will include a 540 sq. ft. breakroom/office building for the use of Sunline Transit Agency staff, three bus shelters, landscape improvements, and a corner focal point for a future public art installation located at the Southeast corner of Cesar Chavez Street and 4th Street. Applicant: Sunline Transit Agency

PRESENTATIONS:

3. Coachella Lakes RV Resort Update

PUBLIC HEARING CALENDAR (QUASI-JUDICIAL):

4. Freestanding Identification Signs – Coachella Lakes RV Resort

Architecture Review No. 22-08 to install a freestanding identification sign at 25’ high and monument sign at 5’ high located at 44800 Dillon Road with the Coachella Lakes RV Resort in the C-T, PUD (Commercial Tourist Planned Unit Development) zone. Imagine Service Inc. (Applicant)

5. Coachella Lakes RV Resort – Type 41 Alcohol Sales

Conditional Use Permit No. 353 to allow liquor sales to allow for a Type 41 Alcohol Beverage Control license for “On-Sale Beer & Wine – Eating Place” for an existing facility registration office and a new clubhouse located at 44790 & 44800 Dillon Road and 86401 Sonoma Creek in the C-T, PD (Commercial Tourist Planned Development Overlay) . Coach RV LLC (Applicant) Continued from June 15, 2022 Planning Commission meeting.

6. Development Agreement (Modification) for the Glenroy Resort project consisting of a mixed-use development with 130 resort bungalows, 130 room hotel, conference center, and retail/entertainment uses on approximately 34 acres located at the southeast corner of Avenue 48 and Van Buren Street (APN # 603-220-061, 603-220-065, 603-220-67, 603-220-068, 603-220-069) Applicant: USREC Coachella, LLC.
Withdrawal Requested

INFORMATIONAL:

ADJOURNMENT:

*Complete Agenda Packets are available for public inspection in the
Development Services Department at 53-990 Enterprise Way, Coachella, California, and on the
City's website www.coachella.org.*

THIS MEETING IS ACCESSIBLE TO PERSONS WITH DISABILITIES



Council Chambers, Hearing Room
1515 6th Street, Coachella, California
(760) 398-3502 ♦ www.coachella.org

AGENDA

DE UNA REUNIÓN ORDINARIA DE
LA
COMISIÓN DE PLANIFICACIÓN
DE LA CIUDAD DE COACHELLA

6 de Julio, 2022
6:00 PM

DE ACUERDO CON EL PROYECTO DE LEY 361 DE LA ASAMBLEA, JUNTO CON LA DECLARACIÓN DEL ESTADO DE EMERGENCIA DEL GOBERNADOR EMITIDA EL 4 DE MARZO DE 2020, ESTA REUNIÓN SE PODRÁ REALIZAR POR TELECONFERENCIA.

Si desea asistir a la reunión a través de zoom, aquí está el enlace:

<https://us02web.zoom.us/j/84544257915?pwd=VTdHWitpYVdOUk1NQW8vZ1pqUm0zQT09>

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Los comentarios públicos se pueden recibir por correo electrónico, por teléfono o por zoom con un límite de 250 palabras o tres minutos:

En vivo:

Si participa en vivo a través de zoom o teléfono, durante el período de comentarios públicos, use la función "levantar la mano" en su computadora, o cuando use un teléfono, los participantes pueden levantar la mano presionando *9 en el teclado.

Por escrito:

Los comentarios escritos pueden enviarse a la comisión electrónicamente por correo electrónico a gperez@coachella.org. Se requiere la transmisión antes del inicio de la reunión. Todos los comentarios escritos recibidos serán enviados a la comisión e ingresados en el registro.

SI LO DESEA, PUEDE DEJAR UN MENSAJE EN EL (760) 398-3102, EXTENSIÓN 122, ANTES DE LAS 4:00 P.M. DEL DÍA DE LA REUNIÓN.

LLAMADO AL ORDEN:

JURAMENTO A LA BANDERA:

PASE DE LISTA:

ORDEN DEL DÍA ESPECIAL

APROBACIÓN DE LA AGENDA:

“En este momento, la Comisión puede anunciar cualquier punto que está siendo retirado de la agenda o continuado a otra fecha o solicitar el traslado de un punto de la agenda”.

APROBACION DE LAS ACTAS:

1. Borrador de las Actas de la Comisión de Planificación – 15 de Junio, 2022

COMUNICACIONES ESCRITAS:

COMENTARIOS DEL PÚBLICO (PUNTOS QUE NO ESTÁN EN LA AGENDA):

“El público puede dirigirse a la Comisión sobre cualquier tema de interés para el público que no esté en la agenda, pero que esté dentro de la jurisdicción de la materia de la misma. Por favor limite sus comentarios a tres (3) minutos”.

INFORMES Y SOLICITUDES:

PUNTOS QUE NO SON DE AUDIENCIA:

2. Coachella Sunline Transportation Hub (Revisión Arquitectónica N.º 21-13) - centro de transporte para Sunline Servicios de la Agencia de Transporte (Línea 111, Línea 91, Línea 92, Línea 95) e incluirá un edificio de oficina/sala de descanso de 540 pies cuadrados para el uso del personal de Sunline Transit Agency, cinco paradas de autobús, mejoras en de paisajismo y un punto focal en la esquina para una futura instalación de arte público ubicada en la esquina sureste de Cesar Chavez Street y 4th Street.
Solicitante: Sunline Transit Agency

PRESENTACIONES:

3. Actualización sobre Coachella Lakes RV Resort

CALENDARIO DE AUDIENCIAS PÚBLICAS (CUASI-JUDICIAL):

4. Permiso de uso condicional N.º 353 para permitir la venta de licores Tipo 41 – Cerveza y vino a la venta – Lugar de comidas en la Clubhouse y Pro Shop existente dentro del Coachella Lakes RV Resort en la zona C-T, PUD (Desarrollo Comercial de Unidades Turísticas Planificadas). Coach RV LLC (Solicitante) – Continuado de la reunión de la PC del 15 de junio de 2022

5. Revisión arquitectónica N.º 22-08 un letrero de identificación independiente a 25' de altura y un letrero monumento a 5' de altura ubicado en 44800 Dillon Road con Coachella Lakes RV Resort en la zona C-T, PUD (Desarrollo Comercial de Unidades Turísticas Planificadas). Imagine Service Inc. (Solicitante)
6. El proyecto propuesto implica un Acuerdo de Construcción (Modificación) para el proyecto Glenroy Resort que consiste en un desarrollo de uso mixto con 130 bungalows tipo resort, un hotel de 130 habitaciones, un centro de conferencias y usos comerciales/de entretenimiento en aproximadamente 34 acres ubicados en la esquina sureste de Avenue 48 y Van Buren Street (APN N.º 603-220-061, 603-220-065, 603-220-67, 603-220-068, 603-220-069) Solicitante: USREC Coachella, LLC.

INFORMATIVO:

SE SUSPENDE LA SESIÓN:

Los paquetes completos de la agenda están disponibles para inspección pública en el Departamento de Servicios de Desarrollo en 53-990 Enterprise Way, Coachella, California, y en el sitio web de la ciudad www.coachella.org.

ESTA REUNIÓN ES ACCESIBLE PARA PERSONAS CON DISCAPACIDAD



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MINUTES

OF A REGULAR MEETING
OF THE
CITY OF COACHELLA
PLANNING COMMISSION

June 15, 2022
6:00 PM

PURSUANT ASSEMBLY BILL 361, ALONG WITH THE GOVERNOR'S STATE OF EMERGENCY DECLARATION ISSUED ON MARCH 4, 2020, THIS MEETING MAY BE CONDUCTED VIA TELECONFERENCE.

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CALL TO ORDER: 6:16 P.M.

PLEDGE OF ALLEGIANCE:

Vice Chair Navarrete

ROLL CALL:

Commissioners Present: Commissioner Figueroa, Commissioner Gonzalez, Commissioner Leal, Vice Chair Navarrete, Chair Virgen.

Absent: *Alternate Commissioner Gutierrez

Staff Present: *Gabriel Perez, Development Services Director
*Nikki Gomez

APPROVAL OF AGENDA:

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IT WAS MOVED BY COMMISSIONER FIGUEROA AND SECOND BY VICE CHAIR NAVARRETE TO APPROVE THE AGENDA.

Approved by the following roll call vote:

AYES: Commissioner Figueroa, Commissioner Gonzalez, Commissioner Leal, Vice Chair Navarrete, Chair Virgen.

NOES: None.

ABSTAIN: None.

ABSENT: None.

APPROVAL OF THE MINUTES:

1. Draft Planning Commission Minutes – June 1, 2022

IT WAS MOVED BY VICE CHAIR NAVARRETE AND SECONDED BY COMMISSIONER GONZALEZ TO APPROVE THE MINUTES.

Approved by the following roll call vote:

AYES: Commissioner Figueroa, Commissioner Gonzalez, Commissioner Leal, Vice Chair Navarrete, Chair Virgen.

NOES: None.

ABSTAIN: None.

ABSENT: None.

WRITTEN COMMUNICATIONS:

None.

PUBLIC COMMENTS (NON-AGENDA ITEMS):

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REPORTS AND REQUESTS:

None.

NON-HEARING ITEMS:

2. Request for a first 12-month time extension for Conditional Use Permit No. 336, to convert an existing 3,400 square foot commercial building into a 1,500 square foot retail cannabis microbusiness and 1,900 square foot coffee shop with art gallery. The project entails façade renovations and a new parking lot and landscaping, with outdoor patio improvements on property located at 85-591 Grapefruit Boulevard. Pueblo Cannabis, LLC.

Gabriel Perez, Development Services Director narrated a power point presentation for the item. A copy of the presentation is on file in the Planning Division. Applicant requested a first 12-month time extension of their application.

IT WAS MOVED BY COMMISSIONER FIGUEROA AND SECONDED BY VICE CHAIR NAVARRETE TO GRANT A FIRST 12-MONTH TIME EXTENSION FOR PUEBLO CANNABIS CUP NO. 336 WITH A NEW EXPIRATION DATE MAY 26, 2023.

Approved by the following roll call vote:

AYES: Commissioner Figueroa, Commissioner Gonzalez, Commissioner Leal, Vice Chair Navarrete, Chair Virgen.

NOES: None.

ABSTAIN: None.

ABSENT: None.

PUBLIC HEARING CALENDAR (QUASI-JUDICIAL):

3. AM PM – Type 21 Alcohol Sales

Conditional Use Permit No. 350 to allow liquor sales (ABC License Type 21, Off-Sale General) at the “AMPM” convenience store in an existing commercial building located at 48055 Grapefruit Blvd. GSC & Son Corporation (Applicant) – Withdrawal

Nikki Gomez, Associate Planner narrated a brief power point presentation for the item. Applicant requested withdrawal of their application. No further action by the Planning Commission needed.

Public Hearing Opened at 6:31 pm by Chair Virgen

Public Hearing Closed at 6:31 pm by Chair Virgen

4. Coachella Lakes RV Resort – Type 41 Alcohol Sales

Conditional Use Permit No. 353 to allow liquor sales to allow for a Type 41 Alcohol Beverage Control license for “On-Sale Beer & Wine – Eating Place” for an existing facility registration office and a new clubhouse located at 44790 & 44800 Dillon Road and 86401 Sonoma Creek in the C-T, PD (Commercial Tourist Planned Development Overlay) . Coach RV LLC (Applicant)

Public Hearing Opened at 6:34 pm by Chair Virgen

Public Hearing Closed at 6:35 pm by Chair Virgen

Nikki Gomez, Associate Planner, narrated a short power point presentation for the item. This Item will continue on our next Planning Commission Meeting July 6, 2022.

IT WAS MOVED BY VICE CHAIR NAVARRETE AND SECONDED BY COMMISSIONER GONZALEZ TO APPROVE THE CONTINUATION OF ITEM NUMBER FOUR (4), TO THE NEXT PLANNING COMMISSION MEETING OF JULY 6, 2022.

Approved by the following roll call vote:

AYES: Commissioner Figueroa, Commissioner Gonzalez, Commissioner Leal, Vice Chair Navarrete, Chair Virgen.

NOES: None.

ABSTAIN: None.

ABSENT: None.

INFORMATIONAL:

Gabriel Perez, Development Services Director, announced the following:

- Sunline Transit Hub Revisions are anticipated to be presented at the next Planning Commission meeting in July 6, 2022.
- Training opportunities for Planning Commissioners in the new fiscal year 22-23 (APA or League of California Cities)

ADJOURNMENT: 6:39 P.M.

Respectfully Submitted by,

Gabriel Perez
Planning Commission Secretary

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Development Services Department at 53-990 Enterprise Way, Coachella, California, and on the
City's website www.coachella.org.*

THIS MEETING IS ACCESSIBLE TO PERSONS WITH DISABILITIES



STAFF REPORT
7/6/2022

TO: Planning Commission

FROM: Gabriel Perez, Development Services Director

SUBJECT: Coachella Sunline Transportation Hub (Architectural Review No 21-13)

SPECIFICS: The Sunline Transit Hub will serve as a transit center for Sunline Transit Agency services (Line 111, Line 91, Line 92, Line 95) and will include a 540 sq. ft. breakroom/office building for the use of Sunline Transit Agency staff, three bus shelters, landscape improvements, and a corner focal point for a future public art installation located at the Southeast corner of Cesar Chavez Street and 4th Street.

EXECUTIVE SUMMARY:

Sunline Transit Agency (Sunline) requests Planning Commission approval of the Coachella Sunline Transportation Hub (Transit Hub) for bus service that includes a 540 sq. ft. Sunline Transit Agency breakroom/office building, 5 bus shelters, landscape improvements and a corner focal point for a future public art installation located at the Southeast corner of Cesar Chavez Street and 4th Street (APN 778-080-021).

BACKGROUND:

The subject site is a .86 acre vacant property, part of the Pueblo Viejo Villas Transit-Oriented Development. The site is proposed as a transit center and associated with the construction of the 105-unit Pueblo Viejo Villas multi-family apartment project. The Pueblo Viejo Villas was completed May 2022 and is currently occupied by tenants. The City Engineer authorized a Lot Line Adjustment (LLA 2018-02) that created the alignment for a one-way loop street that would provide an exit to Cesar Chavez Street and full access from 4th Street for the Transit Hub and the Pueblo Viejo Villas development. The loop street also provides access to angled parking in from of the multifamily residential building.



The transit hub is funded through the Affordable Housing and Sustainable Communities (AHSC) Grant from the California Department of Housing and Community Development for \$14.8 million,

of which \$1,813,500 is designated for transit hub design and construction. The grant also provides funding for 4 Sunline buses, 40 vans, \$696,500 in street improvements, and an \$8,395,407 housing loan for the Pueblo Viejo Villas project.

The project was first presented to the Planning Commission on January 19, 2022 and the Commission requested that staff recommended revisions be integrated into the project and that restrooms be incorporated. The item was presented to the City Council at their regular meeting on February 9, 2022 as a presentation update. Councilmembers expressed concerns about the project design and also reaffirmed Planning Commission's recommendations for a public restroom. The applicant proposes new revisions to the project design with an estimated construction cost \$1,774,859, within the designated budget for the transit hub. The estimated construction cost of the previous project design was \$1,163,000.

DISCUSSION/ANALYSIS

The surrounding land uses and zoning designations are as follows:

- North:** Fourth Street and Chevron gas station and convenience store (C-G, General Commercial).
- South:** Pueblo Viejo Villas apartment development. (C-G PUD, General Commercial – Planned Unit Development).
- East:** Apartment complex and Department of Public Social Services (C-G, General Commercial).
- West:** Cesar Chavez Street and Rite Aid Pharmacy (C-G, General Commercial).

Site Plan

The transit hub is accessed by vehicles entering the Mario Lazcano Drive loop road from 4th Street to pick up and drop off bus riders at the bus shelter locations and exit back onto Cesar Chavez Street or onto 4th Street. The original site plan identified a bus shelter island with two bus shelters with seating and overhead canopy. The revised plans in figure 2 identify one large “swoosh” shaped canopy to replace the two smaller bus shelters. Three bus shelters were originally proposed near the Cesar Chavez Street sidewalk, which can be serviced by buses in the planned bus turnout on Cesar Chavez or the driveway within the transit hub. The revised design as shown in figure 2 identifies two large “swoosh” shaped canopies instead of the original three smaller canopies. Sidewalks throughout the transit center are largely 6 feet in width (natural gray concrete) and provide pedestrians access to the transit hub. Sidewalks are wider at about 8 feet in width near the proposed lounge building.

A 540 sq. ft. Sunline Transit Agency lounge and breakroom building is proposed at the south portion of the transit hub. The building is not available for public use and will be used exclusively by Sunline Transit Agency staff. The floor plan for the building includes a mechanical room, unisex bathroom, breakroom, janitor room, and office. Bike racks are located adjacent to two sides of the building. A public restroom is not incorporated into the project design. Streetscape furniture and amenities include:

- Ten (10) site benches
- Five (5) trash receptacles
- Six (6) bike racks
- A bicycle service stand

Figure 1: Original rendering of transit hub



Figure 2: Revised rendering of transit hub



On-site storm water retention is provided by an above ground retention basin of approximately 12,300 sq. ft. in area that also provides storm water retention for the Department of Public Services building. The retention basin construction is complete with an existing chain link fence. Staff was under the impression that a wrought iron fence would replace the chain link fence upon completion. The proposed renderings identify the retention of the chain link fence for the project and painting it a black color.

Architectural Design

The original transit hub design features includes five (5) bus shelters consisting of two (2) hypar sail panels per bus shelter to provide shade for waiting bus riders. The revised design features include two (2) large “swoosh” shade structures 20’ by 40’ and one (1) small shade structure 15’ x 18’ with a brown colored frame and a “desert sand” fabric. The bus shelter designs are a more modern look and would be a departure from the Pueblo Viejo Villas design them. Two 96” long benches with a wood seat will be located beneath each bus shelter with a bronze powdercoat color. Five (5) litter receptacles are proposed with a bronze powder coat color. Decorative street lighting consistent with 6th Street lighting standards is incorporated.

Figure 3: Original Proposed Bus Hypar Shade Sail



Figure 4: New Proposed Bus “Swoosh” shade structure



The proposed Sunline breakroom building is a pre-fabricated building. Staff expressed concern about the original building design with a pre-fabricated exterior look that is incompatible with the Pueblo Viejo design guidelines. Sunline designers developed modified exterior elevations with a red tile roof, stucco exterior, exposed rafters tail and beams, wood doors, window iron work and a tile sign more in the character of Spanish Colonial Revival architecture as shown in figure 5. The applicant revised the building design further with with foam decorative pop-outs, digital sign, expanded mosaic features, and water fountain with bottle filter. Planning staff still has design concerns regarding the building and believe the architectural recommendations have not been incorporated.

Figure 5: Original Rendering of the Sunline Breakroom Building



Figure 6: Proposed Rendering of the Sunline Breakroom Building



Signage

Signage for the transit hub is proposed on the north elevation of the Sunline breakroom building consisting of a tile design identifying the “Coachella Transit Center.” A digital sign is also proposed as a new addition to the building design.

Landscape Design

The plant palette originally included 12 date palm trees would be planted at a 16 foot brown trunk height. The date palm trees were removed on a revised landscape plan and replaced with shade trees consisting of “Desert Willow” and “Texas Ebony.” The Pueblo Viejo landscape design guidelines suggests California Fan Palm, Hong Kong Orchid, or Chinese Elm for ornamental trees on Cesar Chavez Street. Proposed shrubs include trailing indigo bush, grey desert spoon, California Encelia, and Mexican Bush Sage. These shrubs or plants are not identified as recommended plant types in the Pueblo Viejo landscape design guidelines. Staff recommends that a combination of date palms and shade trees be incorporated into the landscape design to reduce heat island effects and that plants be substituted with the succulents, shrubs, and perennials recommended in the Pueblo Viejo design guidelines. Staff also recommends that the sidewalk along Cesar Chavez Street along landscape areas to create a landscape parkway to separate pedestrian traffic from street and allow for shade from afternoon sun and reduce heat island effect. Alternatively, place tree grates in proposed sidewalk area consistent with Pueblo Viejo Design Guidelines (page 4-180) that reduce heat island effect and provide shade from afternoon sun. Landscape surfaces will utilize decomposed granite, Baja Cresta Rubble (6”-12”) and Palm Springs Gold Boulders (3’-5’).

CONSISTENCY WITH THE GENERAL PLAN

The proposed project is within the Downtown Center land use designation of the General Plan 2035 Land Use and Community Character Element. The Downtown Center is intended to bring the entire community together in a one-of-a-kind Coachella Center. The Coachella Sunline Transit Hub would accomplished the General Plan goals of bringing the community together by created a transit center that bring visitors to the City’s Pueblo Viejo district from the City and throughout the Coachella Valley.

CONSISTENCY WITH ZONING

The subject site is zoned C-G PUD (General Commercial Planned Unit Development) zone. The project complies with the development standards of the Zoning Ordinance.

CONSISTENCY WITH PUEBLO VIEJO DISTRICT DESIGN GUIDELINES

The subject site is located within the Cesar Chavez Street Subarea of the Pueblo Viejo District Design Guidelines. The Guidelines provide for guidance in the design of the architecture, signs, streetscape, street furniture and landscaping and its application is explained throughout this report. The design guidelines have been identified below, along with recommendation on how the project may be modified to improve its consistency with each design guideline.

Spanish Colonial revival Architecture

3.3.1.2 Walls

- Smooth or sand-float finish concrete with tastefully placed adornments
- **Recommendation 1: Use a smooth trowel finish for stucco for Sunline building.**

3.3.1.4 Articulation and Decorative Elements

- Patterned Cutouts in smooth stucco used to decorate walls, chimneys, and vents.
- Terra-cotta or case concrete ornaments
- **Recommendation 2: Use terra-cotta tile ornaments**

3.3.1.5 Windows and Doors

- The window, grilles, and doors are typically earth tones such as dark brown, terra-cotta reds, and light greens.
- **Recommendation 3: Utilize decorative window grilles and incorporate light green window color.**



3.3.1.9 Lighting

Spanish Colonial Revival–style outdoor lighting fixtures with the features listed below include:

- Wrought iron lighting fixtures, including lanterns of many different shapes
- Clear glass shades to accent candle-shaped bulbs or amber shaded glass
- Grandly ornamented fixtures with scrolled metal accents
- Fixtures are either mounted to the wall or hung using chains
- **Recommendation 4: Include outdoor lighting features on the building consisting with 3.3.19.**

3.5.7 Perimeter Fences and Walls

- While security fences are allowed, chain link, barbed wire and razor wire, fences should not be used.
- The design of fences, walls, and other structural landscape features should be compatible with and complementary to the architecture of the building and the surrounding setting.
- All fences, walls, and other related features should be accompanied by landscaping to better integrate the structure within the site and reduce its visual impact.

Perimeter Fence Examples. SOURCES:
Top- Thomas1313; Bottom- C&CGabrielle



- **Recommendation 5: Incorporate decorative wrought iron fencing at the retention basin consisting with Pueblo Viejo Design Guidelines**



Pedestrian and Bicycle Crossings (page 4-173)

- Decorative crosswalks typically made with bricks or other specialty paving are encouraged. “exampled of acceptable treatments include brick lattice patterns, paving bricks, paving stones, setts, cobbles, or other resources designed to simulate such paving. Acceptable colors for these materials would be red, rust, brown, burgundy, clay, tan or similar earth tone equivalents....”
- **Proposed design provides one crosswalk with interlocking pavers (Sandstone color)**

Benches (page 4-178)

- Benches in commercial areas should follow the palette established by the Sixth Street streetscape design: 96-inch Classic series bench, model c-196, bronze powder coat color with IPE wood seat.
- **Proposed design is consistent with Guidelines.**

Bench Used in Sixth Street Streetscape. SOURCE: Victor Stanley



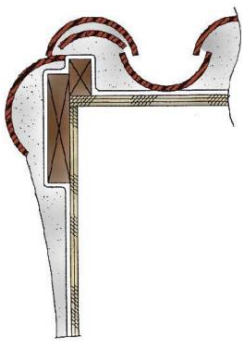
ENVIRONMENTAL IMPACT CONSIDERATION

The City of Coachella has determined that the proposed project qualifies for a CEQA exemption under Section 15332. Class 32 consists of projects characterized as in-fill development meeting the conditions described in this section; (a) The project is consistent with the applicable general plan designation and all applicable general plan policies as well as with applicable zoning designation and regulations; (b) The proposed development occurs within city limits on a project site where no more than five acres are to be developed and substantially surrounded by urban uses; (c) The project site has no value as habitat for endangered, rare or threatened species; (d) Approval of the project would not result in any significant effects relating to traffic, noise, air quality, or water quality; (e) The site can be adequately served by all required utilities and public services. Therefore, the City has acknowledged that the project is exempt from CEQA.

RECOMMENDATIONS

Staff can support the project as proposed incorporating the following recommendations, in addition to those noted above to improve the overall appearance of the project. Staff believes it is important that the transit hub make a welcoming entry statement to the Pueblo Viejo District and be demonstrate compatibility with the Pueblo Viejo Villas as contemplated as a transit-oriented development. Some design recommendations may result in additional costs beyond those budgeted for the project and require further appropriation from the City Council. Staff seeks the Planning Commission's comments and welcomes additional comments and recommendations.

1. Slake the stucco at the edge of the gable roof on the Sunline building to encapsulate the tile.



This creates an attractive shadow pattern



Source: San Clemente Architectural Design Guidelines

2. Add a decorative feature along the expanse of the Sunline building wall to avoid appearance of a long blank wall. Features such as cornice trim with terra-cotta or decorative tile accents similar to the Veteran's Park restroom serves as an example of added decorative features to an otherwise blank wall expanse.



3. Setback the sidewalk along Cesar Chavez Street along landscape areas to create a landscape parkway to separate pedestrian traffic from street and allow for shade from afternoon sun and reduce heat island effect. Alternatively, place tree grates in proposed sidewalk area consistent with Pueblo Viejo Design Guidelines (page 4-180) that reduce heat island effect and provide shade from afternoon sun.
4. Widen 6 foot sidewalk areas to 8 feet to enhance pedestrian access.

5. Incorporate date palm tree with the landscape design and substitute trees and plants with the types recommended in the Pueblo Viejo design guidelines.
6. Add trees and plants at the retention basin between fencing and curb where landscaping is currently sparse.
7. Utilize a larger caliper groundcover material similar to that used at Veteran's Park.

ALTERNATIVES:

- 1) Adopt Resolution No. PC 2022-01 approving Architectural Review No. No. 21-13;
- 2) Adopt Resolution No. PC 2022-01 approving Architectural Review No. No. 21-13 removing design and landscape conditions of approval 7-21;
- 3) Deny the proposed project;
- 4) Continue this item and provide staff and the applicant with direction.

RECOMMENDED ALTERNATIVE(S):

Staff recommends alternative #1.

Attachments:

1. PC Resolution No. 2022-01 for AR No. 21-13
Exhibit A - Conditions of Approval for AR No. 21-13
2. Vicinity Map
3. Site Plan and Civil Plan
4. Landscape Plan
5. Original Transit Hub Architectural Renderings
6. New Revised Transit Hub Architectural Renderings
7. Updated Building Design presented to Planning Commission January 19, 2022
8. Original Sunline Building Design with Floor Plan
9. Bus Shelter Examples
10. Veteran's Park Building Photo Examples
11. Existing Site Conditions
12. Pueblo Viejo Design Guidelines
13. Planning Commission Meeting Minutes January 19, 2022

RESOLUTION NO. PC2022-01

Attachment 1

A RESOLUTION OF THE CITY OF COACHELLA PLANNING COMMISSION APPROVING ARCHITECTURAL REVIEW 21-13 TO ALLOW THE CONSTRUCTION OF THE COACHELLA SUNLINE TRANSPORTATION HUB CONSISTING OF A 540 SQ. FT. BREAKROOM/OFFICE BUILDING, THREE BUS SHELTERS, STREETScape AND LANDSCAPE IMPROVEMENTS LOCATED AT THE SOUTHEAST OF FOURTH STREET AND CESAR CHAVEZ STREET (APN 778-080-021) SUNLINE TRANSIT AGENCY (APPLICANT).

WHEREAS Sunline Transit Agency filed an application for Architectural Review (AR) 21-13 to allow the construction of a transit center for Sunline Transit Agency services (Line 111, Line 91, Line 92, Line 95) and will include a 540 sq. ft. breakroom/office building for the use of Sunline Transit Agency staff, three bus shelters, landscape improvements, and a corner focal point for a future public art installation located at the Southeast corner of Cesar Chavez Street and 4th Street located at the northeast corner of First Street and Cesar Chavez Street (APN 778-080-021); and,

WHEREAS on January 19, 2022, the Planning Commission of the City of Coachella considered the subject application, considered written and oral comments, and facts and evidence presented by the applicant, City staff, and other interested parties and requested project design revisions; and

WHEREAS on July 6, 2022, the Planning Commission of the City of Coachella again considered the subject application, considered written and oral comments, and facts and evidence presented by the applicant, City staff, and other interested parties and requested project design revisions; and

WHEREAS the proposed use is necessary or desirable for the development of the community, is consistent with the objectives of the City’s General Plan, and is not detrimental to the existing uses or the uses specifically permitted in the zone in which the proposed use is to be located; and,

WHEREAS the proposed site is adequate in size and shape to accommodate the proposed development; and,

WHEREAS, the Planning Division completed an initial environmental assessment of the above matter and in accordance with the California Environmental Quality Act (CEQA) recommends the Planning Commission determine the project qualifies for a CEQA exemption under Section 15332.

NOW, THEREFORE, BE IT RESOLVED, that the Planning Commission of the City of Coachella, California hereby resolve as follows:

Section 1. Incorporation of Recitals

The Planning Commission hereby finds that all of the facts in the Recitals are true and correct

and are incorporated and adopted as findings of the Planning Commission as fully set forth in this resolution.

Section 2. CEQA Findings

Based upon its review of the entire record, including the Staff Report, any public comments or testimony presented to the Planning Commission, and the facts outlined below, the Planning Commission hereby finds and determines that the proposed project project qualifies for a CEQA exemption under Section 15332. Class 32 consists of projects characterized as in-fill development meeting the conditions described in this section; (a) The project is consistent with the applicable general plan designation and all applicable general plan policies as well as with applicable zoning designation and regulations; (b) The proposed development occurs within city limits on a project site where no more than five acres are to be developed and substantially surrounded by urban uses; (c) The project site has no value as habitat for endangered, rare or threatened species; (d) Approval of the project would not result in any significant effects relating to traffic, noise, air quality, or water quality; (e) The site can be adequately served by all required utilities and public services. Therefore, the City has acknowledged that the project is exempt from CEQA.

Section 3. Architectural Review Findings

With respect to Architectural Review 21-13, the Planning Commission finds as follows for the proposed for the proposed transit hub:

1. The Architectural Review is consistent with the goals, objectives, policies, and implementation measures of the Coachella General Plan 2035. The proposed project is within the Downtown Center land use designation of the General Plan 2035 Land Use and Community Character Element. The Downtown Center is intended to bring the entire community together in a one-of-a-kind Coachella Center. The Coachella Sunline Transit Hub would accomplished the General Plan goals of brining the community together by created a transit center that bring visitors to the City’s Pueblo Viejo district from the City and throughout the Coachella Valley. The proposed uses on the site are in keeping with the policies of the Downtown Center land use classification and the Project is internally consistent with other General Plan policies for this type of development.
2. The proposed uses will be located, designed, constructed, operated and maintained so as to be compatible with the existing or intended character of the general vicinity and shall not change the essential character of the same area. The proposed project is in compliance with the applicable development standards for the C-G (General Commercial) Zoning District of the City's Zoning Code.
3. Consideration is given to harmony in scale, bulk, coverage and density, to the availability of public facilities and utilities, to harmful effect, if any, upon desirable neighborhood character, to the generation of traffic and the capacity of surrounding streets, and to any other relevant impact of the development. The proposed project is in compliance with the applicable development standards for the C-G (General Commercial) Zoning District of the City's Zoning Code. The project as designed and conditioned would designed to be consistent with architecture, streetscape, and landscape design recommended in the Pueblo Viejo District

Design Guidelines.

- 4. The Project will be compatible with neighboring properties with respect to land development patterns. The proposed development would develop according to the development standards of the C-G Zone at a scale, massing, and aesthetic appeal of existing development is in keeping with development of neighboring properties.
- 5. The proposed use will include vehicular approaches to the property designed to improve off-site and on-site vehicular circulation for existing traffic and buses on surrounding public streets or roads. Evidence of this is reflected in the provided site plan design.

Section 5. Planning Commission Approval

Based on the foregoing recitals and findings above, and the written and oral comments, facts and evidence presented, the City of Coachella Planning Commission hereby approves Architectural Review 21-13 for the Coachella Sunline Transportation Hub subject to the conditions approval identified in Exhibit A.

PASSED APPROVED and ADOPTED this 6th day of July 2022.

Stephanie Virgen, Chairperson
Coachella Planning Commission

ATTEST:

Gabriel Perez
Planning Commission Secretary

APPROVED AS TO FORM:

Carlos Campos
City Attorney

I HEREBY CERTIFY that the foregoing Resolution No. PC2022-01, was duly adopted at a regular meeting of the Planning Commission of the City of Coachella, California, held on the 6th day of July 2022, by the following roll call vote:

AYES:

NOES:

ABSENT:

ABSTAIN:

Gabriel Perez
Planning Commission Secretary

Exhibit A - Resolution No. PC2022-01
CONDITIONS OF APPROVAL
ARCHITECTURAL REVIEW NO. 21-13 SUNLINE TRANSIT HUB

General Conditions

1. Architectural Review 21-13 shall be valid for 12 months from the effective date of said Planning Commission approvals unless the applicant requests an extension of time and granted by the Planning Commission. The conditional use permit shall expire and shall become void one year following the date on which the conditional use became effective, unless prior to the expiration of one year a building permit is issued and construction is commenced and diligently pursued toward completion, or a certificate of occupancy is issued.
2. The applicant shall defend, indemnify and hold harmless the City of Coachella, its officials, officers, employees, and agents from and against any claim, action, or proceeding against the City, its officials, officers, employees or agents to attack, set aside, void or annul any project approval or condition of approval of the city concerning this project, including but not limited to any approval or condition of approval or mitigation measure imposed by the City Council or Planning Commission. The City shall promptly notify the applicant of any claim, action, or proceeding concerning the project and the City shall cooperate fully in the defense of the matter. The City reserves the right, at its own option, to choose its own attorney to represent the City, its officials, officers, employees and agents in the defense of the City Attorney, within five days of the effective date of this approval.
3. Within five business days of project approval, the applicant shall submit to the Planning Division a check made payable to the County of Riverside in the amount of \$50 for filing the CEQA Notice.
4. All plans, as shown, are considered “conceptual,” subject to revisions as called out by the conditions of this resolution. The plans shall not be stamp-approved until all conditions requesting revisions have been satisfied during the building plan check process. Any substantial changes to the plans, including changes shown on future building permit plans deemed by Staff to not be within substantial conformance with this approval, will require an amendment to the approval of the conditional use permit or architectural review.
5. All plan submittals are the responsibility of the developer; this includes plan submittals to the City of Coachella, the Riverside County Fire Department or to other agencies for whom plan review and approval is required.
6. The working drawings shall include within the first four pages a list of all conditions of approval included in this resolution.

Architectural Design

7. The applicant shall include project architectural design modifications prior to building permit submittal that include:
 - a. Ensuring sufficient window and door recesses on breakroom building.
 - b. Add stucco pattern that encapsulate the tile at the roof gables to create attractive shadow patterns to the breakroom building.
 - c. Incorporate date palm tree with the landscape design and substitute trees and plants with the types recommended in the Pueblo Viejo design guidelines.
 - d. Remove existing retention basin chain link fence and replace with decorative wrought iron fencing.
 - e. Utilize decorative window grilles and incorporate light green window color.
 - f. Add decorative lighting to the breakroom building.
8. Utilize two-piece clay tile roofing with booster tiles on the edges and random mortar packing. The mortar shall packed on 100 percent of the tiles in the first two rows of tiles and along any rake and ridgeline, and packed on 25 percent of the tiles on the remaining field. Mortar packing shall serve as a bird stop at roof edges. The volume of mortar pack to achieve the appropriate thickness shall be equivalent to a 6 inch diameter sphere of mortar applied to reach tile.
9. Stucco walls shall consist of 'steel, hand trowel', smooth Mission finish and slight undulations (applied during brown coat) and bull-nose corners and edges.
10. Any proposed trash enclosures installed for the project shall be compatible architecturally with the building. The enclosure shall be constructed to Burrtec Waste Management Standards and an open trellis shall be incorporated on all three proposed trash enclosures.

Public Realm/Streetscape

11. Setback the sidewalk along Cesar Chavez Street along landscape areas to create a landscape parkway to separate pedestrian traffic from street and allow for shade from afternoon sun and reduce heat island effect. Alternatively, place tree grates in proposed sidewalk area consistent with Pueblo Viejo Design Guidelines (page 4-180) that reduce heat island effect and provide shade from afternoon sun.
12. Widen 6-foot sidewalk areas to 8 feet to enhance pedestrian access.

Landscaping

13. Final landscaping and irrigation plans shall be submitted to the Development Services Department for review and approval. Said plans shall conform to the landscaping plan

submitted as part of the subject Architectural Review, and as conditioned herein. Landscape lighting shall be incorporated. All landscape materials shall be identified on a legend and identified graphically on a landscape plan including planting counts, tree caliper, and planted tree heights.

14. Add trees and plants at the retention basin between fencing and curb.
15. Decomposed granite for landscape areas shall be a minimum of ¾ inch.
16. Landscaping and irrigation shall be provided in accordance with Section 17.54.010(J) of the Municipal Code and in accordance with the State Model Water Efficient Landscape Ordinance (AB 1881). Water budget calculations, including the Maximum Applied Water Allowance (MAWA) and Estimated Total Water Use (ETWU) shall be provided as part of the landscaping and irrigation plan.
17. Combination of date palms and shade trees shall be incorporated into the landscape design which shall include Hong Kong Orchid or Chinese Elm trees, which are eligible trees in the Pueblo Viejo design guidelines. Plants shall be substituted with the succulents, shrubs, and perennials recommended in the Pueblo Viejo design guidelines.
18. The applicant shall submit, and must obtain approval from the Development Services Director, a letter from a registered landscape architect confirming that landscaping and irrigation have been installed in accordance with the approved plans prior to issuance of a certificate of occupancy. Any damaged, dead or decayed landscaping landscape areas within the remainder of the commercial center shall be replaced per approved landscape plans upon landscape inspection by the Development Services Department and prior to issuance of a certificate of occupancy/project completion.



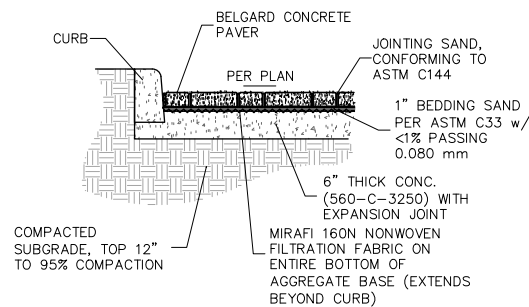
VICINITY MAP

CONSTRUCTION NOTES

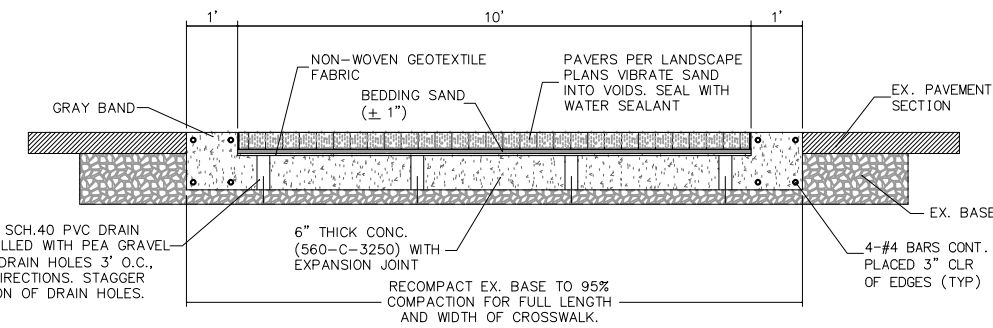
	UNITS	QUANTITY
1) PROTECT IN PLACE	--	--
(1) 2) REMOVE CURB & GUTTER	LF	195
(1) 3) REMOVE SIDEWALK	SF	1,075
(1) 4) REMOVE DRIVEWAY	SF	164
5) SALVAGE & REINSTALL STREET SIGN	EA	1
6) COMMUNICATIONS POLE TO BE REMOVED BY OTHERS	EA	3
7) CONST. 6" CONC. w/ FIBER MESH OVER 8" CMB	SF	13,050
8) CONST. 6" TYPE D CURB PER COC STD S-10	LF	508
9) CONST. 6" C&G PER COC STD S-7	LF	366
10) CONST. SIDEWALK (WIDTH PER PLAN) PER COC STD S25.1	SF	5,741
11) CONST. CURB RAMP TYPE 1, CASE B PER SPPIC DWG 111-5	EA	2
12) CONST. CROSSWALK w/ TRAFFIC RATED INTERLOCKING PAVERS (SANDSTONE COLOR) PER DETAIL ON SHEET 2	SF	228
13) CONST. BUS TURNOUT PER RCTD STD 814	SF	2,292
14) CONST. 8" CONCRETE BUILDING SLAB PER DETAIL ON SHEET 2	SF	514
15) F&I METAL BOLLARD PER DETAIL ON SHEET 2	EA	3
16) CONST. CONC. CURB OPENING w/ SPILLWAY PER DETAIL ON SHEET 2	SF	225
17) RIPRAP PER DETAIL ON SHEET 2	SF	45
18) CONST. PARTIAL CONC. SPANDREL PER DETAIL ON SHEET 2	SF	142
19) F&I PARKING LOT LIGHT, POST TYPE PER DETAIL ON SHEET 9	EA	5
20) F&I PARKING LOT LIGHT, SINGLE HEAD MAST, PER DETAIL ON SHEET 9	EA	3
21) F&I PARKING LOT LIGHT, DUAL HEAD MAST, PER DETAIL ON SHEET 9	EA	3
22) F&I 1.5" PVC CONDUIT & CONDUCTORS FOR SITE LIGHTING	LF	560
23) F&I IID TRANSFORMER PAD PER IID PLANS	EA	1
24) F&I 4" SCH 40 PVC CONDUIT (IID)	LF	20
25) F&I 6" SCH 40 PVC CONDUIT (IID)	LF	112
26) INSTALL SIGN	EA	4
27) INSTALL STOP BAR & "STOP" PM	EA	1
28) F&I 6" SDR-26 PVC SEWER LATERAL w/ BENDS & WYES PER COC STD. D-11	LF	105
29) F&I 6" SEWER CLEAN OUT PER COC STD D-1	EA	1
30) F&I 2" PVC SDR-21 DOMESTIC WATER SERVICE w/ BENDS & THRUST BLOCKS PER COC STD W-8	LF	250
31) F&I 1.5" PVC CONDUIT w/ PULL ROPE (DATA & FUTURE)	LF	570
32) F&I ELECTRICAL PULL BOX PER SPPPP STD 513-3	EA	3
33) F&I SIGN POST ONLY FOR SUNLINE BUS STOP SIGNAGE	EA	6
34) F&I SECURITY CAMERA & MOUNTING BRACKETS	EA	9
35) BUS STOP INFO SIGN INSTALLED BY SUNLINE	EA	5
36) F&I SIGNAL PULL BOX, CONDUIT & CABLING TO CONTROLLER CABINET	EA	1
37) F&I 3/4" PVC SDR-21 DOMESTIC WATER SERVICE w/ BENDS, CORP STOP & VALVE BOX	LF	40

(1) QUANTITY PROVIDED FOR INFORMATION ONLY. ITEMS OF WORK PER THE SPECIFICATIONS ARE INCIDENTAL TO OTHER CONSTRUCTION ITEMS LISTED IN THE BID SCHEDULE.

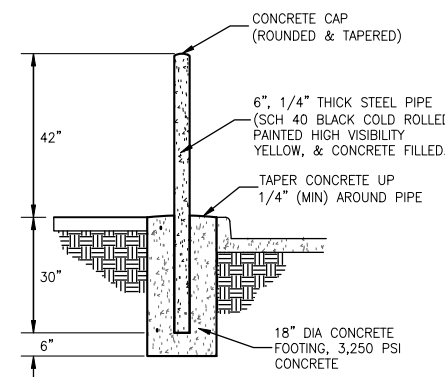
NOTE:
QUANTITIES LISTED ABOVE ARE PROVIDED FOR CONTRACTOR INFORMATION. THE BID ITEMS LISTED IN THE PROJECT BID PACKAGE UNDER THE BID SCHEDULE WILL GOVERN IN THE CASE OF A DISCREPANCY IN THE ABOVE LISTED QUANTITIES.



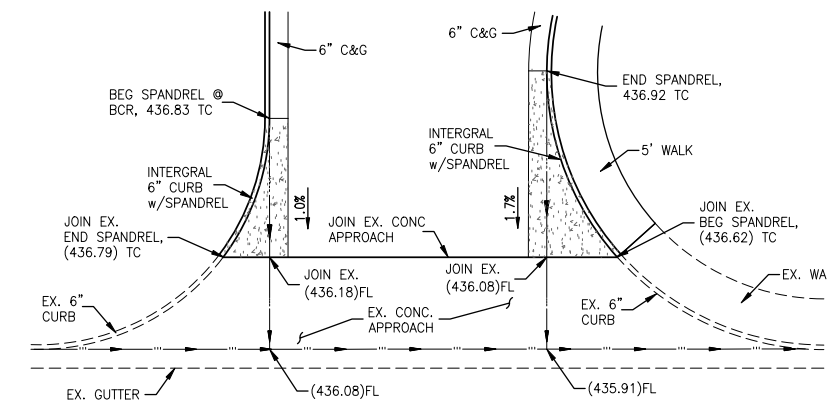
12) CROSSWALK DETAIL @ CURB
NOT TO SCALE



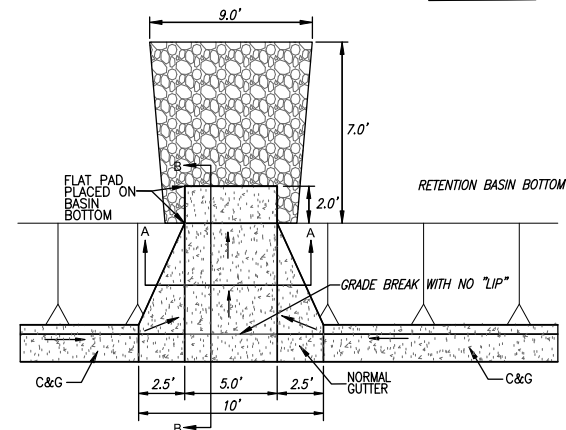
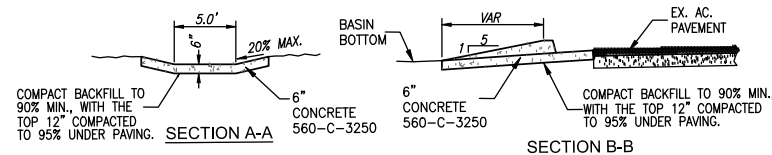
12) CROSSWALK CROSS SECTION DETAIL
NOT TO SCALE



15) METAL BOLLARD
NOT TO SCALE



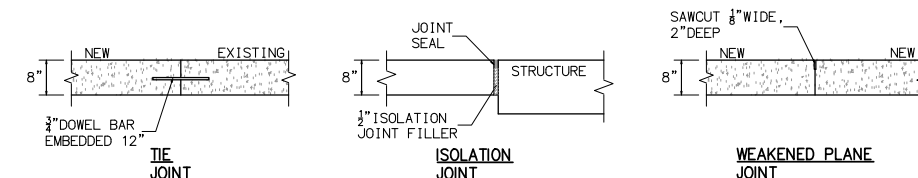
18) PARTIAL CONC. SPANDREL
NOT TO SCALE



16) 17) CURB OPENING w/ SPILLWAY & RIPRAP
NOT TO SCALE

WAITING ON RECOMMENDATIONS FROM THE BUILDING MANUFACTURER

14) CONCRETE BUILDING SLAB
NOT TO SCALE



7) CONCRETE PAVEMENT JOINTS
NOT TO SCALE

CONCRETE PAVEMENT CONSTRUCTION NOTES

1. THE TOP 12" OF SUBGRADE SHALL BE COMPACTED TO 95% RELATIVE COMPACTION, WITH THE SUBGRADE BROUGHT TO JUST OVER OPTIMUM MOISTURE CONTENT DURING COMPACTION OPERATIONS.
2. THE CONCRETE STRUCTURAL SECTION FOR THE FRONT PARKING LOT/DRIVEWAY AREA SHALL BE AS FOLLOWS:
6.0" THICK 560-C-4500 CONCRETE W/ TYPE III FIBER MESH AND REINFORCEMENT OVER 8.0" THICK CRUSHED MISC. BASE (CMB) CLASS II OVER 12.0" THICK COMPACTED SUBGRADE TO 95% RELATIVE COMPACTION
3. THE FRONT PARKING LOT/DRIVEWAY AREA SHALL BE REINFORCED WITH #4 REINFORCEMENT PLACED 18" O.C., AND THE FIBER MESH SHALL BE TYPE III, BOTH IN ACCORDANCE WITH THE GREENBOOK STANDARD SPECIFICATION 201-2.
4. MAXIMUM JOINT SPACING IS 12 FEET, UNLESS OTHERWISE NOTED. LAYOUT JOINTS SHALL BE SQUARE. RECTANGULAR PANELS MAY BE ALLOWED AS LONG AS THE LONG DIMENSION IS NO MORE THAN 1.5 TIMES THE LENGTH OF THE SHORT DIMENSION.
5. WEAKENED PLANE JOINTS SHALL HAVE A DEPTH OF AT LEAST ONE-QUARTER (1/4) OF THE SLAB THICKNESS (WITH A MINIMUM DEPTH OF 1.5"). EXPANSION JOINTS SHALL EXTEND THROUGH THE ENTIRE SLAB THICKNESS AND SHALL BE USED TO ISOLATE FIXED OBJECTS ABUTTING OR WITHIN THE CONCRETE AREA. SAWED JOINTS SHALL BE COMPLETED AS SOON AS POSSIBLE WITHOUT RAVELING THE NEW CONCRETE.
6. ALL NEW CONCRETE ADJACENT TO EXISTING CONCRETE PAVEMENT, FOR THE BACK YARD PAVING, SHALL BE TIED TO THE EXISTING CONCRETE WITH A 3/8" DOWEL BAR EMBEDDED A MINIMUM OF 12" INTO BOTH THE NEW AND EXISTING CONCRETE. DOWEL BARS SHALL BE SPACED AT 24" O.C., AND A SHALL BE PLACED NO CLOSER THAN 12" FROM A WEAKENED PLANE JOINT.

NOTE: WORK CONTAINED WITHIN THESE PLANS SHALL NOT COMMENCE UNTIL AN ENCROACHMENT PERMIT AND/OR A GRADING PERMIT HAS BEEN ISSUED.

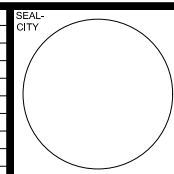
THE PRIVATE ENGINEER SIGNING THESE PLANS IS RESPONSIBLE FOR ASSURING THE ACCURACY AND ACCEPTABILITY OF THE DESIGN HEREON. IN THE EVENT OF DISCREPANCIES ARISING AFTER CITY APPROVAL OR DURING CONSTRUCTION THE PRIVATE ENGINEER SHALL BE RESPONSIBLE FOR DETERMINING AN ACCEPTABLE SOLUTION AND REVISIONS TO THE PLANS FOR APPROVAL BY THE CITY.



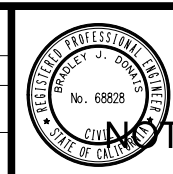
BENCHMARK:
GENMARK: CITY OF COACHELLA 1007 ELEVATION: 437.448 DATUM: NAVD83 + 500 FEET
DESCRIPTION: 3 1/2" BRASS DISK SET IN TOP OF THE NORTHWEST CORNER OF A 1.5 FOOT HIGH PLASTER WALL AROUND THE "CITY OF COACHELLA" MONUMENT SIGN, LOCATED 25 FEET SOUTHEAST FROM THE BEGINNING OF THE SOUTH CURB RETURN AND 35 FEET SOUTH WEST OF SOUTHERLY CURB RETURN LOCATED AT THE INTERSECTION OF GRAPEFRUIT BLVD. (HIGHWAY 111) AND CESAR CHAVEZ (HARRISON STREET).

BASES OF BEARINGS: THE BASIS OF BEARINGS FOR THIS SURVEY IS THE CALIFORNIA COORDINATE SYSTEM (CCS83), ZONE 14, AS DETERMINED LOCALLY BY THE LINE BETWEEN CONTINUOUS OPERATING GPS BASE STATION STATIONS P491 AND P1N1, (EPOCH 2017.50) COORDINATES, AS COMPUTED AND PUBLISHED BY SCRIPPS ORBIT AND PERMANENT ARRAY CENTER (SPAC) AND THE CALIFORNIA SPATIAL REFERENCE CENTER (CSRC), BEING: NORTH 78° 59' 35.49" WEST.

BY	DATE	REVISIONS	APPR.	DATE	SEAL	CITY



RECOMMENDED FOR APPROVAL:
NIEL OWLSLEY R.C.E. 39877
DATE: EXP. 12-31-2021
CITY OF COACHELLA
APPROVED FOR CONSTRUCTION:
ANDREW R. SIMONS R.C.E. 72868
DATE: EXP. 09-30-2022

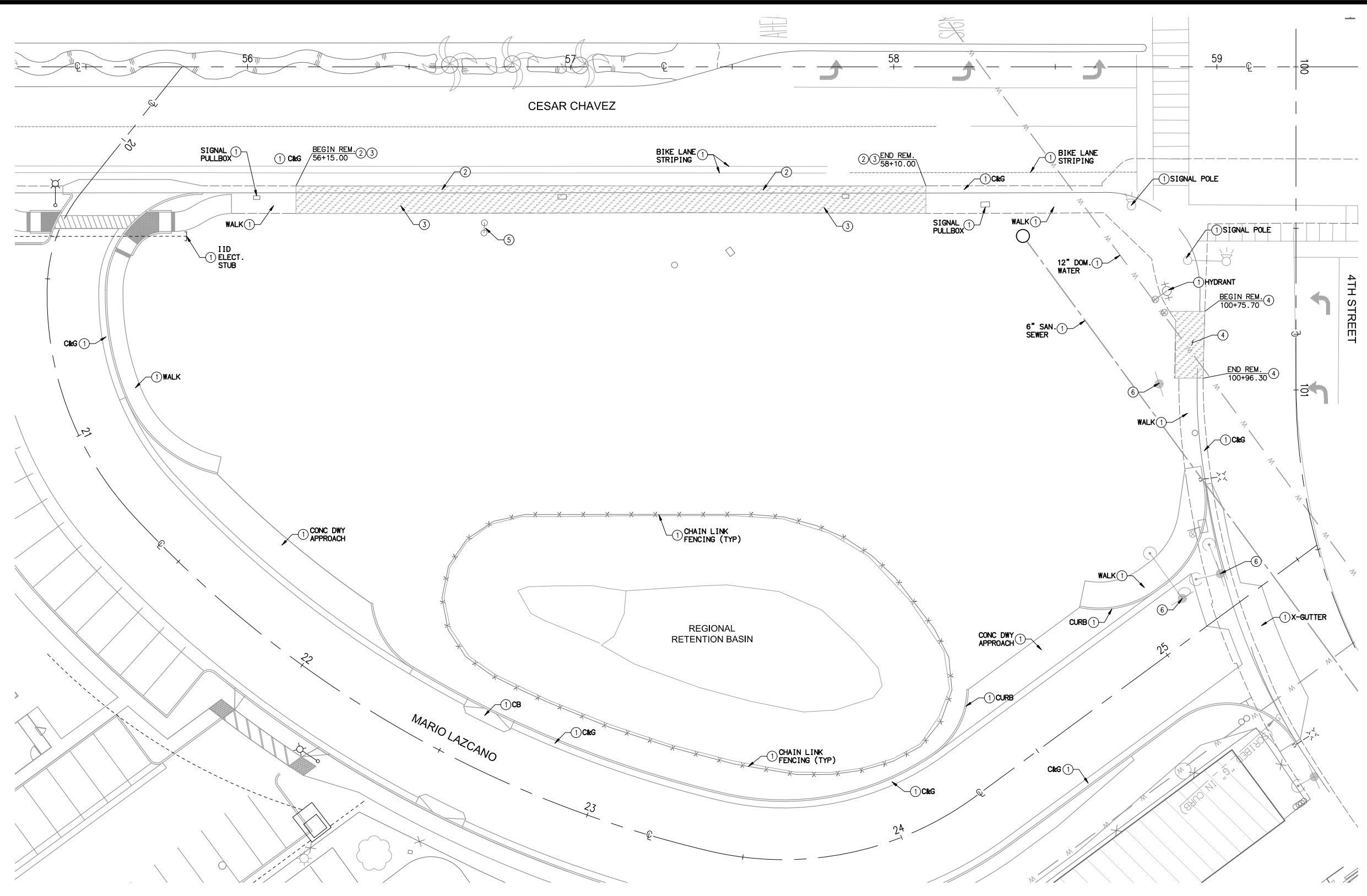


QUALITY, DEDICATION & EXPERIENCE
HEPTAGON SEVEN
8413 E BASELINE RD SUITE 106
MESA, AZ 85209
480.757.0997
PREPARED UNDER THE DIRECT SUPERVISION OF:
BRADLEY J. DOWNS, R.C.E. 68828

CITY OF COACHELLA
SUNLINE TRANSIT
COACHELLA TRANSIT HUB
CONSTRUCTION NOTES & DETAILS
FOR: CITY OF COACHELLA
W.O. 77100149
CITY FILE NO.

07/08/2021

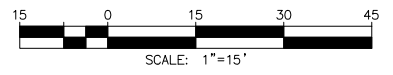
SHEET NO. **2**
OF 9 SHEETS
31



- CONSTRUCTION NOTES**
- ① PROTECT IN PLACE
 - ② REMOVE CURB & GUTTER
 - ③ REMOVE SIDEWALK
 - ④ REMOVE DRIVEWAY
 - ⑤ SALVAGE & REINSTALL STREET SIGN
 - ⑥ COMMUNICATIONS POLE TO BE REMOVED BY OTHERS

LEGEND
 REMOVE CONCRETE ②③④

MARIO LAZCANO NOTE:
 MARIO LAZCANO STREET IMPROVEMENTS FROM STATION 23+00 TO 4TH STREET, ARE A PART OF A SEPARATE CITY IMPROVEMENT PROJECT. SUNLINE'S CONTRACTOR SHALL COORDINATE ALL CONSTRUCTION ACTIVITIES WITH THE CITY'S CONTRACTOR FOR CONNECTIONS INTO MARIO LAZCANO.

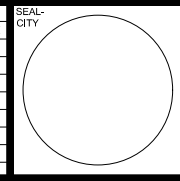


NOTE: WORK CONTAINED WITHIN THESE PLANS SHALL NOT COMMENCE UNTIL AN ENCROACHMENT PERMIT AND/OR A GRADING PERMIT HAS BEEN ISSUED.
 THE PRIVATE ENGINEER SIGNING THESE PLANS IS RESPONSIBLE FOR ASSURING THE ACCURACY AND ACCEPTABILITY OF THE DESIGN HEREON. IN THE EVENT OF DISCREPANCIES ARISING AFTER CITY APPROVAL OR DURING CONSTRUCTION THE PRIVATE ENGINEER SHALL BE RESPONSIBLE FOR DETERMINING AN ACCEPTABLE SOLUTION AND REVISING THE PLANS FOR APPROVAL BY THE CITY.



BENCHMARK: CITY OF COACHELLA 1007 ELEVATION: 437.448 DATUM: NAVD83 + 500 FEET
DESCRIPTION: 3 1/2" BRASS DISK SET IN TOP OF THE NORTHWEST CORNER OF A 1.5 FOOT HIGH PLASTER WALL AROUND THE "CITY OF COACHELLA" MONUMENT SIGN, LOCATED 25 FEET SOUTHEAST FROM THE BEGINNING OF THE SOUTH CURB RETURN AND 35 FEET SOUTH WEST OF SOUTHERLY CURB RETURN LOCATED AT THE INTERSECTION OF GRAPEFRUIT BLVD. (HIGHWAY 111) AND CESAR CHAVEZ (HARRISON STREET).
BASES OF BEARINGS: THE BASES OF BEARINGS FOR THIS SURVEY IS THE CALIFORNIA COORDINATE SYSTEM (CCS83), ZONE 14, AS DETERMINED LOCALLY BY THE LINE BETWEEN CONTINUOUS OPERATING GPS BASE STATION STATIONS P491 AND P1N1, (EPOCH 2017.50) COORDINATES), AS COMPUTED AND PUBLISHED BY SCRIPPS ORBIT AND PERMANENT ARRAY CENTER (SOPAC) AND THE CALIFORNIA SPATIAL REFERENCE CENTER (CSRC), BEING: NORTH 78° 59' 35.41" WEST.

BY	DATE	REVISIONS	APP'D	DATE	SEAL-CITY



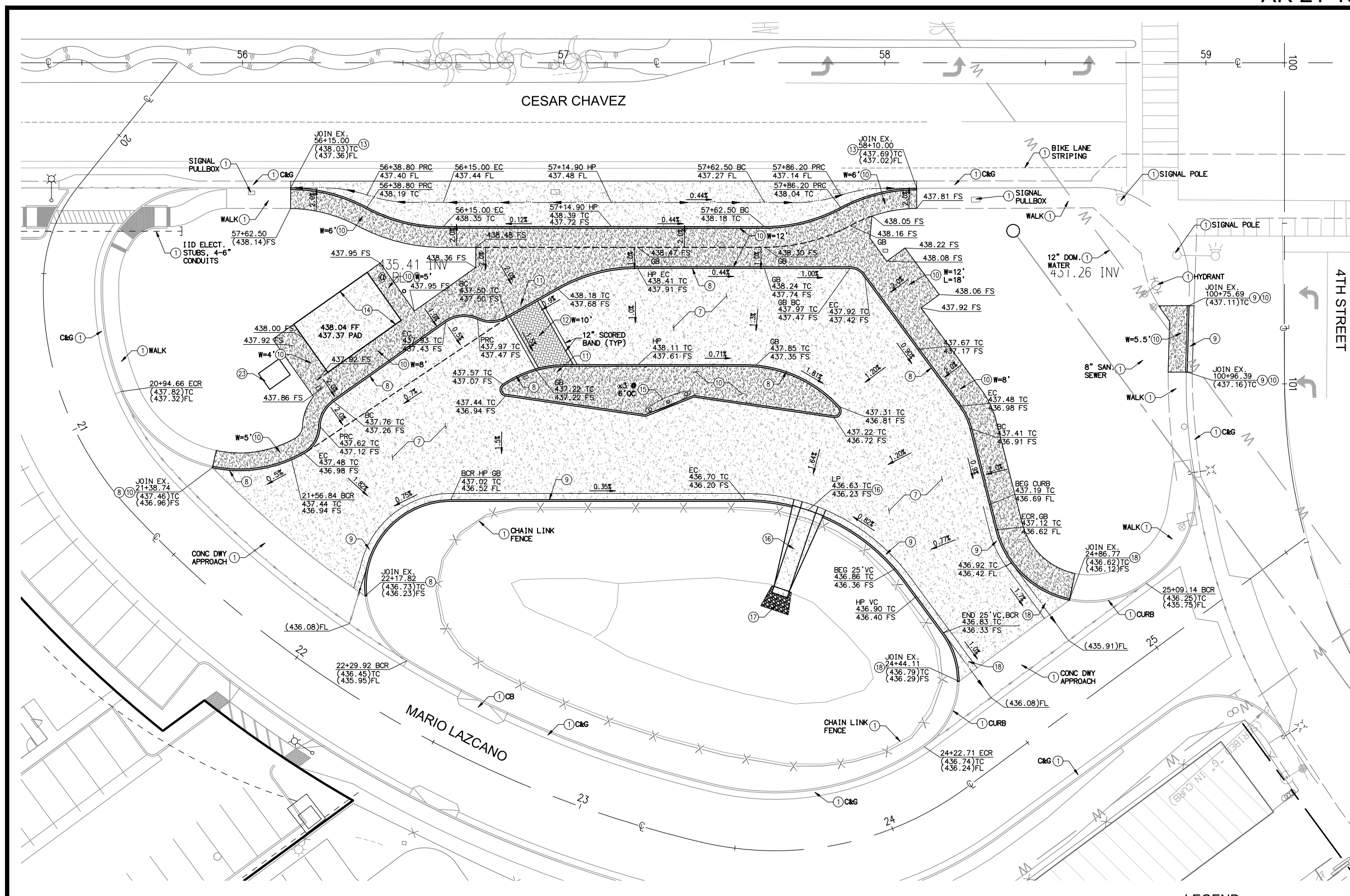
RECOMMENDED FOR APPROVAL
 DATE: EXP. 12-31-2021
CITY OF COACHELLA
 APPROVED FOR CONSTRUCTION
 ANDREW R. SIMONS, R.C.E. 7266 DATE: EXP. 05-30-2022



HEPTAGON SEVEN
 QUALITY, DEDICATION & EXPERIENCE
 8413 E BASELINE RD SUITE 106 MESA, AZ 85209 480.757.0997
 PREPARED UNDER THE DIRECT SUPERVISION OF:
 BRADLEY DONOHUE, R.C.E. 68828 DATE:

CITY OF COACHELLA
SUNLINE TRANSIT
COACHELLA TRANSIT HUB
SITE REMOVALS & EX. UTILITY PLAN
 W.O. 77100149 CITY FILE NO.

06/18/2021
 SHEET NO. **3** OF 9 SHEETS
 32



- CONSTRUCTION NOTES**
- ① PROTECT IN PLACE
 - ⑦ CONST. 6" CONC. w/ FIBER MESH OVER 8" C&G
 - ⑧ CONST. 6" TYPE D CURB PER COC STD S-10
 - ⑨ CONST. 6" C&G PER COC STD S-7
 - ⑩ CONST. SIDEWALK (WIDTH PER PLAN) PER COC STD S25.1
 - ⑪ CONST. CURB RAMP TYPE 1, CASE B PER SPPWC DWG 111-5
 - ⑫ CONST. CROSSWALK w/ TRAFFIC RATED INTERLOCKING PAVERS (SANDSTONE COLOR) PER DETAIL ON SHEET 2
 - ⑬ CONST. BUS TURNOUT PER RCTD STD 814
 - ⑭ CONST. 8" CONCRETE BUILDING SLAB PER DETAIL ON SHEET 2
 - ⑮ F&I METAL BOLLARD PER DETAIL ON SHEET 2
 - ⑯ CONST. CONC. CURB OPENING w/ SPILLWAY PER DETAIL ON SHEET 2
 - ⑰ RIPRAP PER DETAIL ON SHEET 2
 - ⑱ CONST. PARTIAL CONC. SPANDREL PER DETAIL ON SHEET 2
 - ⑳ F&I IID TRANSFORMER PAD PER IID PLANS

MARIO LAZCANO NOTE:
 MARIO LAZCANO STREET IMPROVEMENTS FROM STATION 23+00 TO 4TH STREET, ARE A PART OF A SEPARATE CITY IMPROVEMENT PROJECT. SUNLINE'S CONTRACTOR SHALL COORDINATE ALL CONSTRUCTION ACTIVITIES WITH THE CITY'S CONTRACTOR FOR CONNECTIONS INTO MARIO LAZCANO.

LINE / CURVE DATA TABLE
 SEE SHEET 5 FOR LINE / CURVE INFORMATION ON CURB AND CENTERLINES.

- LEGEND**
- MASONRY PAVERS ⑫
 - CONCRETE WALK ⑩ ⑪
 - CONCRETE CONST ⑦
 - RIP RAP ⑰



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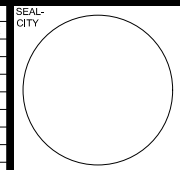
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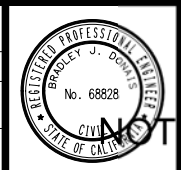
BENCHMARK: CITY OF COACHELLA 1007 ELEVATION: 437.448 DATUM: NAVD83 + 500 FEET
 DESCRIPTION: 3 1/2" BRASS DISK SET IN TOP OF THE NORTHWEST CORNER OF A 1.5 FOOT HIGH PLANTER BOLLARD AROUND THE "CITY OF COACHELLA" MONUMENT SIGAL, LOCATED 25 FEET SOUTHWEST FROM THE BEGINNING OF THE SOUTH CURB RETURN AND 35 FEET SOUTH WEST OF SOUTHERLY CURB RETURN LOCATED AT THE INTERSECTION OF GRAPPESTRIT BLVD. (HIGHWAY 111) AND CESAR CHAVEZ (HARRISON STREET).

BASIS OF BEARINGS: THE BASIS OF BEARINGS FOR THIS SURVEY IS THE CALIFORNIA COORDINATE SYSTEM (CCS83), ZONE 14, AS DETERMINED LOCALLY BY THE LINE BETWEEN CONTINUOUS OPERATING GPS BASE STATION STATIONS P491 AND P1N1, (EPOCH 2017.50) COORDINATES), AS COMPUTED AND PUBLISHED BY SCRIPPS ORBIT AND PERMANENT ARMY CENTER (SOPAC) AND THE CALIFORNIA SPATIAL REFERENCE CENTER (CSRC), BEING: NORTH 78° 59' 35.41" WEST.

NO.	REVISIONS	DATE	BY	CHKD.	APP'D.	DATE



RECOMMENDED FOR APPROVAL:
 NIDEL OWLSLEY R.C.E. 39877 DATE: EXP. 12-31-2021
CITY OF COACHELLA
 APPROVED FOR CONSTRUCTION:
 ANDREW R. SIMONS R.C.E. 72868 DATE: EXP. 09-30-2022

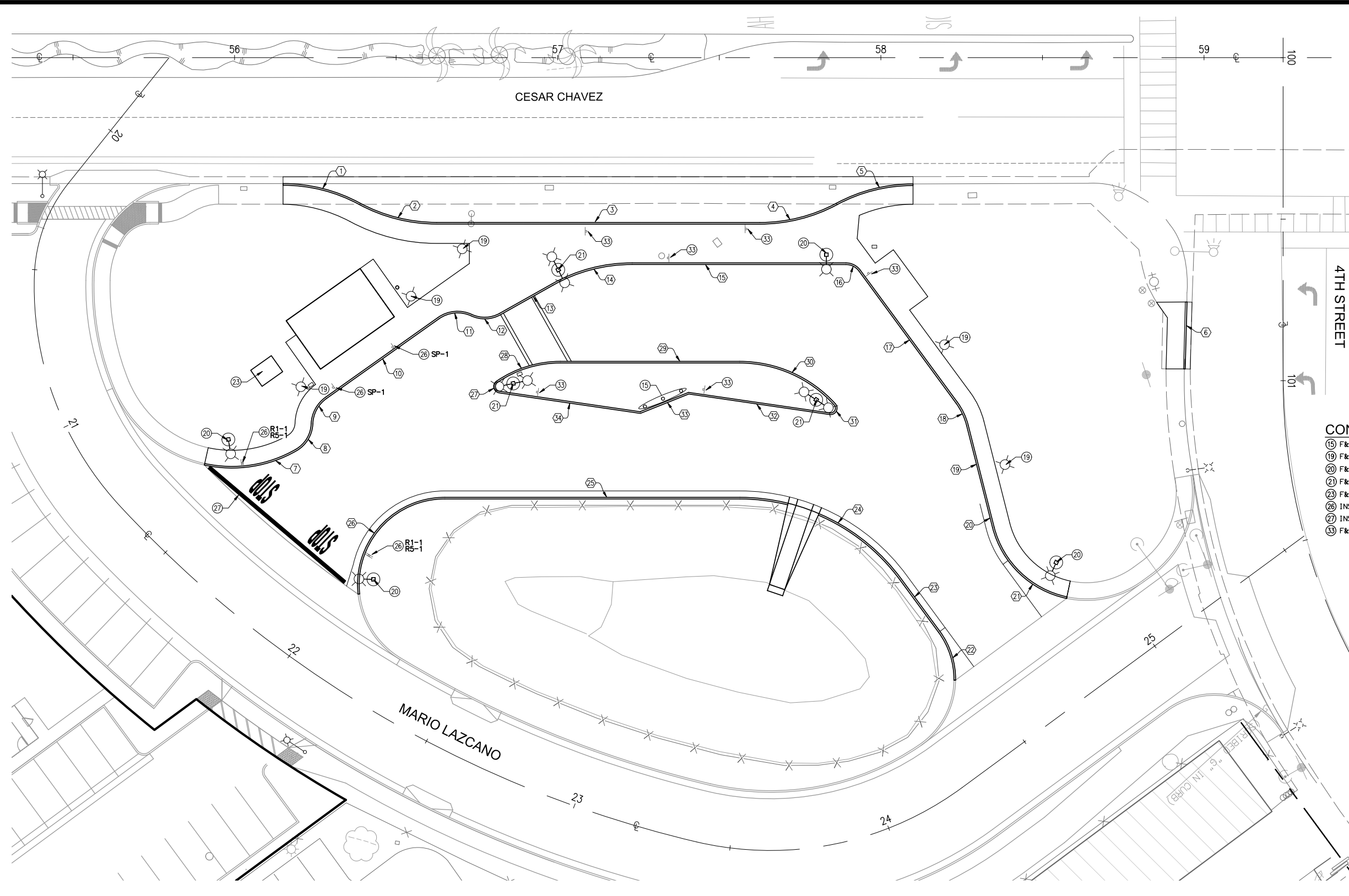


HEPTAGON SEVEN
 QUALITY, DEDICATION & EXPERIENCE
 8413 E BASELINE RD SUITE 108 MESA AZ 85209 480.757.0997
 PREPARED UNDER THE DIRECT SUPERVISION OF:
 DRAPLEY JOHNSON INC. ENGINEERS

CITY OF COACHELLA
 SUNLINE TRANSIT
 COACHELLA TRANSIT HUB
 PRECISE GRADING

06/18/2021
 SHEET NO. **4**
 OF 9 SHEETS
 33

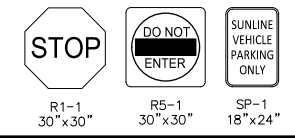
NOT FOR CONSTRUCTION



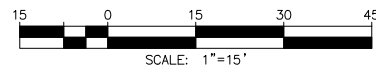
- CONSTRUCTION NOTES**
- 15 F&I METAL BOLLARD PER DETAIL ON SHEET 2
 - 19 F&I PARKING LOT LIGHT, POST TYPE PER DETAIL ON SHEET 9
 - 20 F&I PARKING LOT LIGHT, SINGLE HEAD MAST, PER DETAIL ON SHEET 9
 - 21 F&I PARKING LOT LIGHT, DUAL HEAD MAST, PER DETAIL ON SHEET 9
 - 23 F&I IID TRANSFORMER PAD PER IID PLANS
 - 26 INSTALL SIGN
 - 27 INSTALL STOP BAR & "STOP" PM
 - 33 F&I SIGN POST ONLY FOR SUNLINE BUS STOP SIGNAGE

LINE / CURVE DATA	NO	BEARING/DELTA	RADIUS	LENGTH	DESCRIPTION
1	Δ=28°23'29"	50.00'	24.78'	BUS TURNOUT CURB	
2	Δ=28°21'18"	50.00'	24.74'	BUS TURNOUT CURB	
3	N00°13'00"E	---	100.00'	BUS TURNOUT CURB	
4	Δ=28°17'47"	50.00'	24.69'	BUS TURNOUT CURB	
5	Δ=28°20'14"	49.95'	24.71'	BUS TURNOUT CURB	
6	N88°14'57"W	---	20.70'	6" CURB	
7	Δ=42°22'25"	40.00'	29.58'	6" CURB	
8	Δ=57°58'18"	10.00'	10.12'	6" CURB	
9	Δ=52°40'09"	10.00'	9.19'	6" CURB	
10	N35°08'29"W	---	42.68'	6" CURB	
11	Δ=60°09'19"	10.00'	10.50'	6" CURB	
12	Δ=54°12'10"	10.00'	9.46'	6" CURB	
13	N29°11'20"W	---	18.57'	6" CURB	
14	Δ=29°24'20"	50.00'	25.66'	6" CURB	
15	N00°13'00"E	---	66.14'	6" CURB	
16	Δ=53°39'58"	5.00'	4.68'	6" CURB	
17	N53°52'58"E	---	50.73'	6" CURB	
18	Δ=22°03'10"	20.00'	7.70'	6" CURB	
19	N75°56'08"E	---	24.52'	6" CURB	
20	N75°56'08"E	---	8.04'	6" CURB	
21	Δ=63°53'39"	30.00'	33.45'	6" CURB SPANDREL	
22	Δ=36°57'37"	25.00'	16.13'	6" CURB SPANDREL	
23	N53°51'06"E	---	20.15'	6" CURB	
24	Δ=53°39'02"	62.00'	58.06'	6" CURB	
25	N00°12'04"E	---	91.31'	6" CURB	
26	Δ=96°15'16"	27.00'	45.36'	6" CURB	
27	Δ=137°20'38"	2.00'	4.79'	6" CURB	
28	Δ=34°22'53"	35.00'	21.00'	6" CURB	
29	N00°13'00"E	---	55.47'	6" CURB	
30	Δ=47°45'11"	40.00'	33.34'	6" CURB	
31	Δ=140°31'18"	2.00'	4.91'	6" CURB	
32	N08°29'29"E	---	44.05'	6" CURB	
33	N21°35'12"W	---	16.16'	6" CURB	
34	N08°29'29"E	---	44.10'	6" CURB	

SIGN LEGEND



SITE LIGHTING NOTE
SEE SHEET 6, PRIVATE UTILITY, FOR DETAILS ON SITE LIGHTING.

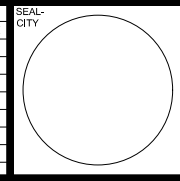


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DIGALERT
CALL BEFORE YOU DIG
TOLL FREE 1-800-485-2800
A PUBLIC SERVICE BY UNDERGROUND SERVICE ALERT

BENCHMARK: CITY OF COACHELLA 1007 ELEVATION: 437.448 DATUM: NAD83 + 500 FEET
DESCRIPTION: 3 1/2" BRASS DISK SET IN TOP OF THE NORTHWEST CORNER OF A 1.5 FOOT HIGH PLASTER WALL AROUND THE "CITY OF COACHELLA" MONUMENT SIGN, LOCATED 25 FEET SOUTHWEST FROM THE BEGINNING OF THE SOUTH CURB RETURN AND 35 FEET SOUTH WEST OF SOUTHERLY CURB RETURN LOCATED AT THE INTERSECTION OF GRAPEFRUIT BLVD. (HIGHWAY 111) AND CESAR CHAVEZ (HARRISON STREET).
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BY	DATE	REVISIONS	APP'D	DATE	SEAL	CITY



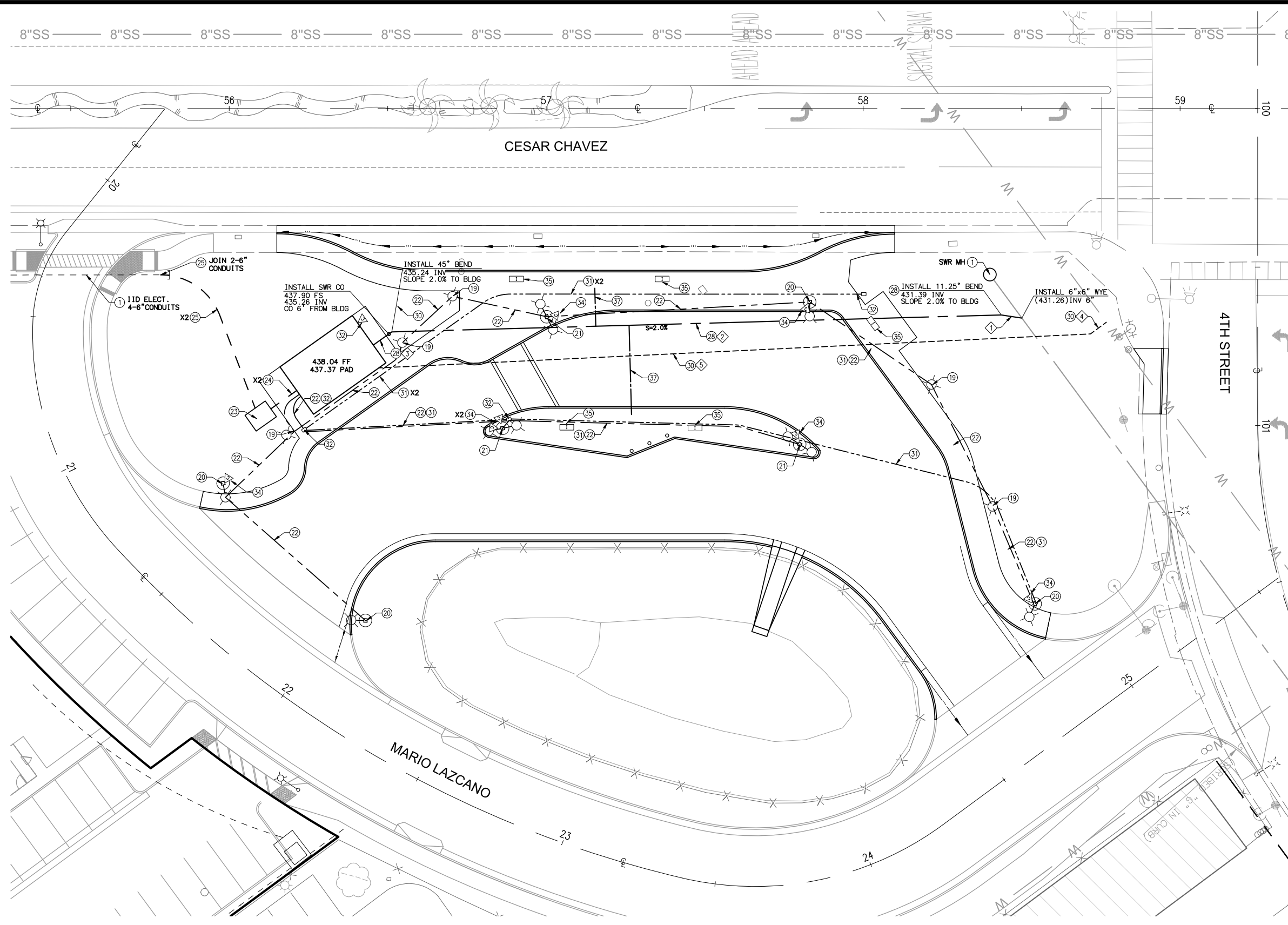
RECOMMENDED FOR APPROVAL
DATE: EXP. 12-31-2021
CITY OF COACHELLA
APPROVED FOR CONSTRUCTION
ANDREW R. SIMONS, R.C.E. 72868 DATE: EXP. 09-30-2022



HEPTAGON SEVEN
QUALITY, DEDICATION & EXPERIENCE
8413 E BASELINE RD SUITE 106 MESA, AZ 85209 480.757.0997
PREPARED UNDER THE DIRECT SUPERVISION OF:
BRADLEY DOWNS, R.C.E. 68828 DATE:

CITY OF COACHELLA
SUNLINE TRANSIT
COACHELLA TRANSIT HUB
HORIZONTAL CONTROL, SIGNING & STRIPING
CITY OF COACHELLA W.O. 77100149 CITY FILE NO.

07/08/2021
SHEET NO. 5 OF 8 SHEETS
34



WATER / SEWER DATA				
ID	BEARING/DELTA	RADIUS	LENGTH	NOTE
1	N08°51'08"E	--	6.58'	6" PVC SEWER
2	N01°23'29"W	--	192.36'	6" PVC SEWER
3	N35°08'29"W	--	7.17'	6" PVC SEWER
4	N36°07'02"W	--	6.20'	2" DW SERVICE
5	N02°32'02"W	--	224.80'	2" DW SERVICE

- CONSTRUCTION NOTES**
- ① PROTECT IN PLACE
 - ⑱ F&I PARKING LOT LIGHT, POST TYPE PER DETAIL ON SHEET 9
 - ⑳ F&I PARKING LOT LIGHT, SINGLE HEAD MAST, PER DETAIL ON SHEET 9
 - ㉑ F&I PARKING LOT LIGHT, DUAL HEAD MAST, PER DETAIL ON SHEET 9
 - ㉒ F&I 1.5" PVC CONDUIT & CONDUCTORS FOR SITE LIGHTING
 - ㉓ F&I IID TRANSFORMER PAD PER IID PLANS
 - ㉔ F&I 4" SCH 40 PVC CONDUIT (IID)
 - ㉕ F&I 6" SCH 40 PVC CONDUIT (IID)
 - ㉖ F&I 6" SDR-26 PVC SEWER LATERAL w/ BENDS & WYES PER COC STD. D-11
 - ㉗ F&I 2" PVC SDR-21 DOMESTIC WATER SERVICE w/ BENDS & THRUST BLOCKS PER COC STD W-8
 - ㉘ F&I 1.5" PVC CONDUIT w/ PULL ROPE (DATA & FUTURE)
 - ㉙ F&I ELECTRICAL PULL BOX PER SWPPP STD 513-3
 - ㉚ F&I SECURITY CAMERA & MOUNTING BRACKETS
 - ㉛ BUS STOP INFO SIGN INSTALLED BY SUNLINE
 - ㉜ F&I 3/4" PVC SDR-21 DOMESTIC WATER SERVICE w/ BENDS, CORP STOP & VALVE BOX



07/08/2021

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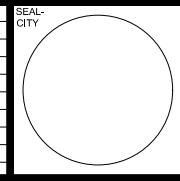
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BY	DATE	REVISIONS	APP'D	DATE	SEAL



RECOMMENDED FOR APPROVAL
 NCEL OWLSLEY R.C.E. 39877
 DATE: EXP. 12-31-2021

APPROVED FOR CONSTRUCTION
 ANDREW R. SIMONS R.C.E. 72868
 DATE: EXP. 09-30-2022

CITY OF COACHELLA



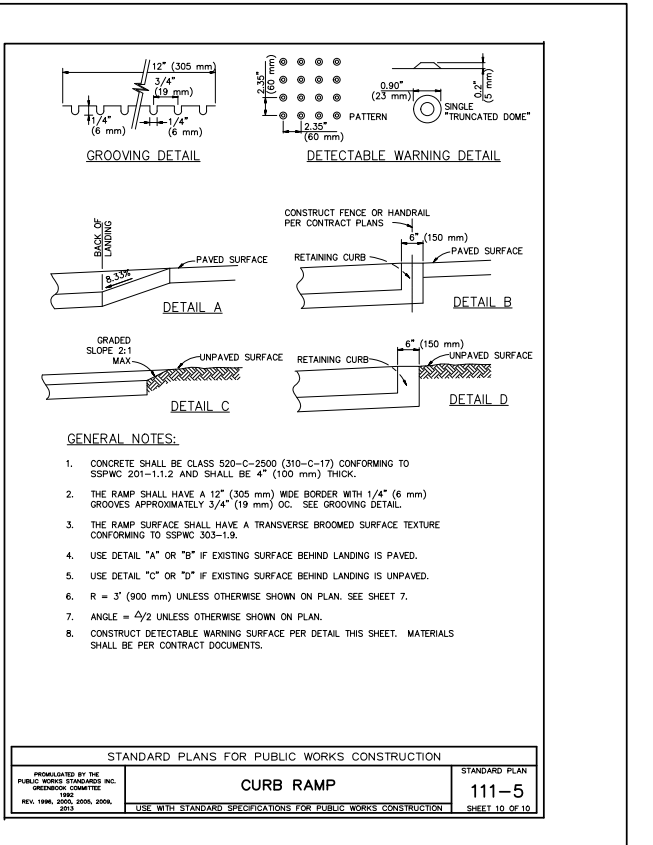
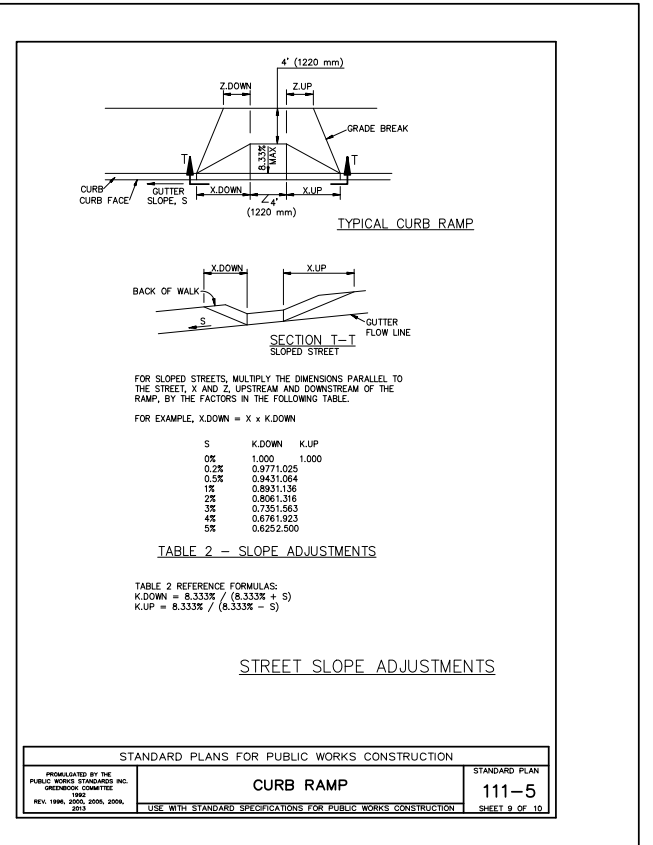
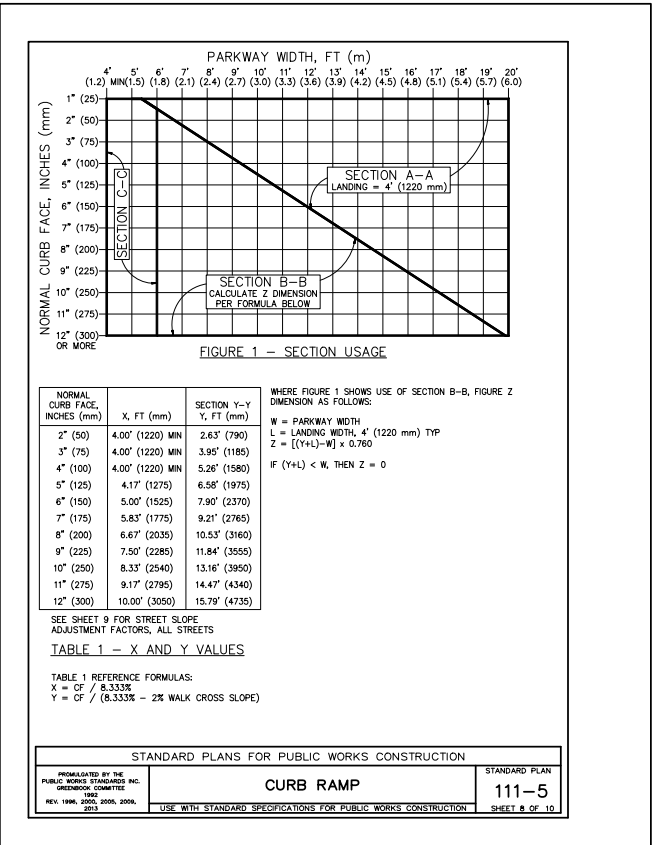
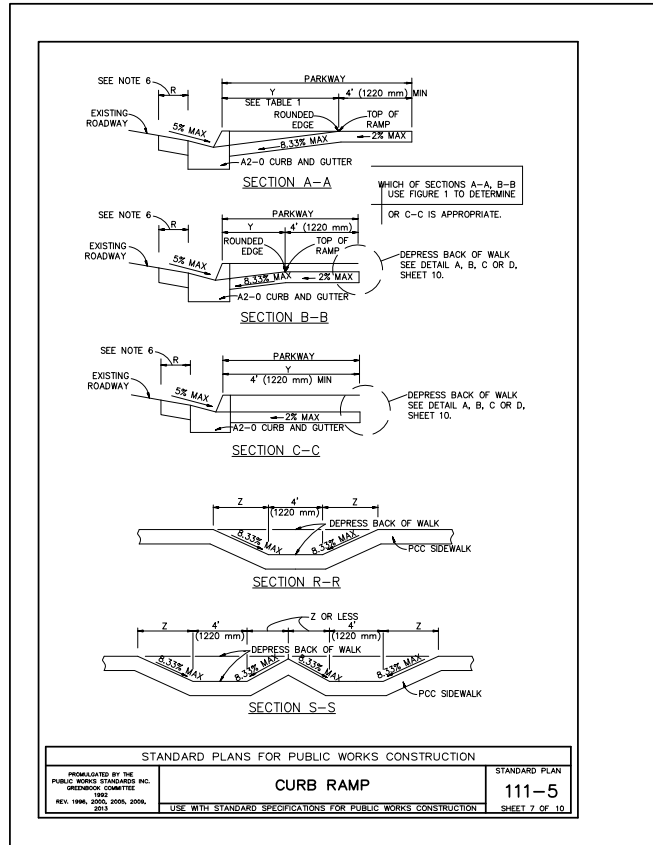
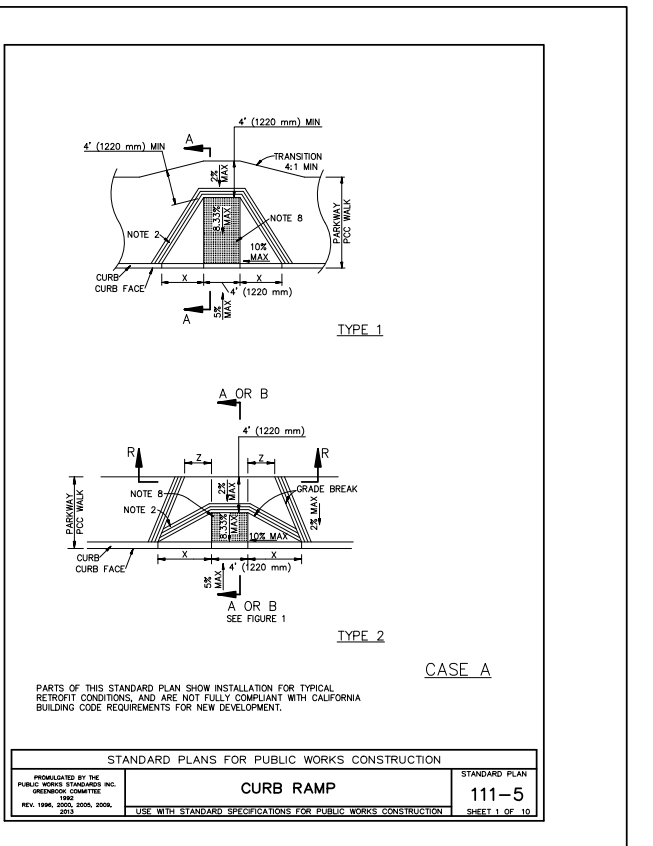
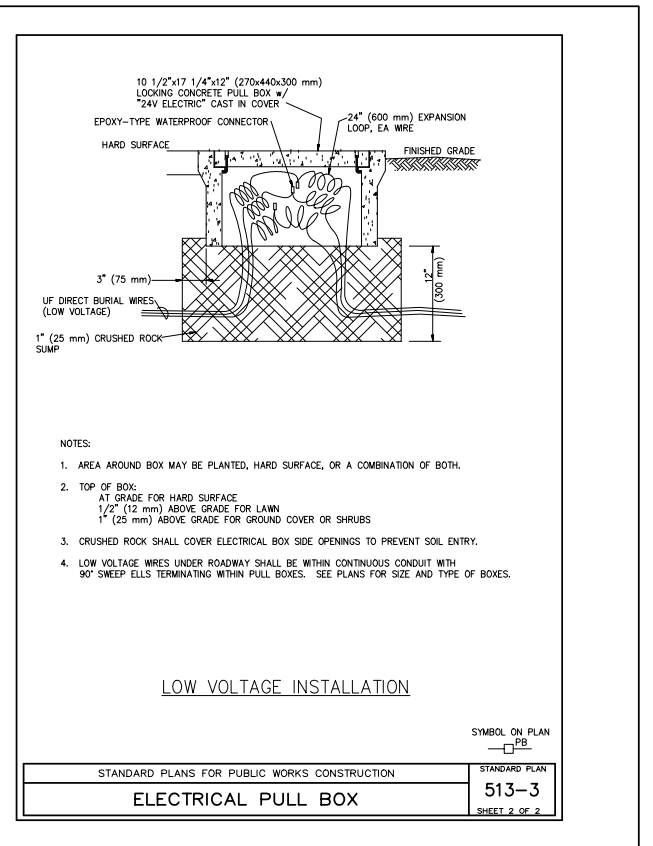
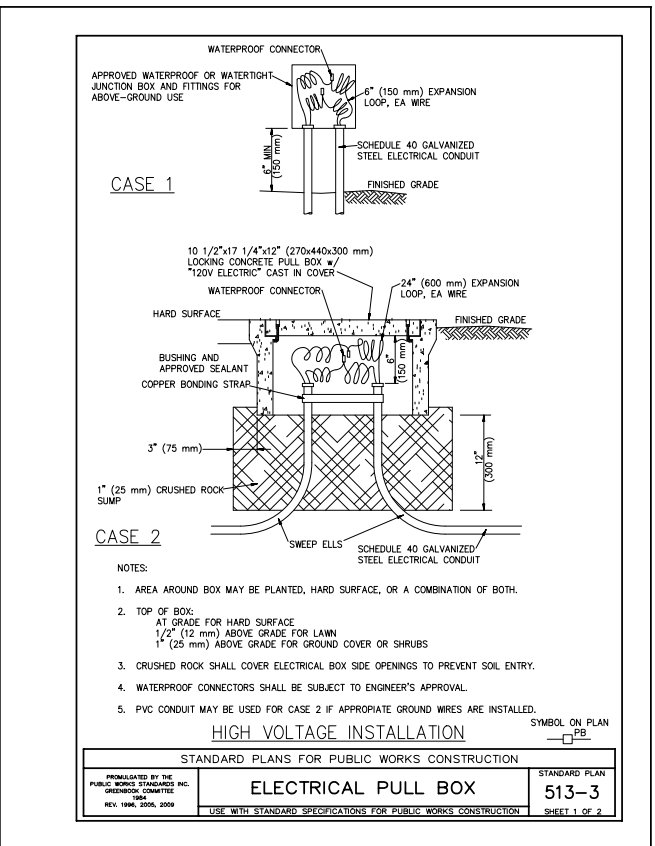
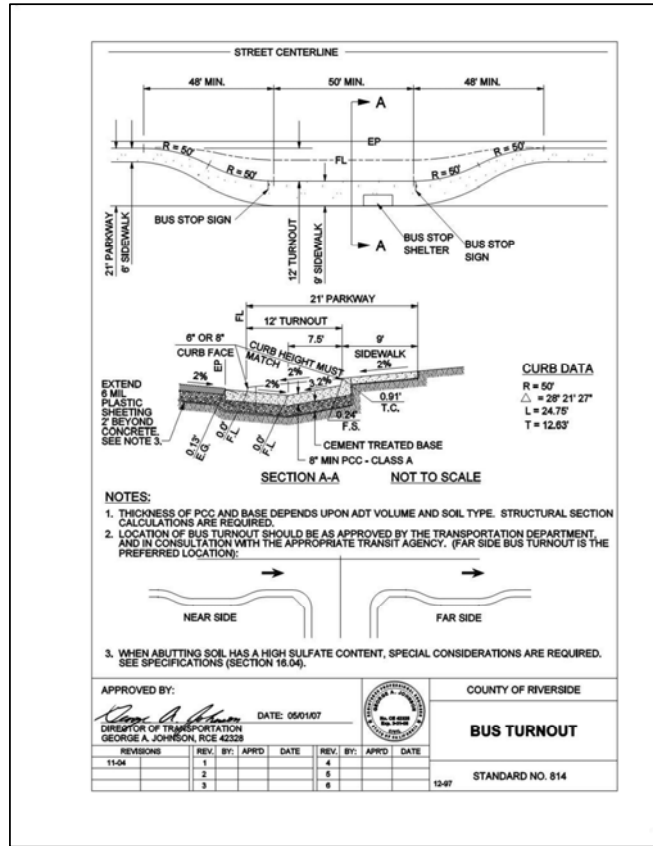
HEPTAGON SEVEN
 QUALITY, DEDICATION & EXPERIENCE
 8413 E BASELINE RD SUITE 106 MESA AZ 85209 480.757.0997

PREPARED UNDER THE DIRECT SUPERVISION OF:
 BRADLEY DOWNS, R.C.E. 68828

CITY OF COACHELLA
 SUNLINE TRANSIT
 COACHELLA TRANSIT HUB
 PRIVATE UTILITY PLAN

FOR THE CITY OF COACHELLA

SHEET NO. **6**
 OF 9 SHEETS
 FILE NO. 77100149



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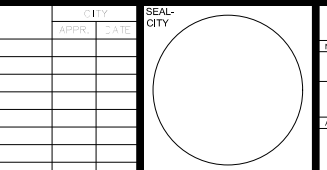
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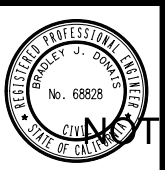
BENCHMARK: CITY OF COACHELLA 1007 ELEVATION: 437.448 DATE: 11/08/98 + 500 FEET
 DESCRIPTION: 3 1/2" BRASS DISK SET IN TOP OF THE NORTHWEST CORNER OF A 1.5 FOOT HIGH PLASTER WALL ALONG THE "CITY OF COACHELLA" MONUMENT SIGAL, LOCATED 25 FEET SOUTHWEST FROM THE BEGINNING OF THE SOUTH CURB RETURN AND 35 FEET SOUTH WEST OF SOUTHERLY CURB RETURN LOCATED AT THE INTERSECTION OF GRAPESFRUIT BLVD. (HIGHWAY 111) AND CESAR CHAVEZ (HARRISON STREET).

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BY	DATE	REVISIONS	APP'D	CHECKED	DATE

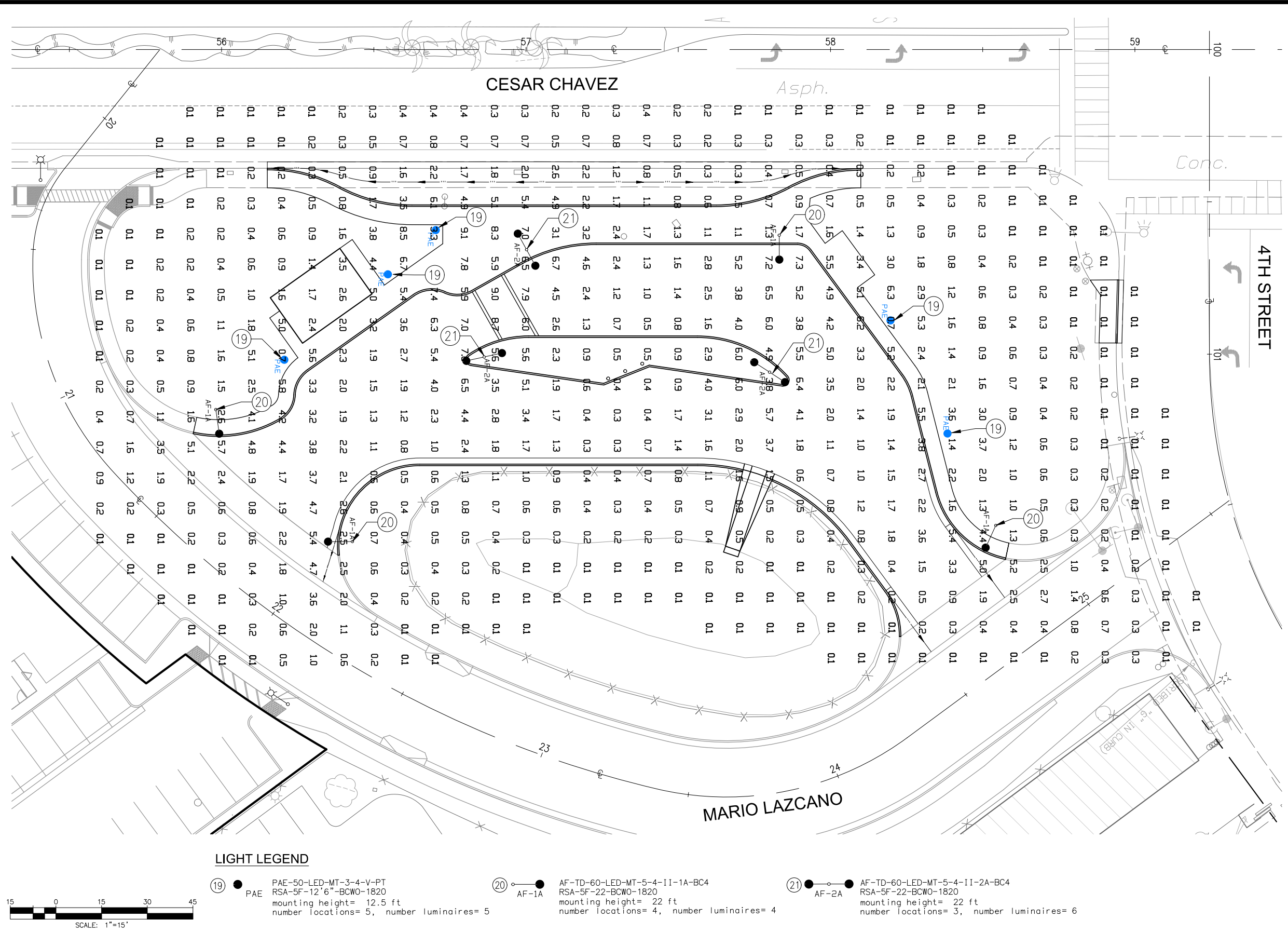


RECOMMENDED FOR APPROVAL
 NIEL OWSELY R.C.E. 39877 DATE: EXP. 12-31-2021
CITY OF COACHELLA
 APPROVED FOR CONSTRUCTION:
 ANDREW R. SIMONS R.C.E. 7266 DATE: EXP. 09-30-2022



QUALITY, DEDICATION & EXPERIENCE
HEPTAGON SEVEN
 8413 E BASELINE RD SUITE 108 MESA, AZ 85209 480.757.0997
 PREPARED UNDER THE DIRECT SUPERVISION OF:
CONTRACTOR FOR CONSTRUCTION
 GRANVILLE LINDEN, R.C.E. 68288 DATE:

CITY OF COACHELLA
SUNLINE TRANSIT COACHELLA TRANSIT HUB STANDARD DETAILS
 W.O. 77100149 CITY FILE NO.



- CONSTRUCTION NOTES**
- ①9 F&I PARKING LOT LIGHT, POST TYPE PER DETAIL ON SHEET 9
 - ②0 F&I PARKING LOT LIGHT, SINGLE HEAD MAST, PER DETAIL ON SHEET 9
 - ②1 F&I PARKING LOT LIGHT, DUAL HEAD MAST, PER DETAIL ON SHEET 9

PACIFIC LIGHTING & STANDARD
 2831 LOS FLORES BLVD.
 LYNWOOD, CA 90262
 (310)603-9344

LIGHT READINGS ARE ESTIMATED LEVELS IN PRIME CONDITIONS AND SHOULD NOT BE TAKEN AS EXACT READINGS. LIGHT READINGS ARE 10 FT. APART

Target Point Set
 625 points at z=0, sp 10ft by 10ft
 HORIZONTAL FOOTCANDLES
 Average 1.5
 Maximum 9.9
 Minimum 0.1
 Avg: Min 14.91
 Max: Min 99.00
 Coef Var 1.28
 UnifGrad 9.00

LIGHT LEGEND

- ①9 ● PAE PAE-50-LED-MT-3-4-V-PT
 RSA-5F-12'6"-BCWO-1820
 mounting height= 12.5 ft
 number locations= 5, number luminaires= 5
- ②0 ● AF-1A AF-TD-60-LED-MT-5-4-II-1A-BC4
 RSA-5F-22-BCWO-1820
 mounting height= 22 ft
 number locations= 4, number luminaires= 4
- ②1 ● AF-2A AF-TD-60-LED-MT-5-4-II-2A-BC4
 RSA-5F-22-BCWO-1820
 mounting height= 22 ft
 number locations= 3, number luminaires= 6



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BY	DATE	REVISIONS	APP'D	DATE	SEAL	CITY

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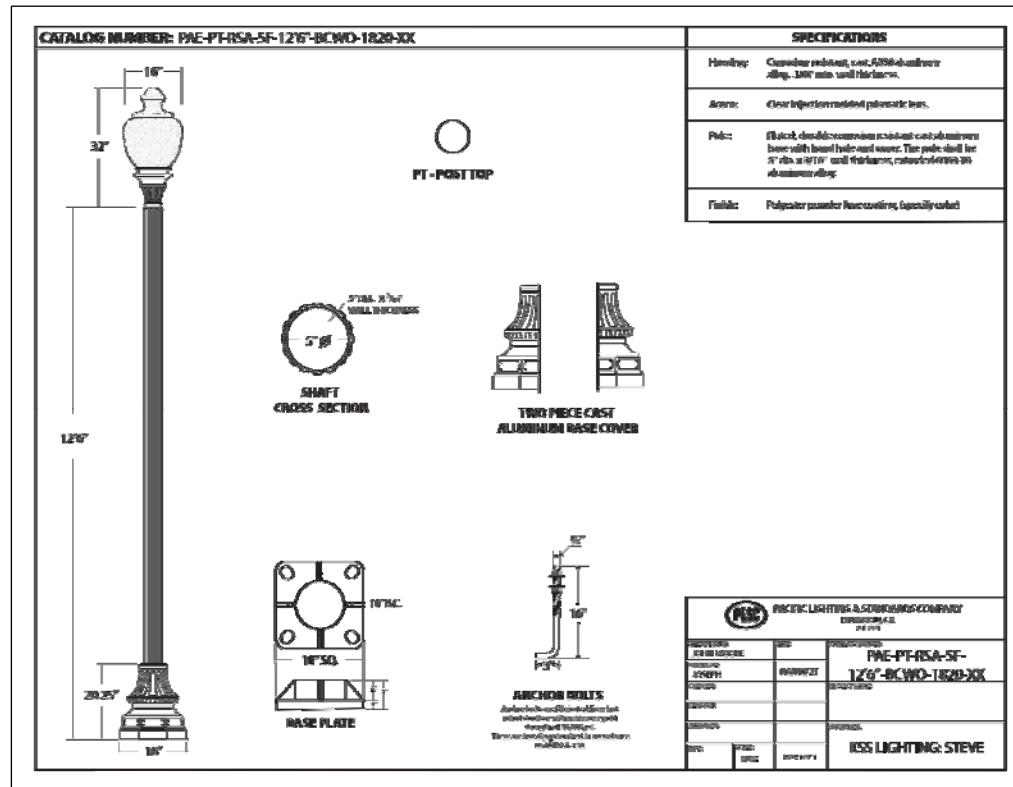


HEPTAGON SEVEN
 QUALITY, DEDICATION & EXPERIENCE
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 PREPARED UNDER THE DIRECT SUPERVISION OF:
 BRADLEY DOWNS, R.C.E. 68828 DATE:

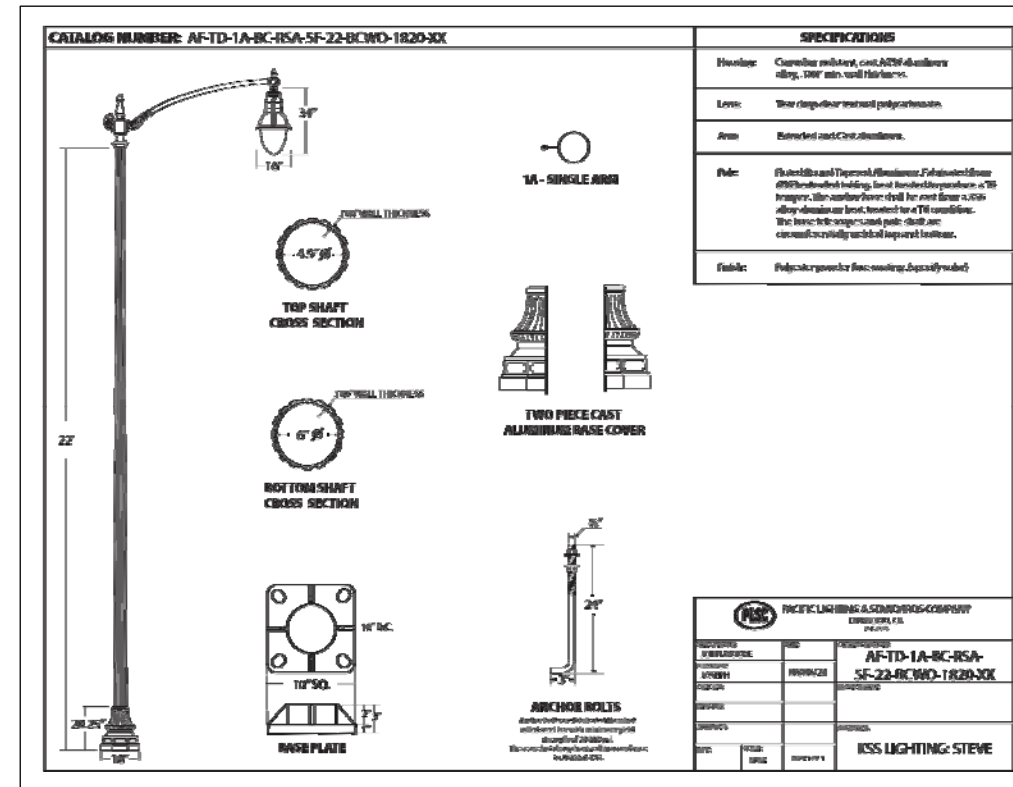
CITY OF COACHELLA
 SUNLINE TRANSIT
 COACHELLA TRANSIT HUB
 SITE LIGHTING
 PHOTOMETRICS

SHEET NO. **8**
 OF 9 SHEETS
 W.O. 77100149 CITY FILE NO. 37

07/08/2021



19 POST LIGHT POLE DETAIL
NOT TO SCALE



20 SINGLE HEAD MAST ARM POLE DETAIL
NOT TO SCALE

NOTE: WORK CONTAINED WITHIN THESE PLANS SHALL NOT COMMENCE UNTIL AN ENCROACHMENT PERMIT AND/OR A GRADING PERMIT HAS BEEN ISSUED.

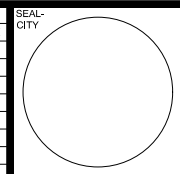
THE PRIVATE ENGINEER SIGNING THESE PLANS IS RESPONSIBLE FOR ASSURING THE ACCURACY AND ACCEPTABILITY OF THE DESIGN HEREON. IN THE EVENT OF DISCREPANCIES ARISING AFTER CITY APPROVAL OR DURING CONSTRUCTION THE PRIVATE ENGINEER SHALL BE RESPONSIBLE FOR DETERMINING AN ACCEPTABLE SOLUTION AND REVISORING THE PLANS FOR APPROVAL BY THE CITY.

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 CALL BEFORE YOU DIG
 TOLL FREE 1-800-224-3800
 A PUBLIC SERVICE BY UNDERGROUND SERVICE ALERT

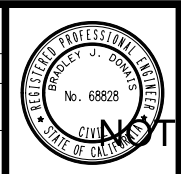
BENCHMARK: CITY OF COACHELLA 1007 ELEVATION: 437.448 DATUM: NAVD83 + 500 FEET
DESCRIPTION: 3 1/2" BRASS DISK SET IN TOP OF THE NORTHWEST CORNER OF A 1.5 FOOT HIGH PLANTER BOLLARD LOCATED 25 FEET SOUTHEAST FROM THE BEGINNING OF THE SOUTH CURB RETURN AND 35 FEET SOUTH WEST OF SOUTHERLY CURB RETURN LOCATED AT THE INTERSECTION OF GRAPESFRUIT BLVD. (HIGHWAY 111) AND CESAR CHAVEZ (HARRISON STREET).

BASES OF BEARINGS: THE BASIS OF BEARINGS FOR THIS SURVEY IS THE CALIFORNIA COORDINATE SYSTEM (CCS83), ZONE 14, AS DETERMINED LOCALLY BY THE LINE BETWEEN CONTINUOUS OPERATING GPS BASE STATION STATIONS P491 AND P1N1, (EPOCH 2017.50) COORDINATES, AS COMPUTED AND PUBLISHED BY SCRIPPS ORBIT AND PERMANENT ARRAY CENTER (SOPAC) AND THE CALIFORNIA SPATIAL REFERENCE CENTER (CSRC), BEING: **NORTH 78° 59' 35.44" WEST.**

BY	DATE	REVISIONS	APP'D	DATE	SEAL CITY



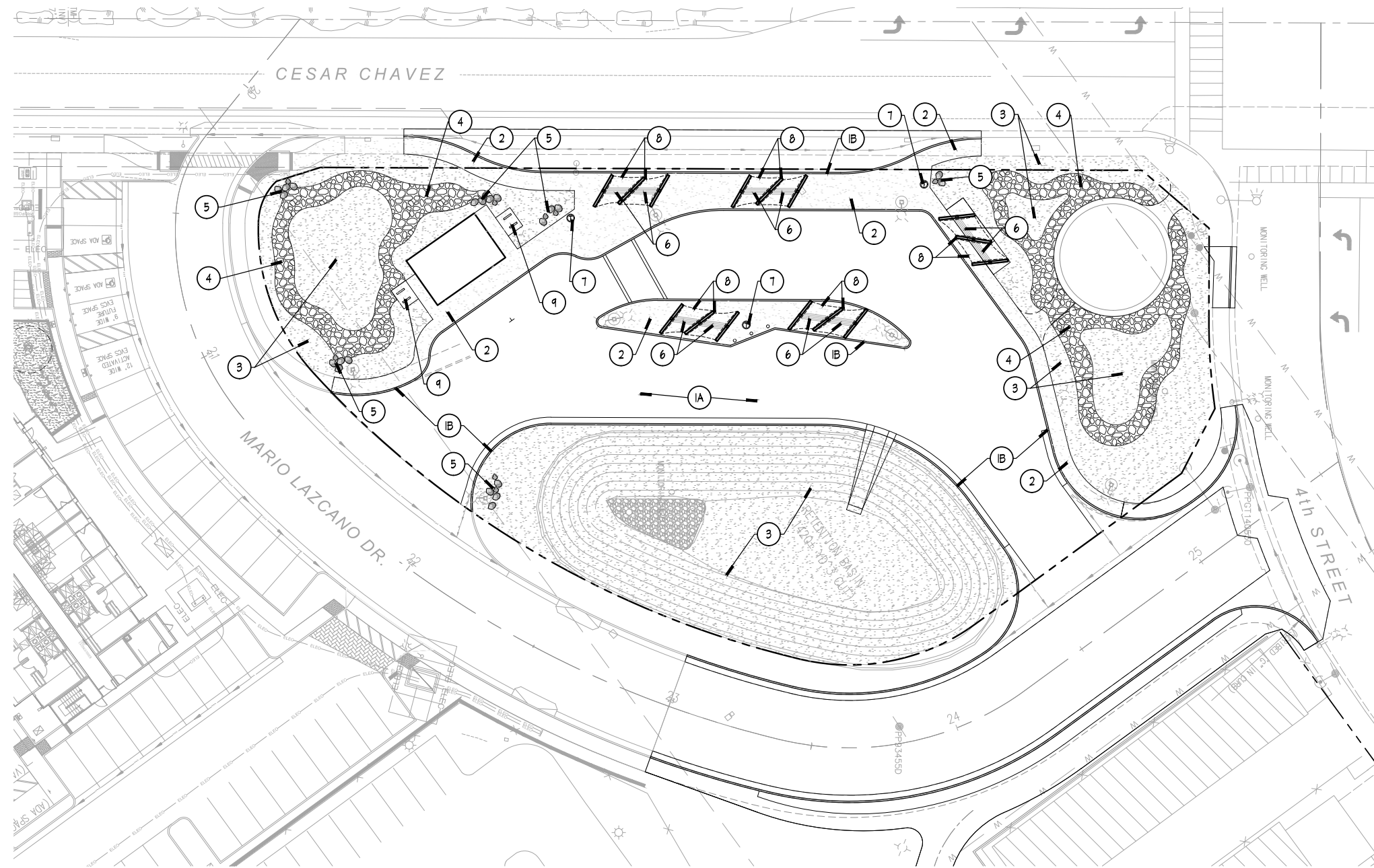
RECOMMENDED FOR APPROVAL
 NCEL OWLSLEY R.C.E. 39877
 DATE: EXP. 12-31-2021
CITY OF COACHELLA
 APPROVED FOR CONSTRUCTION
 ANDREW R. SIMONS R.C.E. 72868
 DATE: EXP. 05-30-2022



HEPTAGON SEVEN
 QUALITY, DEDICATION & EXPERIENCE
 8413 E BASELINE RD SUITE 106 MESA, AZ 85209 480.757.0997
 PREPARED UNDER THE DIRECT SUPERVISION OF:
 BRADLEY DOWNEY, R.C.E. 68828 DATE:

CITY OF COACHELLA
SUNLINE TRANSIT
COACHELLA TRANSIT HUB
SITE LIGHTING DETAILS
 FOR: CITY OF COACHELLA
 W.O. 77100149
 CITY FILE NO.

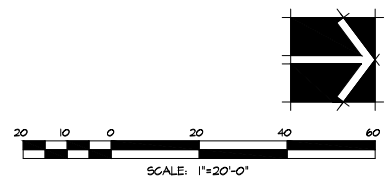
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 SHEET NO. **9**
 OF 9 SHEETS
38



REFERENCE NOTES SCHEDULE

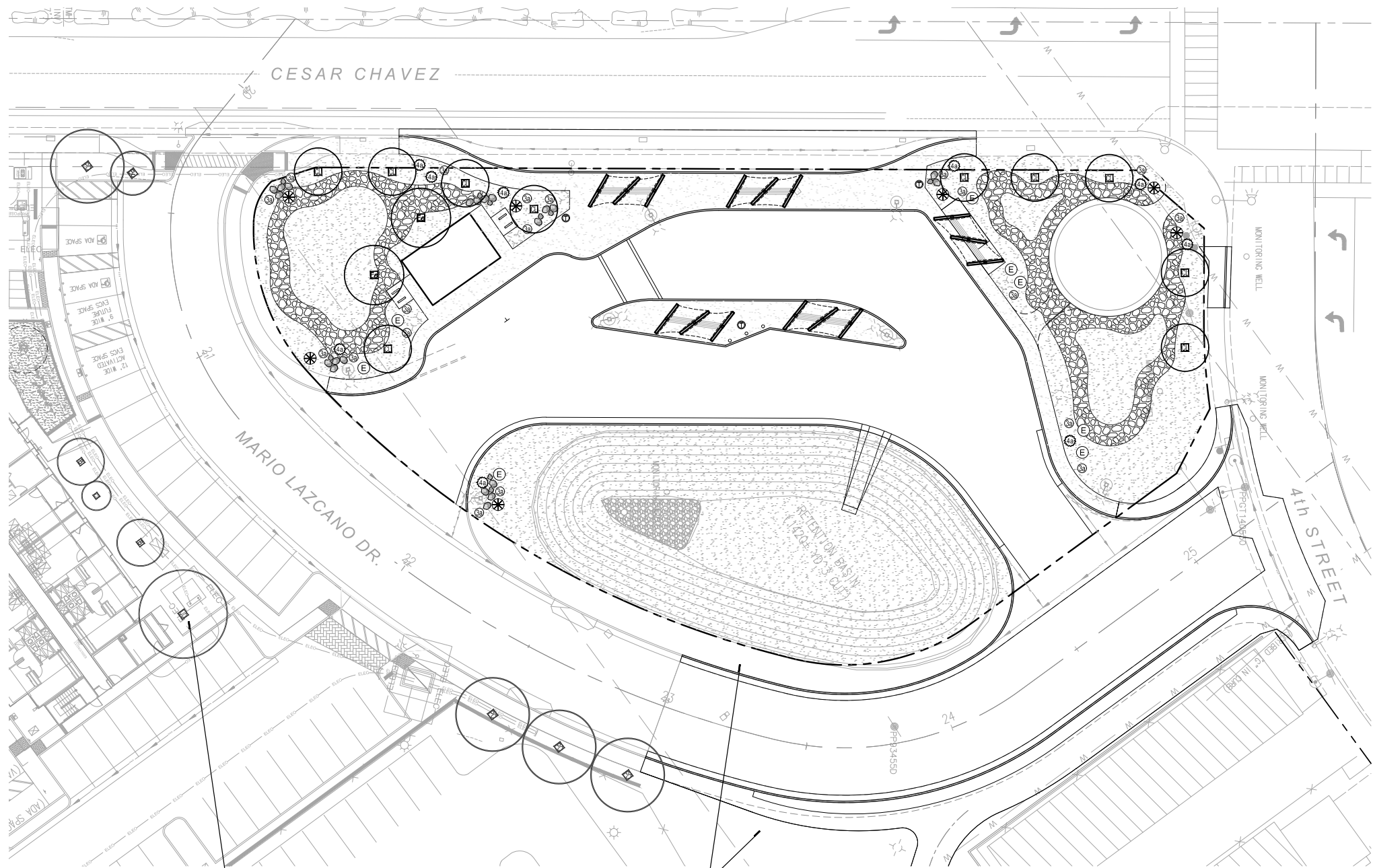
SYMBOL	DESCRIPTION	QTY	DETAIL
(1A)	AC PAVING PER CIVIL ENGINEER		
(1B)	CURB & GUTTER PER CIVIL ENGINEER		
(2)	SIDEWALK # PER CIVIL ENGINEER - NATURAL GRAY CONCRETE W/ SANDBLAST FINISH	A/LC-02	
(3)	DECOMPOSED GRANITE LANDSCAPE PLANTER (NEED ABATEMENT, SOIL PREPARATION AND FINE GRADE)	B/LC-02	
(4)	'BAJA CRESTA RUBBLE' FROM SOUTHWEST BOULDER AND STONE, 6"-12" SIZES (24 TOTAL)		
(5)	'PALM SPRINGS GOLD BOULDERS' FROM SOUTHWEST BOULDER AND STONE, VARIOUS SIZES RANGING FROM 3' TO 5" (24 TOTAL)	C/LC-02	
(6)	FURNISH AND INSTALL 96" CLASSIC SERIES BENCH, MODEL C-146, BRONZE POWDERCOAT COLOR WITH IPE WOOD SEAT, MANUFACTURED BY VICTOR STANLEY. ATTACH TO GROUND PER MANUFACTURER SPECIFICATIONS (10 TOTAL)		
(7)	FURNISH AND INSTALL DYNASTY SERIES LITTER RECEPTACLE MODEL DYN-SD-36, BRONZE POWDERCOAT COLOR WITH SIDE OPENING, MANUFACTURED BY VICTOR STANLEY. ATTACH TO GROUND PER MANUFACTURER SPECIFICATIONS (3 TOTAL)		
(8)	OVERHEAD SHADE STRUCTURE TO BE 'FB-1 - HYPAR WALKWAY' FROM 'POLIGON' (CUSTOM TWO-SAIL PANELS), CONTRACTOR TO CONFIRM WITH LANDSCAPE ARCHITECT AND MANUFACTURER, INSTALL PER MANUFACTURER'S RECOMMENDATION CONTACT: BRYCE LAWRENCE @ BRYCELAWRENCE@MIRACLEPLAYGROUND.COM (5 TWO-PANEL SECTIONS TOTAL)		
(9)	FURNISH AND INSTALL CYCLE SENTRY BIKE RACK, MODEL BRCS-103, BRONZE POWDERCOAT COLOR, MANUFACTURED BY VICTOR STANLEY - ATTACH TO GROUND PER MANUFACTURER SPECIFICATIONS (6 TOTAL)		

ROOT BARRIER NOTE:
 ALL TREES PLANTED WITHIN 6' OF ANY CURB, WALL, HARDSCAPE ELEMENT, BUILDING, FIRE HYDRANT, UTILITY VAULT, OR LIGHT FIXTURE SHALL RECEIVE A 10' LENGTH OF 48" DEEP ROOT BARRIER. NO ROOT BARRIER SHALL ENCIRCLE THE ROOT BALL.



<p>NOTE: WORK CONTAINED WITHIN THESE PLANS SHALL NOT COMMENCE UNTIL AN ENCROACHMENT PERMIT AND/OR A GRADING PERMIT HAS BEEN ISSUED.</p> <p>THE PRIVATE ENGINEER SIGNING THESE PLANS IS RESPONSIBLE FOR ASSURING THE ACCURACY AND ACCEPTABILITY OF THE DESIGN HEREON. IN THE EVENT OF DISCREPANCIES ARISING AFTER CITY APPROVAL OR DURING CONSTRUCTION THE PRIVATE ENGINEER SHALL BE RESPONSIBLE FOR DETERMINING AN ACCEPTABLE SOLUTION AND REVISIONS TO THE PLANS FOR APPROVAL BY THE CITY.</p>	<p>CALL BEFORE YOU DIG 800-487-2277 951-261-2200 A PUBLIC SERVICE BY UNDERGROUND SERVICE ALERT</p>	<p>BENCHMARK: CITY OF COACHELLA 1007 ELEVATION: 437.448 DATUM: NAVD83 + 500 FEET DESCRIPTION: 3 1/2" BRASS DISK SET IN TOP OF THE NORTHWEST CORNER OF A 1.5 FOOT HIGH PLANTER WALL AROUND THE "CITY OF COACHELLA" MONUMENT SIGN, LOCATED 25 FEET SOUTHEAST FROM THE BEGINNING OF THE SOUTH CURB RETURN AND 35 FEET SOUTH WEST OF SOUTHERLY CURB RETURN LOCATED AT THE INTERSECTION OF GRAPEFRUIT BLVD. (HIGHWAY 111) AND CESAR CHAVEZ (HARRISON STREET).</p> <p>BASIS OF BEARINGS: THE BASIS OF BEARINGS FOR THIS SURVEY IS THE CALIFORNIA COORDINATE SYSTEM (CCSRS), ZONE 10, AS DETERMINED LOCALLY BY THE LINE BETWEEN CONTIGUOUS OPERATING GPS BASE STATION STATIONS P491 AND P417, (EPOCH: 2017.50) COORDINATES, AS COMPUTED AND PUBLISHED BY SCORPUS ORBIT AND PERMANENT ARRAY CENTER (SOPAC) AND THE CALIFORNIA SPATIAL REFERENCE CENTER (CSRC), BEING: NORTH 78° 59' 35.48" WEST.</p>	<table border="1"> <tr> <th>DESIGNED BY</th> <th>DATE</th> <th>REVISIONS</th> <th>APPR. DATE</th> <th>SEAL-CITY</th> </tr> <tr> <td> </td> <td> </td> <td> </td> <td> </td> <td> </td> </tr> </table>	DESIGNED BY	DATE	REVISIONS	APPR. DATE	SEAL-CITY						<p>RECOMMENDED FOR APPROVAL:</p> <p>NOEL OWSELEY R.C.E. 39927 DATE: 12-31-2021</p> <p>CITY OF COACHELLA</p> <p>APPROVED FOR CONSTRUCTION:</p> <p>ANDREW R. SIMMONS R.C.E. 72988 DATE: 06-30-2022</p>			<p>PREPARED UNDER THE DIRECT SUPERVISION OF: JOHN P. PATTERSON, I.A.A. 3003</p>	<p>CITY OF COACHELLA W.O. 20210552 CITY FILE NO.</p>	<p>FOR: CITY OF COACHELLA</p>	<p>PROJECT NO. 2021-013</p>	<p>DATE: 07-21-21</p>	<p>SHEET NO. 02 OF 13 SHEETS</p>	<p>39</p>
			DESIGNED BY	DATE	REVISIONS	APPR. DATE	SEAL-CITY																

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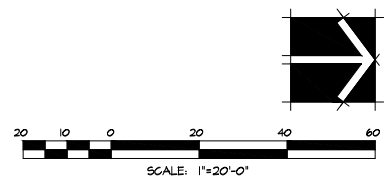
PLANT SCHEDULE					
TREES	QTY	BOTANICAL NAME	COMMON NAME	SIZE	WUCOLS
□	10	CHILOPSIS LINEARIS	DESERT WILLOW	24"BOX	VL
■	2	EBENOPSIS EBANO	TEXAS EBONY	24"BOX	L
SHRUBS	QTY	BOTANICAL NAME	COMMON NAME	SIZE	WUCOLS
⊙	20	DALEA GREGGII	TRAILING INDIGO BUSH	5 GAL	L
⊙	4	DASYLIRION WHEELERI	GREY DESERT SPOON	5 GAL	L
⊙	8	ENCELIA CALIFORNICA	CALIFORNIA ENCELIA	5 GAL	VL
⊙	7	SALVIA LEUCANTHA	MEXICAN BUSH SAGE	5 GAL	L

SEE DETAILS ON SHEET 18:
 'A' - SHRUBS
 'B' - PALMS
 'C' - PALMS

TREES PER PUEBLO VIEJO VILLAS PLANS PREPARED BY GMP

PROPOSED TREES/PALMS PER PUEBLO VIEJO SUSTAINABLE TRANSPORTATION PROJECT IMPROVEMENT PLANS PREPARED BY GMP

ROOT BARRIER NOTE:
 ALL TREES PLANTED WITHIN 8' OF ANY CURB, WALL, HARDSCAPE ELEMENT, BUILDING, FIRE HYDRANT, UTILITY VAULT, OR LIGHT FIXTURE SHALL RECEIVE A 10' LENGTH OF 48" DEEP ROOT BARRIER. NO ROOT BARRIER SHALL ENCIRCLE THE ROOT BALL.



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			NO.	DATE	DESCRIPTION										
<p>FOR: CITY OF COACHELLA</p>		<p>PROJECT: PUEBLO VIEJO SUSTAINABLE TRANSPORTATION PROJECT COACHELLA SUNLINE TRANSPORT HUB</p>		<p>LANDSCAPE PLANTING PLAN</p>		<p>DATE: 07-21-21</p>		<p>40</p>							

PROJECTS\20-066-00\LandArch\C0s\20-066 PSh.dwg 07/21/21



PHOENIX DACTYLIFERA 'MEDJOO' / DATE PALM



SHADE STRUCTURE - POLIGON 'HYPAR WALKWAY'



DALEA GREGGII / TRAILING INDIGO BUSH



DASYLIRION WHEELERI / SPOON YUCCA



ENCELIA CALIFORNICA / BRITTLEBUSH



SALVIA LEUCANTHA / MEXICAN BUSH SAGE



DECOMPOSED GRANITE



LANDSCAPE BOULDERS









Item 2.







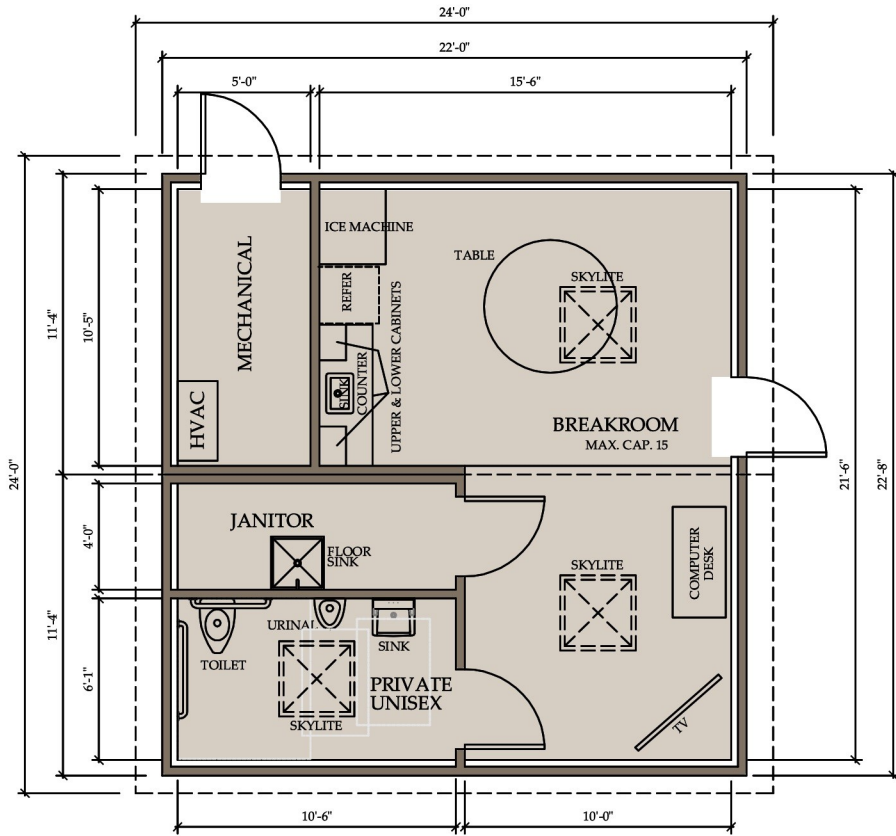
COACHELLA
TRANSIT HUB










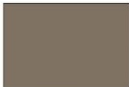






- 


 STUCCO 1:
 SHERWIN WILLIAMS
 SW 7562
 ROMAN COLUMN
- 


 STUCCO 2:
 SHERWIN WILLIAMS
 SW 7632 MODERN GRAY
- 


 STUCCO 3:
 SHERWIN WILLIAMS
 SW 9522
 Meander
- 

 STUCCO 4:
 SHERWIN WILLIAMS
 SW 0038
 LIBRARY PEWTWER
- 

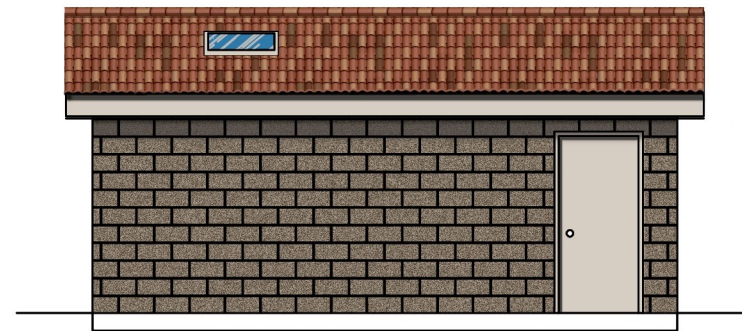
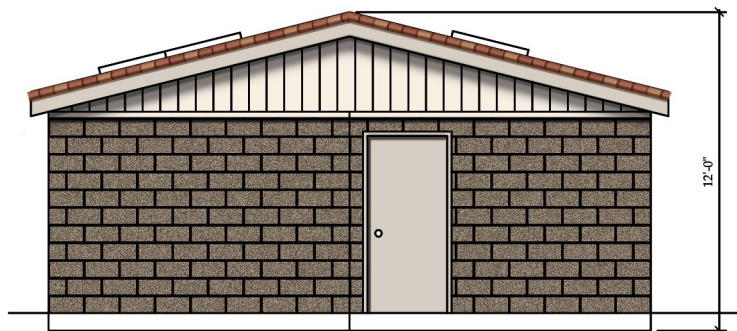
 WINDOW & DOOR TRIM:
 SHERWIN WILLIAMS
 SW 7514
 FOOTHILLS
- 

 SHUTTERS & FASCIA:
 SHERWIN WILLIAMS
 SW 2802 ROCKWOOD RED
- 

 FABRIC AWNINGS:
 MATCH SW2802
 ROCKWOOD RED
- 

 METAL RAILINGS & ACCENTS:
 BLACK
- 

 CONCRETE ROOFING:
 BORAL
 BARCELONA
 TERRA COTTA FLASHED



COACHELLA TRANSIT CENTER, PLAN 'E'

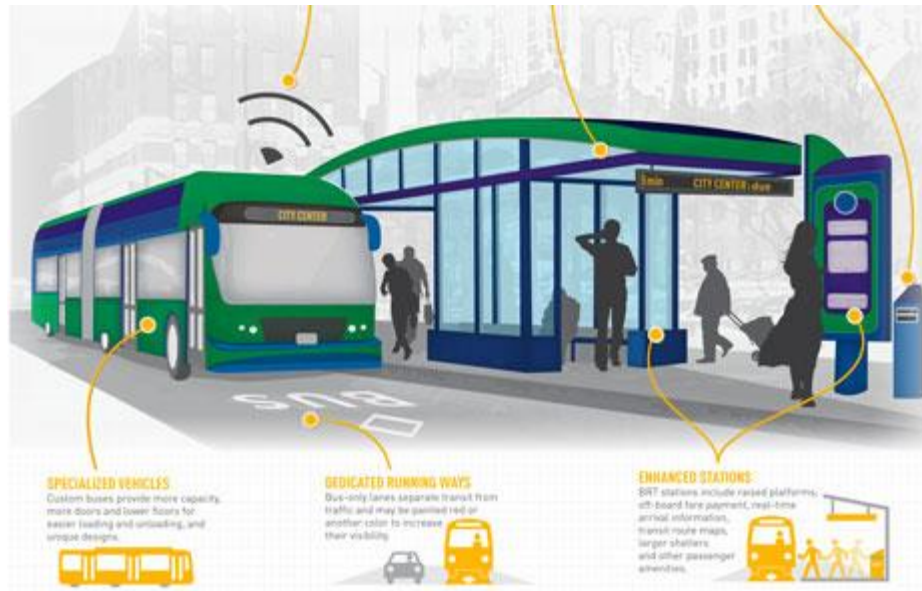












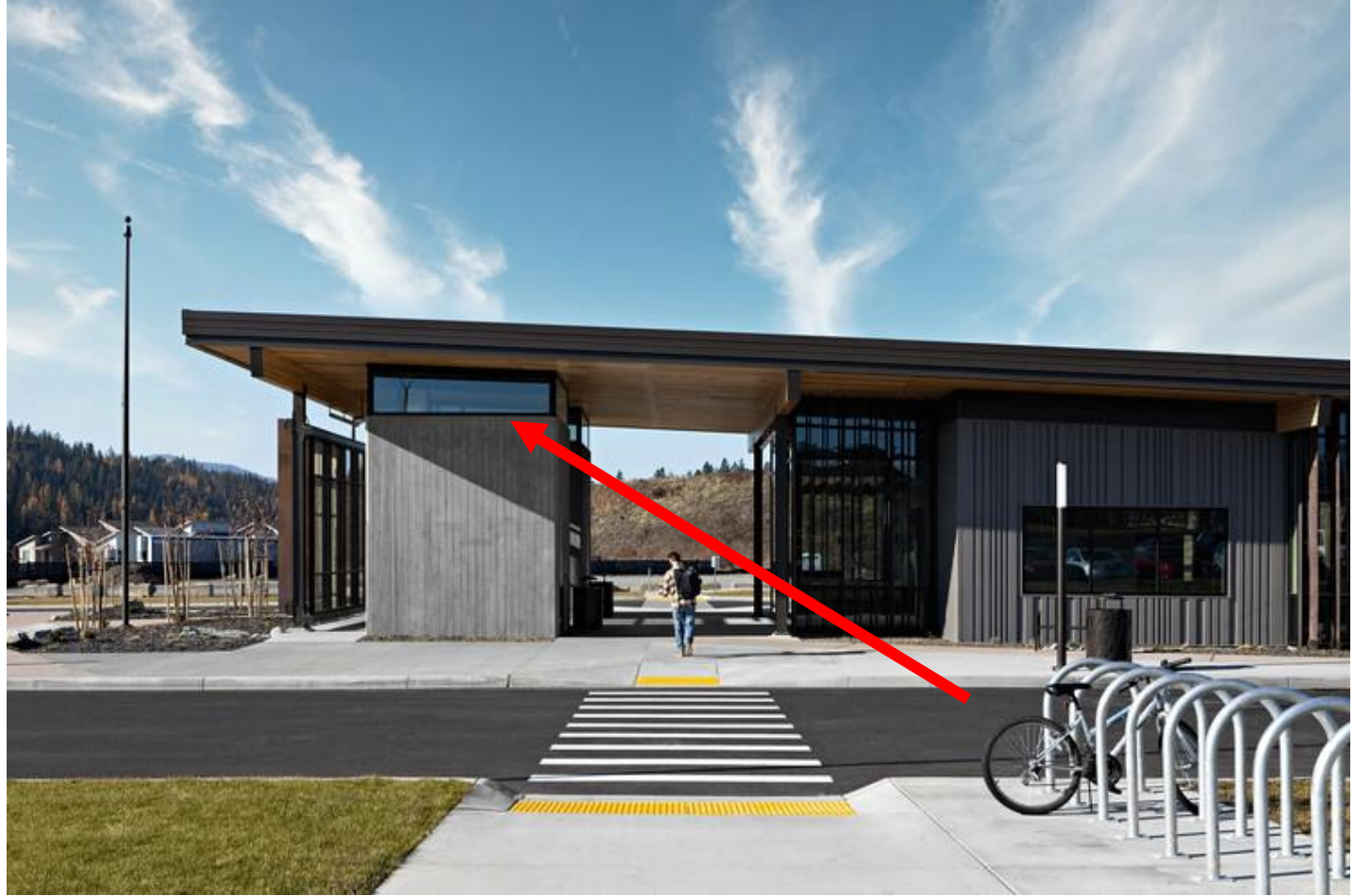




hdesigngroup

City Utilities of Springfield
Downtown City Bus Transfer Station

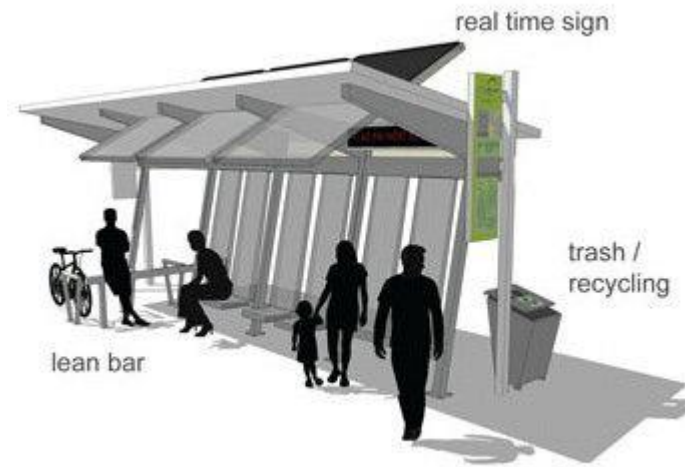














Restroom at Veteran's Park with smooth stucco, tile accents, exposed rafter tails, grates and rounded doors.



A side wall with no windows or doors utilizes tile accents and decorative pop outs to enhance the appearance.



Rear of the Veteran's Park restroom buildings.



Decorative Wrought Iron fencing incorporated at Veteran's Park.



Larger Size pebbles uses for landscaped areas at Veteran's Park



Example of an Ancillary Municipal Snack building with the slaking of the stucco at the edge of the tile roof.

Existing Conditions Photos of Sunline Transit Hub site

Photo looking North from Pueblo Viejo Villas



Photo at the crosswalk at Cesar Chavez and Mario Lazcano Drive



Photo looking south toward site and Pueblo Viejo Villas



Photo looking south from site along Cesar Chavez Street showing existing sidewalk



Photo of 4th Street and Mario Lazcano Drive



Pueblo Viejo District

Design Guidelines

City of Coachella



TABLE OF CONTENTS

1 INTRODUCTION.....1-81

1.1 LOCATION1-81

1.2 PURPOSE AND INTENT1-82

1.3 HISTORY AND HERITAGE1-83

1.4 DOCUMENT REVIEW1-84

1.5 ORGANIZATION AND USE1-85

2 CHARACTER AND DESIGN PRINCIPLES2-87

2.1 DEFINED SUBAREA CHARACTER2-87

2.2 COMMUNITY CHARACTER PRINCIPLES2-90

3 PRIVATE REALM DESIGN GUIDELINES.....3-93

3.1 INTERPRETATION AND APPLICATION3-93

3.2 HOW TO USE GUIDELINES DOCUMENT3-93

3.3 ALLOWED AND ENCOURAGED ARCHITECTURAL STYLES3-95

3.4 PROXIMITY OF SIMILAR STYLES3-107

3.5 GENERAL PROVISIONS3-108

3.6 SIXTH STREET MIXED USE SUBAREA3-122

3.7 GRAPEFRUIT BOULEVARD SUBAREA.....3-136

3.8 CESAR CHAVEZ STREET SUBAREA3-143

3.9 TRANSITION AREA.....3-150

3.10 SIGNS AND PUBLIC ART.....3-156

4 PUBLIC REALM DESIGN GUIDELINES.....4-165

4.1 STREETScape CONCEPTS4-165

4.2 GENERAL GUIDELINES4-171

4.3 TRAFFIC CALMING FEATURES4-183

4.4 LANDSCAPING GUIDELINES AND PLANTING PALETTE4-190

1 INTRODUCTION

1

1.1 LOCATION

The Pueblo Viejo District serves as the heart of the City of Coachella. The triangular district is bounded by Cesar Chavez Street (formerly Harrison Street) on the west, Grapefruit Boulevard on the east, and Bagdad Avenue on the south. The area houses a variety of businesses, institutional buildings such as the City Hall, the Post Office, the new Library, recreational facilities such as Veterans' Park and numerous other uses. The area is also poised for additional growth with a County facility to be located at 6th Street and Cesar Chavez Street as well as interest in additional development at 6th Street and Cesar Chavez Street.

With Grapefruit Boulevard bordering the plan area, the Pueblo Viejo District is connected to the wider region and has the potential for attracting additional retail and retail customers through tourism, additional housing opportunities and maintaining and growing this unique place and heart of the community. As part of the implementation of the Pueblo Viejo Revitalization Plan, these Design Guidelines serve as a tool to maintaining and growing the sense of place that defines what the Coachella community loves about the Pueblo Viejo and how the community can attain its vision for a more vibrant and active district.

1.2 PURPOSE AND INTENT

The Pueblo Viejo Revitalization Plan, created in 2010, established a vision for the area following extensive public engagement and consultation with stakeholders and City officials. This plan established the following vision:

“Pueblo Viejo is the civic and cultural heart of Coachella. The community is proud of the historic charm, locally-owned businesses, and vibrant civic center. As you enter through the attractive gateways on Sixth Street, you are immersed in a lively street scene offering shady walkways, cooling water fountains, outdoor dining, and unique shopping. Once empty lots are now filled with mixed-use buildings that respect the heritage, climate, and community values. Family-friendly events and festivals fill the streets and public spaces. As you relax in the clean, well maintained civic center core, you know . . . you have arrived in Pueblo Viejo!”

The plan included tools for implementation of this vision including conceptual design guidelines. The City has already started implementing the plan with the revamping of the Veterans’ Park area and the streetscape along three blocks of Sixth Street and around Veterans’ Park and the construction of the new Library.

Pueblo Viejo Vision Diagram. SOURCE: Michael Baker International



As part of the implementation, these Design Guidelines were adopted to create a cohesive look and feel and maintain and enhance the sense of place that is the Pueblo Viejo. More specifically these Design Guidelines:

- Provide additional guidance on private realm architecture and site design to enhance the physical, visual, and functional compatibility between uses;
- Provide guidance on the development of the public realm, including streetscapes and public infrastructure; and
- Ensure that new public and private development in the District is compatible, honors the history of the City of Coachella, and contributes to the unique character of Pueblo Viejo District.

These Design Guidelines are intended to provide design professionals, property owners, residents, and the City a clear understanding of the City of Coachella’s expectations for the planning and design of properties in Pueblo Viejo. The Design Guidelines are supplemental to the City’s Municipal Code (including Zoning Ordinance and Sign Ordinance), General Plan and the Pueblo Viejo Revitalization Plan. These Design Guidelines replace the citywide Design Guidelines for the defined Pueblo Viejo District as outlined in Section 1.1.

1.3 HISTORY AND HERITAGE

Coachella’s early beginnings date back to 1898 when a wood terminal was established on a Southern Pacific Railroad siding. This terminal transported mesquite wood to Los Angeles. Many Mexican American railroad track workers, also known as Traqueros, came to the area to work on this siding and the extension of the Southern Pacific Railroad to the Coachella Valley.

Development of the city did not truly begin until the Rector Brothers, Jason L. Rector and Lon B. Rector, dug a well to access artesian water in November 1900. This well is now covered by the intersection of Grapefruit Avenue and Fifth Street. The first dwelling in the city was an adobe structure owned by Mr. Rector. Many businesses were established by Mr. Rector, including the Coachella Valley Produce Association and the Coachella Valley Refrigerating Company. These businesses served as the basis for growth and future industrial development in the city.

Due in part to the establishment of the Coachella Valley Produce Association, the city has a very strong agrarian history, and for many years, it was the closest place for mostly Mexican-American farmworkers to receive services. In an effort to secure fair wages for these farmworkers, Cesar Chavez, Dolores Huerta, and Gilbert Padilla founded Farmworkers United in 1962 in the Coachella Valley. Cesar Chavez led strikes and boycotts in the 1960s. He is highly regarded throughout the

The history and heritage of the City of Coachella is extremely important to the city’s residents. The Design Guidelines help create a unique place and combat the architecture of nowhere by striving to protect this history and heritage. The Design Guidelines encourage development that is sensitive to both, the existing and historically significant buildings in the District while allowing for innovation and growth that maintains the eclectic and unique sense of place that is the Pueblo Viejo District.

city. In 2018, Harrison Street at the eastern boundary of Pueblo Viejo District was renamed Cesar Chavez Street.

1.4 DOCUMENT REVIEW

1.4.1 2010 Pueblo Viejo Revitalization Plan

The Pueblo Viejo Revitalization Plan established the vision for the redevelopment of the revitalized District. The plan analyzed the current citywide Design Guidelines for the area and called for amending those guidelines in its implementation program. These Design Guidelines provide clear guidance as to how new development and existing development modifications should be designed to be compliant with the Vision for the Pueblo Viejo.

1.4.2 Coachella General Plan

These Design Guidelines help to implement the General Plan. More specifically this includes:

- Implementation of the General Plan’s Guiding Principles
- Land Use and Community Character
 - Downtown Center
 - Public Designations including Mini Parks and Plazas/Greens
 - Public Facilities and Buildings
 - Building Types
 - Subarea 2 – Downtown policies
- Mobility
 - Goal 1. Complete Streets
 - Goal 2. Traffic Calming
 - Goal 3. Pedestrian Network
 - Goal 4. Bicycle Trail Network
 - Goal 5. Transit Supportive Development Patterns
- Community Health and Wellness
 - Policy 2.3 – Housing diversity
 - Policy 8.24 – Public Plazas
- Sustainability and Natural Environment
 - Policy 1.6 – Climate-appropriate building types
 - Policy 1.10 – Adaptation strategy
 - Policy 1.11 – Urban forest
 - Policy 1.14 – Designing for changing precipitation patterns
 - Policy 2.2 – Passive solar design
 - Policy 2.3 – Alternative energy
 - Policy 2.8 – Renewable energy-open space areas
 - Policy 2.9 – Energy-efficient street lighting

- Policy 3.4 – Low impact development
 - Policy 3.7 – Landscape design
 - Policy 3.8 – Groundwater Infiltration
 - Policy 4.6 – Public realm shading
 - Policy 6.5 – Dark sky
- Infrastructure and Public Services
 - Policy 1.7 – Infill
 - Policy 1.9 – Land use compatibility
- Noise
 - Policy 3.2 – Traffic Calming
- Housing
 - Policy 1.5 – Housing Downtown
 - Policy 1.8 – Innovative Construction

1.4.3 Coachella Zoning Ordinance

These design guidelines provide additional guidance for the properties within the Pueblo Viejo District. The guidelines are not intended to replace or supersede the design standards included in the City’s Municipal Code, Title 17, Zoning. These guidelines are intended to enhance those items required by the City’s Zoning Ordinance and provide additional guidance for architectural review, as required in the zoning districts within Pueblo Viejo District. Where conflicts between the Municipal Code and these Design Guidelines occur, the Municipal Code shall prevail. However, the planning Director and Planning Commission will use these Guidelines as a policy document in conditioning the approval of new development projects or substantive remodel projects.

1.5 ORGANIZATION AND USE

This document is organized into five sections:

1. Introduction
2. Design Principles
3. Private Realm Design Guidelines
4. Public Realm Design Guidelines
5. Definitions

The **Introduction** orients the reader to the design guidelines document. The section opens with a description of the Pueblo Viejo District and introduces the purpose and intent of the Design Guidelines. It also provides a brief history of Coachella so that the reader understands the District’s setting and its importance in relation to the larger city. The Introduction summarizes related documents such as the City’s General Plan, 2010 Pueblo Viejo Revitalization Plan, and Zoning Ordinance that also form part of the policy framework for the Pueblo Viejo.

The **Design Principles** section paints a picture of the overall character that is to be achieved in various subareas of the Pueblo Viejo District rather than citing details. It sets forth the overarching principles and/or best practices that are to be followed when considering the Design Guidelines. The section also includes case studies of recent developments in the Pueblo Viejo District.

The **Private Realm Design Guidelines** section details the guidelines that are to be followed when developing private property. The section begins by describing the Pueblo Viejo's historic background and discusses appropriate architectural styles for the commercial areas of the district based on history, cultural influences, changing demographics, and public engagement feedback. It also includes detailed guidelines for various elements based on the styles of architecture that are allowed in the Pueblo Viejo's three commercial subareas. The section also includes suggestions and encouragement for public art, plazas, paseos and gathering spaces.

The **Public Realm Design Guidelines** section provides suggestions for development of public streets, alleys, and sidewalk areas. The section provides suggested typical cross sections and facilities for various streets. While detailed plans are to be created for streetscape concepts for each commercial street, the guidelines include best practices to incorporate in public infrastructure design or expectations the City has for private development of spaces to be included in the public realm. Residential streetscapes are not suggested; rather, traffic calming strategies are laid out for these streets. The section also provides guidance on landscape improvements and includes a planting palette for commercial areas.

The **Definitions** section explains various planning terms that have been used in the document. The words are arranged in alphabetical order for ease of use by the reader. Care has been taken to use simple terms in the document and to avoid planning jargon as much as possible.

2 CHARACTER AND DESIGN PRINCIPLES

2

2.1 DEFINED SUBAREA CHARACTER

The Pueblo Viejo District is divided into six distinct subareas. The focus of the Design Guidelines is on those subareas that may experience in-fill development, commercial and/or mixed-use development, such as the Sixth Street Subarea, Grapefruit Boulevard, Cesar Chavez Street, and the Transition Area surrounding Veterans' Park. The Design Guidelines establish the character, architectural style, massing, materials, and colors for architecture in each of these subareas.

While most of the Pueblo Viejo District consists of low-density residential uses and will remain so, the vision calls for more mixed-use development throughout the core of the Pueblo Viejo. However, the guidelines do not suggest the nature and character of development that might occur in the residential areas except as envisioned in General Plan; rather, they focus on the public realm aspect in these areas and do not provide Design Guidelines for residential uses in these areas.

Below is a brief summary of the subareas, as shown on Map A.

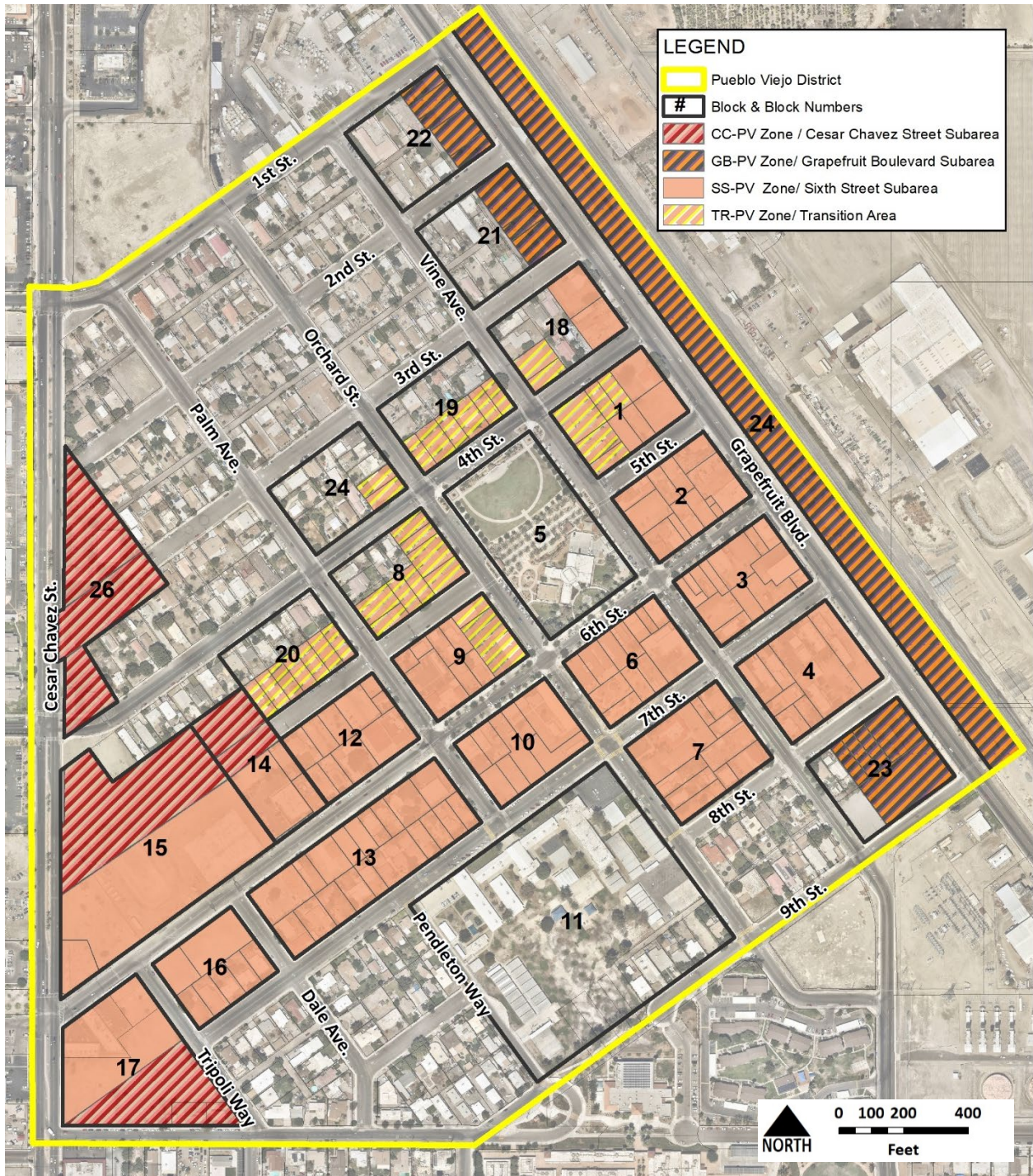
2.1.1 Sixth Street Subarea

The Sixth Street Subarea comprises the area along Fifth, Sixth, and Seventh Streets and includes the Post Office as well as the area surrounding intersections with Grapefruit Boulevard and Cesar Chavez Street. The area is envisioned as a walkable "main street" environment with higher densities, multistory structures with a mix of uses both horizontally and vertically. However, the density should not be overwhelming for a pedestrian, and building architecture should focus on the human scale by creating a superior pedestrian environment.

Sixth Street Subarea Views. SOURCE: Michael Baker International



Map A Subarea Limits Map. SOURCE: Michael Baker International



This is achieved by incorporating significant details in the buildings' architecture, especially on the ground floor. Businesses should have good pedestrian scale frontages to provide a rewarding visual experience for the people passing by. Retail stores should incorporate displays that trigger the interest of pedestrians. The two newly built Library building and the County office building under construction at the time of this report) are good examples of architecture and details that will be suitable for Sixth Street Subarea.

Fifth and Seventh Streets are expected to enjoy a spillover effect from Sixth Street and will follow similar development patterns. However, the predominant shaded walkway and wider sidewalks along Sixth Street will create a different look and feel for the space between the streets. The intersections with Grapefruit Boulevard and Cesar Chavez Street are also part of the Sixth Street Subarea. These are entryways to the Sixth Street main street; thus, greater detail in both the private and public realms is advocated in the guidelines to create an inviting environment.

2.1.2 Grapefruit Boulevard

The Grapefruit Boulevard Subarea is envisioned to be automobile focused but also a walkable and bikeable environments. The corridor caters to freestanding structures with their own parking lots and used for commercial, light industrial, and automobile-oriented uses. Multi-family structures may be integrated in the horizontal mix of the area.

The Design Guidelines focus on the cosmetic improvement of existing building façades and enhancement of the streetscape to accommodate pedestrians and bicyclists. New development and modifications to existing development should experiment with architectural styles to create auto-oriented yet attractive buildings.

2.1.3 Cesar Chavez Street Subarea

Cesar Chavez Street is a commercial street with mainly auto-oriented franchise stores. The Design Guidelines for this area are similar to those for Grapefruit Boulevard and focus on the improvement of building façades and the implementation of complete and walkable streets. While the uses may differ, the style of architecture, density, height, and general ambiance/placemaking elements will follow the "main street" characteristics described above for Sixth Street as the district transitions towards 6th Street and away from the existing built retail in the subarea.

*Library Building along Sixth Street.
SOURCE: Michael Baker International*



*Sixth Street Subarea Views. SOURCE:
Michael Baker International*



*Grapefruit Boulevard Subarea Views.
SOURCE: Michael Baker International*



2.1.4 Transition Area

The Transition subarea refers to the parcels surrounding Veterans’ Park. Because the area is envisioned to serve as a transition between residential and commercial use utilizing the existing residential structures and maintaining similar density and massing. New development and modification of existing uses that fits a transition between residential and commercial scale should be encouraged.

Palm View Elementary subarea views



2.1.5 Palm View Elementary School

The block in which the school is located features institutional uses. Built in 1928, Palm View Elementary is the oldest school in the District and has been plagued for years by obsolescence due to the building age and materials originally used. The current school building features Spanish Revival style architecture with an arched opening at the entrance. It is proposed to be demolished and a new school is to be built on the existing property. The historic nature of the school will be taken into account and will be incorporated into the new design. Since the development is underway, and approved by the City, Design Guidelines are not provided for this subarea.

Civic Center and Veterans’ Park Subarea Views. SOURCE: Michael Baker International



2.1.6 Civic Center and Veterans’ Park

The 6th Street streetscape and Veterans’ Park projects provide a respite from the surrounding buildings. The wide sidewalks in this subarea can incorporate public art that complements the style of these developments. Since the development has recently taken place and is not expected to change in near future, the Design Guidelines are not provided for this subarea.

2.2 COMMUNITY CHARACTER PRINCIPLES

The physical city comprises both the public and the private realms. The public realm includes streets, sidewalks, and areas between the street and the sidewalk, as well as civic buildings, public plazas, parks, and greenways. Private property, including public gathering spaces not owned and operated as part of the public rights-of-way, are considered the private realm. Community character principles are the overarching principles that will define the character of the subareas. These principles are the basis for the specific guidelines pertinent to both the public and private realm that make up a neighborhood character.

2.2.1 Development Principles

- Promote dense and mixed-use development
- Promote development that is mixed both horizontally and vertically
- Promote development that is consistent to current market demand and flexible to accommodate future demand
- Promote reuse of existing historic buildings
- Promote context-sensitive infill projects and site planning
- Promote live building edges and articulated building façades emphasizing human-scale design
- Apply Crime Prevention through Environmental Design principles (CPTED) to development projects

Mixed Use Development Example. SOURCE: Brett VA



Placemaking Element Example. SOURCE: Sixflashphoto



2.2.2 Placemaking Principles

- Provide public places that are memorable and interesting
- Promote placemaking elements that pay tribute to history and tell a story of the City of Coachella
- Design public spaces to accommodate activity

2.2.3 Circulation, Mobility, and Connectivity Principles

- Integrate land use and transportation
- Utilize Complete Streets principles to create a multimodal environment
- Enhance connectivity to transit
- Create a comfortable pedestrian environment
- Create a safe environment for pedestrians and bicyclists
- Incorporate environmental design standards and green development techniques in street design
- Provide an adequate amount of on-street parking while maintaining block integrity.

Complete Streets Cater to All Users and Promote Activity. SOURCE: PeopleForBikes Foundation



3 PRIVATE REALM DESIGN GUIDELINES

3

3.1 INTERPRETATION AND APPLICATION

The guidelines are general and may be interpreted with some flexibility in their application to specific projects. The guidelines will be used during the City's design review process to ensure new development implements General Plan goals and objectives and becomes a compatible part of the total community environment.

These guidelines shall apply to all new development within the Pueblo Viejo District. They shall also be used to review existing development for any reuse, additions, remodel, reconfiguration or other construction requiring a building permit. In the review of modifications or additions to existing development, the provisions of the guidelines will be imposed to the extent that they are applicable and practical to impose in the situation.

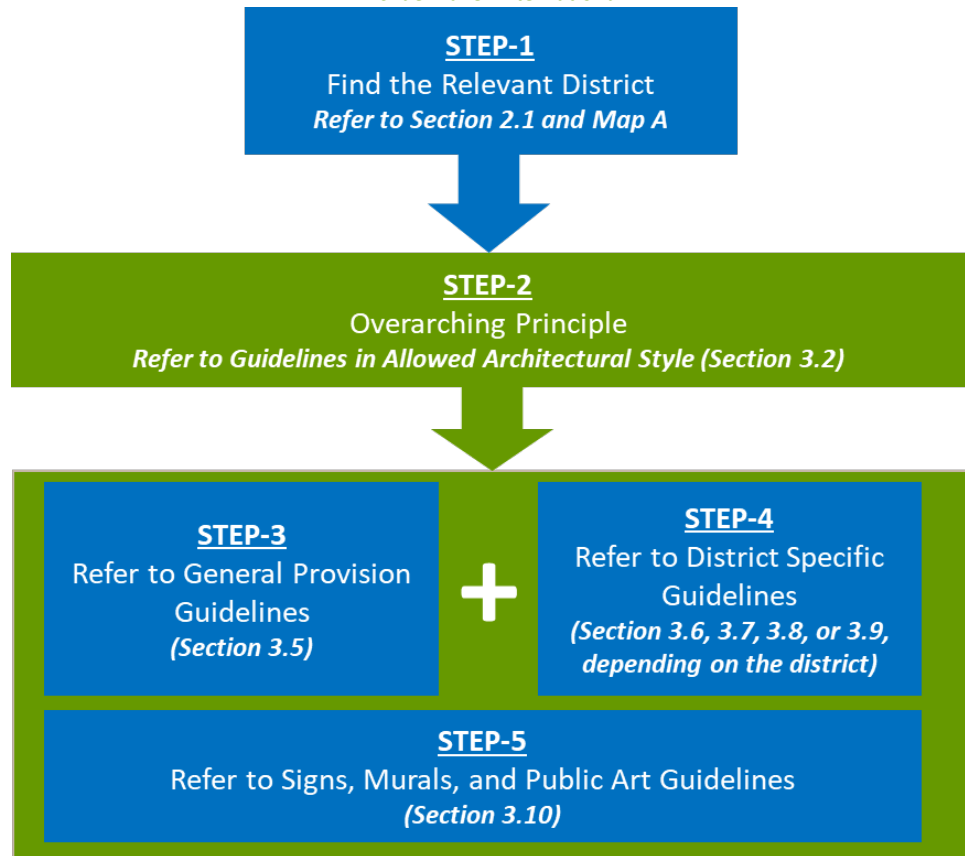
3.2 HOW TO USE GUIDELINES DOCUMENT

The Private Realm Guidelines correspond to development occurring on private property. The City's Zoning Code should always be consulted as the first step of any development project. The guidelines are organized in various sections and the diagram below shows the process of referring to various sections of the guidelines.

- **Step 1:** First step in the usage of the guidelines is to find the relevant district of the project site based on Map A provided in Section 2.1. The section also provides an overall philosophy of the district.

- **Step 2: Overarching Principle:** Finalize the architectural style for the project and refer to Section 3.3 *Allowed and Encouraged Architectural Styles*. These guidelines are referred to in the general provisions as well as subarea guidelines.
- **Step 3:** The applicant should refer to the Section 3.5 *General Provisions*. This section applies to all subareas.
- **Step 4:** Refer to either Section 3.6, 3.7, 3.8 or 3.9 depending on the subareas that the project site is located. These sections give additional guidelines specific to the subarea in addition to general provisions.
- **Step 5:** The Section 3.10 *Signs, Murals, and Public Art*, should be consulted for all projects irrespective of the subarea. The section provides guidelines for each type of signage that is allowed in Pueblo Viejo District. Table A in this section provides a list of various signs and the suggested subareas in which they may be used. The Mural and Public Art guidelines are applicable to all subareas.

Guideline Document Usage Diagram. SOURCE: Michael Baker International



3.3 ALLOWED AND ENCOURAGED ARCHITECTURAL STYLES

A mix of architectural styles and details can create an authentic and timeless downtown. The allowed and encouraged architectural styles will support the Pueblo Viejo’s historic charm, locally owned businesses, and vibrant civic center. A vertical and horizontal mix of uses would set Coachella apart from other cities in the Coachella Valley. In addition, residents prefer a mix of architectural styles that reflect the city’s past and preserve its eclectic style and sense of place to further set the Pueblo Viejo District apart from other areas in the Coachella Valley.

Architectural styles incorporating elements that are responsive to the environmental needs of the desert, including sun-shading, are strongly encouraged. Features such as arcades and colonnades will create livable outdoor spaces that will encourage a vibrant street scene. The architecture of Pueblo Viejo District should support the vision of the district as a vibrant downtown with opportunities for fun, dining, conveniences, and livable residential communities that are close to and integrated with these uses and account for variable economic pressures of changing market conditions.

Pueblo Viejo District Architecture

The existing Pueblo Viejo District architecture in Coachella has been influenced by various architectural styles. A review of historic images captures structures that include elements of the following architectural styles: Classic Revival, Neo-Classical, Western False Front, Spanish Colonial Revival, International Style, and Art Deco.

More recent development is postmodern or international architectural styles. These styles create a platform for experimentation and playfulness often abstractly incorporating historic elements that make a place unique in one respect. It helps break the monotony of traditional styles and enables the creation of unique and iconic structures.

Classic Revival Brick Style



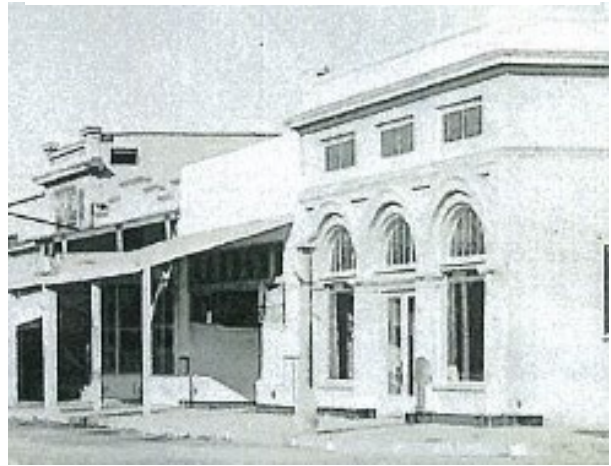
Classic Old West Architecture (Use of Colonnade)



Spanish Colonial Revival



Neoclassical Style



Vernacular Architecture (Use of Shaded Structures) International Style influenced by Mission Revival Style



Historic Images Source: Historic Society of Coachella

Allowed Architectural Styles

To preserve the architectural heritage of the Pueblo Viejo District, but also encourage its eclectic nature, four primary architectural styles are explicitly allowed.

- Spanish Colonial Revival
- Mission Revival
- Neoclassical
- Postmodern
- Architectural Innovation*

**In addition, applicants can depart from these styles with bold new contemporary design, but the design should be in context with the surrounding uses and pay homage to the heritage of the Pueblo Viejo District, the City of Coachella and the Coachella Valley. The detailed guidelines pertaining to each style and applicable to the Pueblo Viejo District are explained in the following sections.*

3.3.1 Spanish Colonial Revival Architecture

The Spanish Colonial Revival style revisits the traditional Spanish architectural themes seen in Spain’s early western colonial settlements. Within the U.S., this style gained national exposure at the 1915 Panama-California Exposition in San Diego. Many of the buildings designed and built for the exposition were in this style. The style is embraced principally in California and Florida and is characterized by a combination of detail from several eras of Spanish and Mexican architecture.

In the Pueblo Viejo District, architecture in the Spanish Colonial Revival style can be found along Sixth Street. Elements of this style that can be identified in Pueblo Viejo District are porch arcades with columns, round arches at entryways, and stucco exterior walls. The Coachella Public Library is designed in this style and bookends the Civic Center subarea.

This style was strongly preferred by stakeholders throughout the development of the 2010 Pueblo Viejo Revitalization Plan and these guidelines. The style blends Coachella’s Mexican heritage with a slightly modern twist and an elegance desired for Pueblo Viejo. Arcades and other attached shade structures are common features of this style and are appropriate to the local desert climate.

3.3.1.1 Overall Building Design

- Spanish Colonial Revival buildings are typically rectangular or L-shaped with horizontal massing and often include interior or exterior courtyards.

Courtyard. SOURCE: the 216



3.3.1.2 Walls

- Smooth or sand-float finish concrete stucco with tastefully placed adornments.

Smooth Stucco Walls. SOURCE: Camaclark



3.3.1.3 Roofs

- Slate or concrete shingles or half-cylindrical/ Spanish (S-shaped) modern concrete tiles should be used for durability.
- Lightly sloped or flat roofs are common to this style and are hidden behind domes or other false parapets.
- Shaped dormers/parapets.

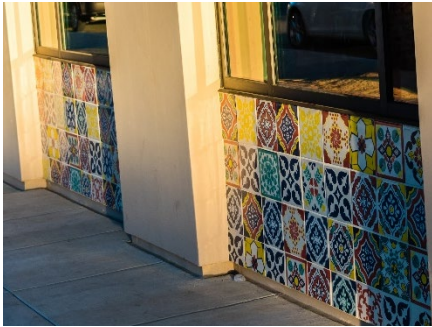
3.3.1.4 Articulation and Decorative Elements

- Patterned cutouts in smooth stucco used to decorate walls, chimneys, and vents.
- Terra-cotta or cast concrete ornaments.

Tower, Arched entry and Windows.



Decorative Tiles SOURCE: Michael Baker International



- Decorative tiles used as a wall surface or as ornament.
- Circular or square columns can be used as vertical elements.
- Rafters and supports may be exposed on the building's exterior.
- Decorative wrought iron and wood brackets or railings.
- Towers are used as decorative elements and are encouraged. Generally round, square, or polygonal towers are used to mark entrances and stairwells.

3.3.1.5 Windows and Doors

- Deeply inset arched or rectangular windows can occur on any side of the building.
- Double-hung and French or wood casement windows are common to this style. Such windows have grilles and usually divide the upper sash into individual panes that are square or close to square, giving the appearance of divided-lite windows.
- Arched entry doorways that are oversized and imposing are a common element of this style.
- French doors are usually used to provide access to porches and verandas, creating a visual connection between the interior and exterior. These are usually made to look like the building's windows.
- The framing for doors and windows is preferred to be wood, though decorative powder-coated metal frames can be used.
- The window, grilles, and doors are typically earth tones such as dark browns, terra-cotta reds, and light greens.
- Windows should be clear glass. Decorative glass is also appropriate in some cases.

3.3.1.6 Porches and Arcades

- Long exterior arcades and galleries that wrap the building are common to this style. Semi-circular arches usually spring from square pillars to form outdoor corridors.
- Small balconies with transparent railings, typically wrought iron.
- Projecting rectangular porticos, porches, or outdoor rooms created with arched openings are prevalent, especially in residential architecture.

Square Pillars for Arcade. SOURCE: Ken Lund



3.3.1.7 Columns and Pillars

- Large square pillars are commonly used with arcades/ exterior corridors of the buildings.

3.3.1.8 Materials and Colors

- The suggested exterior material is smooth, or sand-float finish concrete stucco with a minimum thickness of 7/8 inch. Accent materials can include, but should not be limited to:
 - Natural stone
 - Wood
 - Wrought iron
 - Textured or glazed concrete masonry units
 - High-quality, pre-stressed concrete systems
 - Structural or Corten steel (if a drip area is provided)
 - Hand-painted or natural tile (decorative and otherwise)
- The preferred colors for this architectural style are earthy tone colors used as base color of the buildings. Use of different colors belonging to same family of earth colors is encouraged. Earthy tone colors come from natural things around us: brown soil, green leaf, cloudy sky, as well as the red and yellow sun. These palettes can create a warm, nature-friendly atmosphere.
- Accent colors such as light blues and greens and vibrant blues, greens, reds, and yellows are encouraged. These colors can often be bold or vivid and are used sparingly, to emphasize, contrast or create rhythm and are encouraged.
- Doors usually have a dark wood finish.

Earthy Tone Finishes. SOURCE: Alan



3.3.1.9 Lighting

- Spanish Colonial Revival–style outdoor lighting fixtures with the features listed below include:
 - Wrought iron lighting fixtures, including lanterns of many different shapes
 - Clear glass shades to accent candle-shaped bulbs or amber shaded glass
 - Grandly ornamented fixtures with scrolled metal accents
 - Fixtures are either mounted to the wall or hung using chains

3.3.2 Mission Revival Architecture

Mission Revival architecture in California was inspired by the original Spanish missions. These missions were established in the late eighteenth and early nineteenth century. Mission Revival gained popularity with the train depots of the Santa Fe and Southern Pacific rail companies. By the late nineteenth century, the style began influencing the architecture of residential, commercial, and other institutional buildings. Defining characteristics include roof parapets, simple stucco or plaster siding and

Mission Revival Architecture. SOURCE: Lordkinbote



exposed beams. Below are the suggested guidelines for buildings designed in the Mission Revival architectural style.

3.3.2.1 Overall Building Design

- Enclosed courtyards are common to Mission Revival architecture.

3.3.2.2 Walls

- Unadorned smooth or light-float stucco walls
- Reinforced masonry walls mimicking adobe mud brick

3.3.2.3 Roofs

- Half-cylindrical concrete tiles are common typically of terra cotta or earth tone.
- Low-pitched hipped roofs/ shallow roof lines with wide overhanging eaves and exposed rafters are preferred for the roof structure.
- Shaped dormers/parapets are often used.
- Bell gables can be incorporated as a façade element, along with shaped dormers.

Shaped Dormers. SOURCE: Chris English



3.3.2.4 Articulation and Decorative Elements

- Building designs are usually accompanied with bell gables and rose windows.
- Restrained decorative elements of tile, iron, and wood create articulation on the façade are used, but not to the extent of Spanish Colonial Revival.
- Circular, twisted, or square columns can be used as vertical elements and help break the monotony of the façade.
- While demarcation of floors is preferred, it is not necessary.

Rectangular and Arched Windows, Double Hung Windows and Framing. SOURCE: Jeffrey Beall



3.3.2.5 Windows and Doors

- Deeply inset arched or rectangular windows can occur on any side of the building.
- Double-hung and French or wood casement windows are common to this style. Such windows have grilles and usually divide the upper sash into individual panes that are square or close to square, giving the appearance of divided-lite windows.
- Round or quatrefoil windows are integrated with the building design and create decorative elements on the façade and are encouraged.

- Arched entry doorways are a common element of this style and are encouraged.
- French doors are usually used to provide access to porches and verandas, creating a visual connection between the interior and exterior. These are usually constructed of carved wood and appear heavy. Such doors are encouraged.
- The framing for doors and windows is preferred to be wood, though decorative powder-coated metal frames can be used.
- The windows, grilles, and doors should be of earth tone but can be in a contrasting color to the wall color.
- Simple articulation is preferred in the frames in order to create play of light and shadow.
- Windows should be clear glass. Decorative glass is also appropriate in some cases.

Quatrefoil Windows. SOURCE: Title Insurance and Trust Company



3.3.2.6 *Porches and Arcades*

- Long exterior arcades and galleries that wrap the buildings are common to this style and are encouraged. Arches usually spring from square pillars to form outdoor corridors.
- Projecting porticos, porches, or outdoor rooms created with arched openings are prevalent to this style and are encouraged, especially in residential architecture.

Porches, Pillars. SOURCE: Wolfgang Beckers and Peng



3.3.2.7 *Columns and Pillars*

- Twisted or circular columns are used as decorative elements. These also appear to frame windows in some buildings.
- Large square pillars are commonly used with arcades/ exterior corridors of the buildings.

Arcade. SOURCE: Pretzelpaws



3.3.2.8 *Materials and Colors*

- The suggested exterior material is concrete stucco with a minimum thickness of 7/8 inch. Accent materials can include, but should not be limited to:
 - Natural stone
 - Reinforced brick
 - Textured or glazed concrete masonry units
 - High-quality, pre-stressed concrete systems
 - Other high-quality metals
- The preferred colors for this architectural style are earthy tone colors used as base color of the buildings. Use of different colors belonging to same family of earth colors is encouraged. Earthy tone colors come from natural things around us: brown soil, green leaf, cloudy sky, as well as the red and yellow sun. These palettes can create a warm, nature-friendly atmosphere.

- Accent colors such as light blues and greens and vibrant blues, greens, reds, and yellows are encouraged. These colors can often be bold or vivid and are used sparingly, to emphasize, contrast or create rhythm.

3.3.2.9 Lighting

- Mission Revival–style lighting outdoor fixtures with features listed below:
 - Wrought iron or hammered copper
 - Opaque shades, especially made of amber glass that covers the bare bulbs
 - Use of chains for hanging fixtures is prevalent

3.3.3 Neoclassical Architecture

Neoclassical architecture originated in the eighteenth and early nineteenth centuries. The movement used Greek and Roman details to create structures that are characterized by grandeur of scale through the use of simple geometric forms and columns, as well as blank walls.

Some historic buildings in Coachella bear elements of neoclassical architectural style. However, unlike historic neoclassical buildings seen in other parts of the world that are highly ornate in nature, these buildings are a utilitarian version of the same. The shape and form of buildings, use of columns and other features suggest neoclassical style. Such architectural style with lesser ornamentation compared to traditional neoclassical buildings complements the other styles such as Mission Revival and Spanish Colonial Revival. Hence, this style is suitable for the Pueblo Viejo District.

Rhythm, Order, Geometry and Grandeur in Simplified Form. It is important to note that the overall building height of this example is not appropriate to the Pueblo Viejo.
 SOURCE: Jörg Zügel



3.3.3.1 Overall Building Design

- The building architecture should have order and harmony. Building elements should be symmetrical with a clean geometry, and all parts should fit together to create a cohesive and balanced design.
- Neoclassical buildings usually use the Greek rule of proportion drawn from the golden ratio to design the architectural elements as well as for the overall composition of the building. Such consideration is encouraged.
- Neoclassical architecture style had no domes or towers, so they are discouraged in buildings using this style.
- The building façade is to be flat and long. A screen of freestanding columns, or windows or other elements that mimic columns is usually used. Historic examples within the Coachella valley did

not typically utilize true Greek or Roman columns, but simpler vernacular style.

3.3.3.2 Walls

- The façades are characterized by restrained ornamentation.

3.3.3.3 Roofs

- Roofs are usually flat and horizontal or low-pitched gable.

3.3.3.4 Articulation and Decorative Elements

- While the buildings can have restrained decoration and minimal ornamentation, blank façades are not permitted.
- Corner stones, or quoins, are encouraged to give a sturdy appearance.
- Trims are encouraged to be used around windows and doors.
- Use of a balustrade on upper levels is encouraged.
- Use of dentil molding—a series of closely spaced, rectangular blocks that form a molding—is encouraged below the cornice, along the roofline of a building.
- The decorative dentil molding band is encouraged to be used anywhere on a structure.
- Pilasters—a rectangular support or decorative protrusion that resembles a flat column—are encouraged to be used on the façade. Pilasters should project slightly from the wall and have a base, a shaft, and a capital like a column.

Balustrade and Use of Pilasters, Corner Treatment of Buildings. SOURCE: Jörg Zägel



Vertical Proportioned Windows, Decorative Entryways, Pediment. SOURCE: A.Savin

3.3.3.5 Windows and Doors

- Doorways should have decorative surrounds and pediments (the triangular section found above the entranceway).
- Windows should have vertical proportions with double-hung sashes. Division into six or eight panes is encouraged.
- Windows should be evenly spaced across the building's façade.
- Windows are encouraged to be flanked by shutters.
- Fanlight windows can be used in gables or above doors on the upper floors.

3.3.3.6 Porches and Arcades

- A temple-like pediment over the entry porch is usually seen as a characteristic of neoclassical architecture. However, these architectural features do not reflect the local neoclassical style in Coachella and hence, are discouraged.



- Buildings should have a full-height front porch supported by a row of columns.
- A balustrade along the second-story porch is encouraged.

3.3.3.7 Columns and Pillars

- More vernacular columns or pilasters are preferred over Greek or Roman columns.
- Columns should be even in number.
- Repetition of columns in porches and along the façade is encouraged.

3.3.3.8 Materials and Colors

- Heavier materials such as exposed reinforced brick/brick cladding are to be used in the lower portion of the structure, with lighter materials such as wood used in the upper portions of the façade.
- Exterior materials include:
 - Exposed reinforced brick, stone, and plaster on walls.
 - Trims should be made of wood or materials that are like wood and are durable. The use of foam molding is not permitted.
- If plastered, the buildings are usually in shades of white.

3.3.3.9 Lighting

- Neoclassical outdoor lighting fixtures integrating Greek and Roman ornamentation with the features listed below are encouraged:
 - Wrought iron lighting fixtures, including lanterns of many different shapes
 - Clear glass shades to accent candle-shaped bulbs or amber shaded glass
 - Grandly ornamented fixtures with scrolled metal accents
 - Fixtures are either mounted to the wall or hung using chains

3.3.4 Postmodern Architecture

Postmodernism emerged in the latter half of the twentieth century. It was a reaction against the rigid doctrines of the modernist movement that included austerity, simplicity, functionalism, uniformity, and a lack of ornamentation, color, and human scale, as well as lack of tribute to the

history and culture of the community. Modernist architectural style is a movement based on certain characteristics and features of the building as explained previously (and is not to be confused with contemporary architecture that means architecture of today). Complexity and contradiction of form, style, color, and material is the underlying theme of all postmodern buildings. Postmodernism is a broad movement that draws from a variety of architectural styles, history, culture and heritage and molds them together to create new typically more abstract design.

It is not the intention of this style to limit expression of architectural elements such as roofs, doors, windows, walls, and so on based on strict rules. Rather, the style allows for free expression and leaves room for diverse implementation and is represented by the major characteristics described below. The structures developed in the postmodern style are encouraged to draw from the other architectural styles described in this section: Spanish Colonial Revival, Mission Revival, and neoclassical. The architecture should also draw from local heritage, history and culture, as well as Mexican heritage. Ornamentation, color, and articulation drawn from these styles will allow for continuity while fostering unique architectural expression and respecting the history of the community. Buildings range from using ornamentation on the façade to being works of sculpture themselves.

3.3.4.1 Overall Building Design

- Postmodern buildings often combine astonishing new forms and features with seemingly contradictory elements of earlier periods and architectural styles.
- Contradiction of form with function is also a common element. Using traditional elements with connotations of very modern technology is a prevalent feature of postmodern architecture.
- Fragmentation of one structure into several smaller structures and forms is common with the use of different materials and styles. The concept aligns well with the overall vision of encouraging a building that respects human scale.
- Newer postmodernist compositions are rarely symmetrical, balanced, and orderly. Asymmetrical forms and curved and oblique buildings that tilt, lean, and give a sculptural quality are common. These buildings lend a whimsical character to the street and break the uniformity and order of modernist and international architectural styles.
- Trompe-l'œil, the art of making a two-dimensional object look like it is three-dimensional, makes buildings appear to be more spacious and bigger than they are. This is prevalent in postmodernism.

Use of Architectural Elements and Ornamentation from Different Styles.



Fragmentation of Buildings and Unsymmetrical Structure. SOURCE: Maksim.



Use Colors, Texture and Shapes. SOURCE: Andrew Bossi Laurel,



3.3.4.2 Ornamentation and Colors

- Articulation and ornamentation range from abstraction to monumental and informal forms, as well as traditional and high-tech representations.
- The accumulation of elements freely borrowed from past styles is encouraged to derive ornamentation from Spanish Colonial Revival, Mission Revival, and neoclassical styles.
- Colors and textures are important elements. It is common to use bright vibrant colors as well as different hues and contrasts to make the shapes stand out. Colored glass, ceramic tiles, or stone are also used to add texture.
- Exaggeration of forms is common and is encouraged.

3.3.5 Architectural Innovation

While repeating or reinterpreting the above-mentioned styles of architecture is encouraged, the design guidelines are not intended to curb innovation and the exploration of the evolution of architecture and the built environment. The use of innovative architecture that provides a building superior for its intended use and a benefit to the community can contemporary and international style architecture. The following must be considered if innovative architecture is proposed:

- A narrative explaining the connection of the proposed architecture to the vision, heritage, and/or history of the Pueblo Viejo District
- The use of materials, colors, and design elements that do not clash or disrupt with the surrounding context of structures and uses
- The building’s ability to create a positive impact on its surroundings
- The building’s ability to create a vibrant, inviting environment for its users

Buildings in an innovative style can push the barriers and create developments that are exceptional in their functionality while being unique in their aesthetics but will also be scrutinized during architectural review more closely based upon the criteria as described above. Proposed developments in an innovative style will be reviewed on a case-by-case basis. As part of the approval process, developers may be asked to provide additional architectural drawings and details, research, graphics, visualizations, and narrative to explain the building’s inspiration.

3.4 PROXIMITY OF SIMILAR STYLES

To avoid the creation of mono-architecture that departs from the authenticity and current character and eclectic mix of architecture within the Pueblo Viejo District, buildings of the same architectural style should not be placed directly adjacent to each other. It is recommended that buildings of the same style are placed with at least one building of another style in between them. If buildings of the same architectural style are placed next to one another, the two buildings should use different materials, colors and articulation to avoid a monotonous façade. Buildings of the same style located near one another should also incorporate varied features of the architectural style and varied coloring to ensure the preservation of the downtown charm that currently exists in Pueblo Viejo District. Proximity to similar styles will be considered during the architectural review process.

Breaking Monotony- Use of Variety in Architectural Styles, Materials, Colors and Articulation. SOURCE: Loseto



3.5 GENERAL PROVISIONS

3.5.1 Existing Buildings

Certain existing buildings in the Pueblo Viejo District tell a story of the City's development through years and significantly contribute towards Coachella's heritage. These buildings have distinctive features, finishes, materials, spaces, construction techniques that render a unique character to the buildings and hence the corridor. These buildings and its unique elements should be preserved, restored and/or adapted in creative way for newer use.

- If a building or site has been designated as having historic significance by the Coachella City Council, any development on the site or work to the building should comply with Chapter 15.48, Historic Districts and Site, of the Coachella Municipal Code.
- Applicants/ owners are encouraged to refer to the older pictures of the City and apply preservation, rehabilitation, restoration or reconstruction standards as suggested in the "Secretary of the Interior's Standards for the Treatment of Historic Properties" (<https://www.nps.gov/tps/standards/treatment-guidelines-2017.pdf>).
- Development in out-lots, additions/ alterations to historic structures and adaptive reuse should be done in a manner that they maintain the character of the primary structure and are compatible with the mass, scale, and form of original structure. Such changes should not hinder the ability to interpret the design character of the original building. Compatibility does not necessarily mean imitating the architectural style of the structure but the ability of different components, whether similar or dissimilar, to function together and stand together in harmony.
- Expansions to existing buildings should provide for continuity between the old building and the new addition. It is not necessary to match the existing building but should include prominent design elements of the old building to provide architectural compatibility between old and new.
- New structures built in the outlots shall be treated as new construction and shall comply with the Design Guidelines for new structures.
- Additions to original structures should not interfere with the distinctive or character defining features of the structure and should be limited in the size to preserve the relationship with the existing building
- When adapting the use of a historic building, they should be designed to have the least impact on the historic character of the building. Character defining, and distinctive architectural features should be preserved.

- Original details and materials should be preserved as much as possible. When it is not possible to do so due to extreme financial burden, or safety reasons the replaced materials and details should be as close as possible to the original.
- Regular maintenance and repair are preferred over the replacement of any historic materials or features.
- Energy efficiency during adaptive reuse is encouraged but should be done while being true to the historic character of the building.
- A building's original orientation should not be changed, and the original primary entrance should be used as the building's primary entrance. If an original storefront/ façade has been altered over the years, the preferred treatment is to restore them to their original condition based on historic photos or other evidence.

3.5.2 Energy Efficiency and Conservation Design

Building construction, operation and maintenance can have a significant impact on the environment through use of energy, material resources, water usage and stormwater runoff. The applicant is encouraged to employ efficiency and conservation principles from established assessment systems such as LEED, BREEAM, BEES, or other rating systems during the design and construction of the project in order to create sustainable built environment.

- **Energy Efficiency:** Use of Passive Solar Design principles and energy efficient systems can greatly help with energy conservation.
 - Consider use of renewable energy sources such as small solar or small wind energy, which can increase energy security and reduce greenhouse gas emissions associated with energy use.

Solar Panels in the Parking- Provides Shade and Renewable Energy.
SOURCE: Carol M. Highsmith



Uncovering Original Details of Historic Structures. SOURCE: Michael Baker International



Covered-up facade



Uncovered original facade

- Window orientation should have a good balance between efficiency and other benefits such as bright indoor spaces and views south and should be shaded during the heating season by other buildings or trees. North windows lose significant heat energy and gain very little useful sunlight in the winter. East and west windows are likely to increase air conditioning needs unless and hence should be designed with careful attention to shading.
- The South-facing window should include enough solar glazing for good performance in winter, but not so much that cooling performance in summer will be compromised.
- As much as possible windows should be designed to be operable for easy natural ventilation.
- While building entrances are oriented towards primary streets, daylighting opportunities should be fully investigated, and effort should be made to incorporate daylighting in the design of the building.
- Landscape design should strive to provide trees for summer shading that will help reduce heat load to the building and/or outdoor spaces.
- Use efficient building shapes keep this exposed surface area to a minimum there by saving energy.
- Use efficient insulation systems to reduce heat flow in and out of the building from conduction through attic, sidewalls, basement walls and doors.
- **Materials:** Choices of materials and construction methods are important to reduce energy consumption of a building through reduced solar heat gain or loss, lower maintenance and increase life of the building.
 - Design for long life and adaptability for a variety of uses
 - Use durable low maintenance materials with low embodied energy that are locally sourced (including materials salvaged on site). Low embodied energy materials such as concrete, bricks, and timber will help to reduce energy consumed through mining, processing, manufacturing and transporting the materials as opposed to high embodied energy materials stainless steel, aluminum, and plastics.
 - Select materials that can be re-used or recycled easily at the end of their lives using existing recycling systems.
 - Use efficient and right sized building design to minimize materials

■ **Water Conservation**

- Consider employing system for collecting and using rainwater and grey water for irrigation. This will reduce the consumption of treated water.
- Use low-demand native plants and xeriscaping techniques to reduce water consumption on site.
- Avoid over watering and consider using underground drip irrigation systems, which reduces water loss caused by evaporation of surface water during watering.
- Employ design and construction strategies that reduce storm water runoff and polluted site water runoff.

■ **LID and BMPs**

- Low impact development (LID) best management practices (BMPs) are encouraged in public as well as private landscaped areas including parking areas to encourage infiltration and natural water quality treatment. Traditional retention/detention should be used only if LID options are shown not to work in a particular site due to topography, soil conditions, etc. Examples include:
 - Infiltration basin
 - Infiltration trench
 - Permeable pavement
 - Harvest and use BMPs
 - Bioretention and/or biofiltration facilities
 - Sand filter basin
 - Extended detention basin
- Pervious paving and other permeable surface finishes appropriate for the soil conditions and hydrology are encouraged in hardscaped areas. These paving surfaces add texture and facilitate stormwater management. Particular care should be given to the installation, maintenance, and life cycle of such materials.
- Consult the Riverside County Watershed Protection “Whitewater BMP Design Handbook for Low Impact Development” and City of Coachella Municipal Code Section 13.16.047 for more information.

Xeriscaping. SOURCE: Potomacpalms



Low Impact Development. SOURCE: Chris Hamby



Pervious Paving. SOURCE: Jason Johnson



3.5.3 Exterior Building Materials

- Exterior building materials should be durable to avoid rapid wear and tear, economically maintainable, and of a quality that will retain their appearance or age well over time. For example, the use of foam molding of any kind is not permitted due to its lack of durability.
- In selecting exterior building materials, consideration should be given to the appropriateness of the materials based on the building's architectural style.
- Exterior building materials and colors should be compatible with the surrounding neighborhood setting and should be in keeping with the geographic and climatic conditions specific to the Pueblo Viejo District.
- Materials should be authentic whenever possible, although more durable contemporary materials are acceptable if the quality and appearance truly mimics the original authentic material.
- New inventions and established synthetic materials, if used, should age, in a similar manner to the natural materials they replace (i.e. patina).
- Materials should discolor naturally and only if intended as for historic purposes. Materials that unintentionally discolor should be avoided. Architectural styles that do not depend upon a historical context should be maintained in a manner to avoid discoloration and utilize proper maintenance and replacement.
- Reflective materials should be avoided.
- The combination of materials and colors on a building façade should be appropriate to its style and design.
- A variety of materials or colors should be used that emphasize differentiation between the various components of the building.
- A color palette with a minimum of five colors (with a minimum of three-color families) should be provided unless justification is provided for variation from this guideline.
- Adjacent buildings should have a differing color palette.

Changes in Materials, Colors, and Textures. SOURCE: Christopher L. Riley

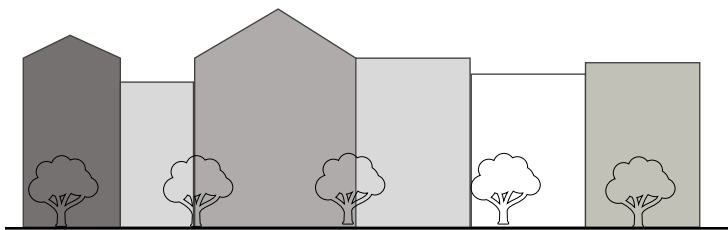


- Larger developments are encouraged to break the monotony of the façades using color and material differentiations in addition to the roofline variations to reflect the small-scale street frontage.

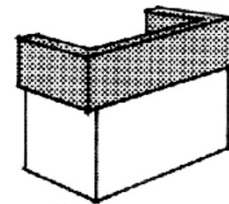
3.5.4 Roofs

- Rooflines should be designed to create architectural interest and to “break” large structures into smaller forms. Rooflines should employ distinctive profiles depending on the building’s architectural style as described in Section 3.3.

Changes in Roofline to Break Monotony. SOURCE: Michael Baker International



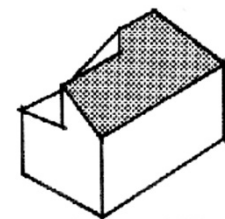
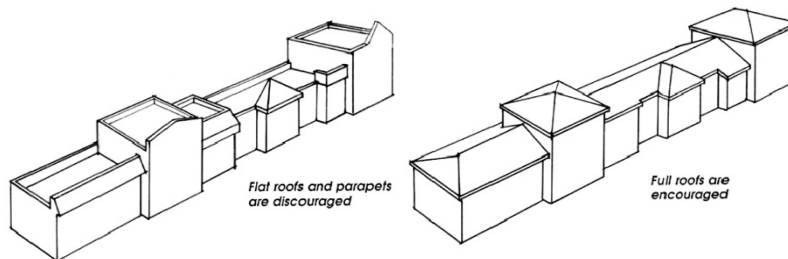
Roofline to Screen the Mechanical Equipment. SOURCE: Michael Baker International



Partial mansard roof discouraged

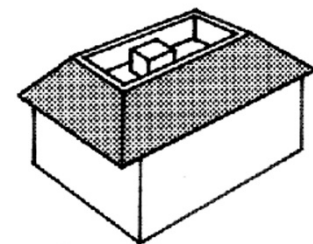
- Parapets facing the street should be subdivided into recognizable segments with shifts in height and architectural treatments.
- Use of flat parapets is discouraged and use of full roofs are encouraged

Flat Parapet vs, Full Roofs. SOURCE: Michael Baker International



Clipped roof to hide rooftop equipment

- The form, color, and texture of the roof should be an integral part of the building design and compatible with both the natural and built settings.
- Rooflines should be designed to screen roof-mounted mechanical equipment. The screening should reflect the architectural style.
- Roof materials should be durable, energy efficient, and relate to the building’s design and architectural style. Roofs should be detailed to be consistent with an appropriate use of the chosen material.



Full mansard roof will hide rooftop equipment

3.5.5 Shade Structures- Awnings

- Refer to subarea guidelines for allowability of awnings on first floor. Awnings are allowed on upper floors of all sub areas.
- Awnings if allowed on first floor should be no longer than a single storefront. Upper-floor awnings should be no longer than the window width.
- Awnings should not be internally illuminated nor used for signage except for lettering on variance.
- Hard material, fabric and canvas awnings with a matte finish are encouraged.
- Awnings with a high gloss finish and plastic awnings are prohibited.
- Awnings should be made of a durable material that is not susceptible to sun-bleaching. If the awnings become unsightly, they should be replaced immediately.
- Awning colors will be compatible with the overall color scheme of the façade from which it projects. Solid colors or subtle striped patterns are preferred.
- Awnings for principal structures if allowed on first floor should be mounted at a consistent height of 9 feet.

Outdoor Furniture Examples.

SOURCES: Top- K.Артём.1; Middle- Lee Cannon; Bottom- lienyuan lee.



3.5.6 Outdoor Furniture

- Areas open to public at all times are encouraged to have a combination of movable as well as fixed furniture. Movable chairs give flexibility and are encourage public gathering in plaza area.
- All furniture used in outdoor dining area and outdoor gathering spaces shall be situated in a manner to maintain adequate and safe pedestrian and consistent with provisions of the Americans with Disabilities Act and State of California accessibility standards.
- Furniture used in outdoor dining area and outdoor gathering spaces shall be of substantial quality to withstand outdoor use, and maintained in good visual appearance, condition, quality and repair at all times.
- All furniture in outdoor dining area and outdoor gathering spaces should be compatible with the building’s architectural style and consider the climatic conditions. For example, the area receiving direct sunlight should refrain from using metal furniture unless there is enough shade to keep the furniture cool.
- The use of the following materials is encouraged for furniture, in or adjacent to sidewalk areas, while still maintaining quality and structure:
 - Metal, Aluminum and wrought iron

- Wood treated for weather and sun- example “French Bistro” dining sets
- Outdoor furniture including but not limited to tables, chairs and umbrellas, used in an Outdoor Dining Area shall not encroach on the property line or frontage of an adjacent business or property.

3.5.7 Perimeter Fences and Walls

- While security fences are allowed, chain link, barbed wire and razor wire, fences should not be used.
- The design of fences, walls, and other structural landscape features should be compatible with and complementary to the architecture of the building and the surrounding setting.
- Fences and walls should be constructed of durable materials and compatible with the architectural style of the building.
- All fences, walls, and other related features should be accompanied by landscaping to better integrate the structure within the site and reduce its visual impact.
- Walls and fences are generally discouraged along the public realm. But if necessary for safety and aesthetic reasons, wrought iron fences with landscaping are preferred over solid walls unless the walls also double as seating and are combined with landscaping.

Perimeter Fence Examples. SOURCES: Top- Thomas1313; Bottom- C&CGabrielle



3.5.8 Exterior Building Lighting

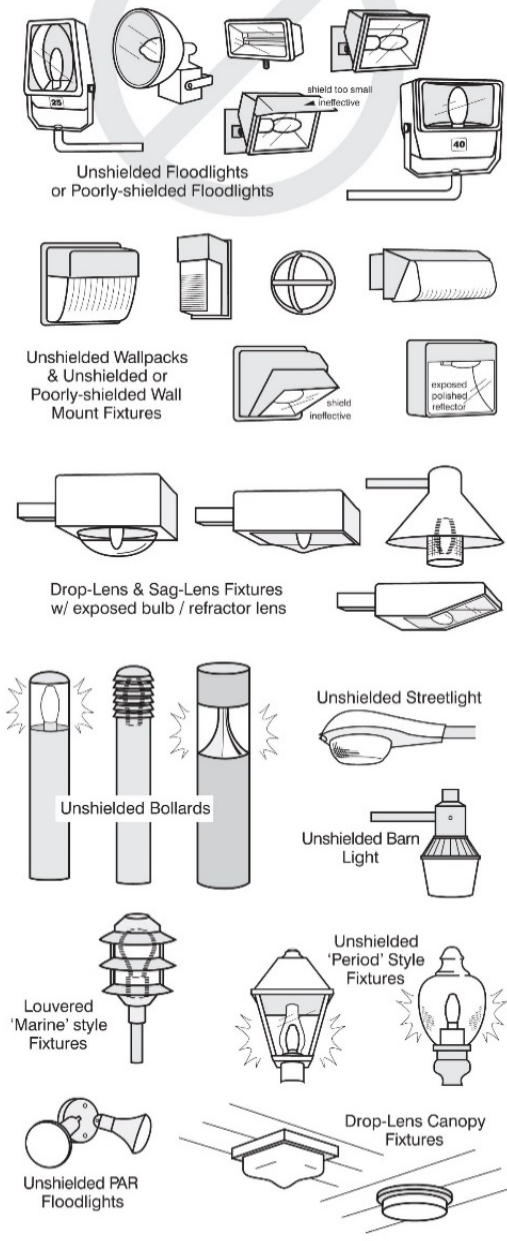
- Exterior building lighting should be used to light building architecture and site areas so as to accentuate design features and promote security in an attractive and understated manner that minimizes off-site impacts.
- Lighting fixtures should complement the façade and be of a style that suits the architectural style of the building.
- Shielded fixtures that minimize sky glow, glare and light trespass should be used. Site lighting should not be directed off-site.
- Direct façade lighting should be provided at all building entrances and along walkways.
- White light that interferes with the circadian rhythms of wildlife and people, and also creates safety issues due to its impact on the retina, should be avoided. Outdoor lighting should be no greater than 3000 degrees Kelvin color temperature as recommended by the American Medical Association.
- Lighting reduction and energy-efficient timer systems should be required after normal business hours except for lighting that is mandated for general safety and security.
- While up-lighting may be allowed to accentuate certain architectural features, care should be taken to make sure that such features minimize sky glow,

- Exterior building lighting should remain in operation till the general peak hours of the Subarea as determined by the City Staff in order to keep the consistency of lighting in the subareas and to encourage pedestrian activity. The lighting may be reduced during non-peak hours, but lighting should not be completely eliminated in parking lots or structures at any time.

Examples of Acceptable and Unacceptable Lighting Features. SOURCE: International Darksky Association (IDA) (<https://www.darksky.org/our-work/lighting/lighting-for-citizens/lighting-basics/>)

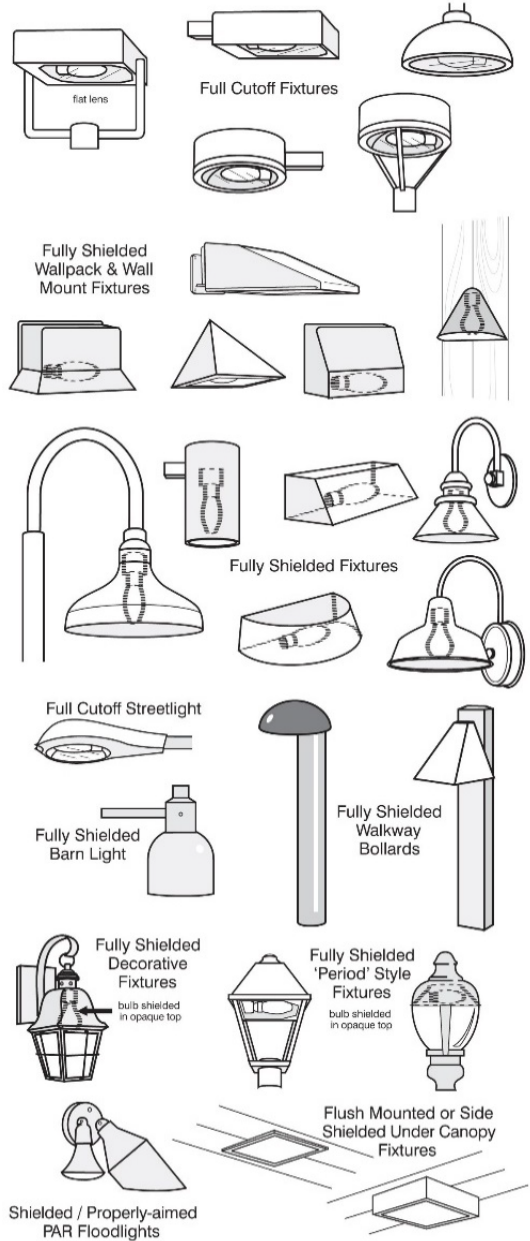
DISCOURAGED

Fixtures that produce glare and light trespass



ENCOURAGED

Fixtures that shield the light source to minimize glare and light trespass and to facilitate better vision at night

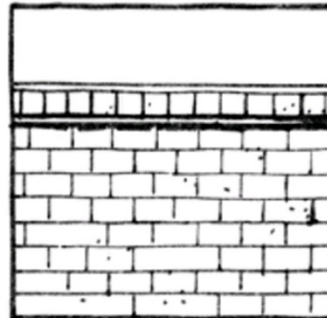
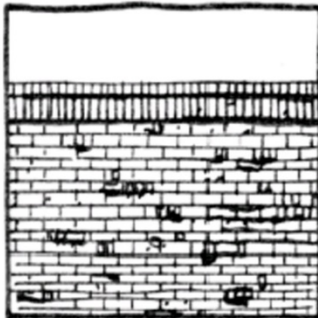


Illustrations by Bob Crelin© 2005. Rendered for the Town of Southampton, NY. Used with permission.

3.5.9 Trash and Recycling Enclosure

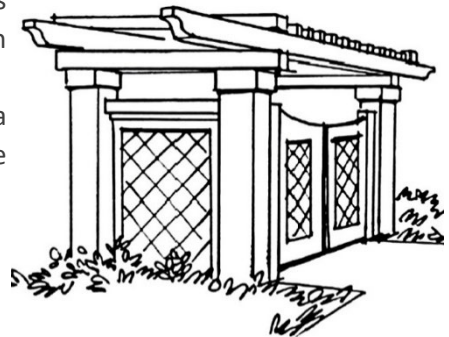
- Trash collection, trash compaction, outdoor storage, utility meters, HVAC equipment, and other service functions should take place behind buildings or on the side of structures and should be visually screened.
- Trash enclosures should be located away from adjacent parcels to minimize noise and odor impacts typically associated with garbage collection and storage.
- The enclosure shall be placed on a concrete pad and have a concrete apron with a minimum depth of 4 inches. Adequate drainage shall be provided around the pad area.

Examples of Solid Masonry or Concrete Tilt-up. SOURCE: Michael Baker International



- All refuse containers shall be screened with a six-foot high (minimum) enclosure of solid masonry or concrete tilt-up with an exterior finish compatible to the main structure.
- Gates shall be solid, heavy-gauge metal or of a heavy-gauge metal frame with a covering of wood or other suitable, opaque material compatible to the main structure.
- The perimeter of the recycling and trash enclosure shall be planted where practical with drought-resistant landscaping, including a combination of shrubs and/or climbing evergreen vines. Attractive xeriscaping may be used as a landscaping option.
- Safety boulders made of durable materials such as steel or concrete should be used to protect recycling and trash enclosure from moving traffic especially in parking and loading area.

Use of Trellis to Screen Trash Receptacle Area. SOURCE: Michael Baker International



Use of Landscaping Around Trash Receptacle Area. SOURCE: Michael Baker International

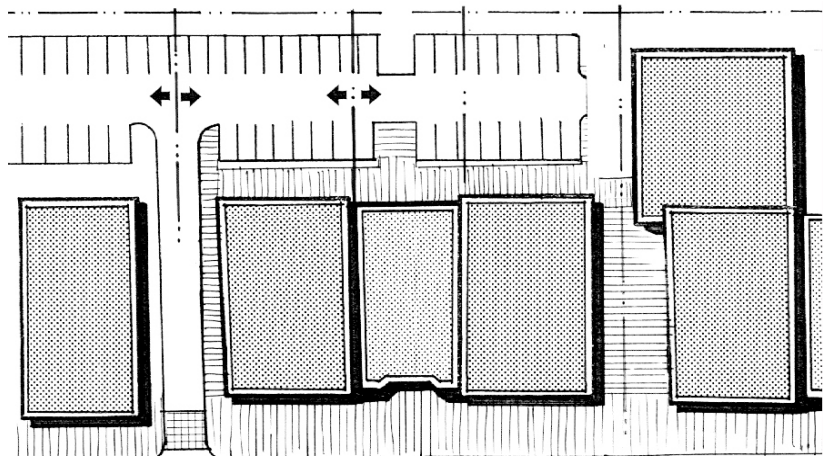


3.5.10 Off-Street Parking Design

3.5.10.1 Parking Lots/Surface Parking

- Landscaping and other architectural features should be used to create attractive buffer and screening between pedestrian pathway and the parking area.
- To avoid large expanses of paved areas, large parking lots should be divided into smaller parking areas. However, Multiple access points to parking areas should be avoided in order to minimize driveways along the street. This in turn minimizes traffic safety conflicts, street congestion, and unnecessarily disrupted street frontage.
- Adjoining properties are encouraged to share access driveways to minimize the number of driveways along public streets.

Shared Access and Shared Parking are Encouraged. SOURCE: Michael Baker International



- Side streets are encouraged to be used for ingress and egress to allow for smoother flow of traffic
- The driveways should be located as far from intersection as possible. However, the location of the driveway is based on available street frontage, site design, expected use of driveways, and traffic safety.
- Traffic calming features within parking lots, including but not limited to speed humps, are encouraged to enhance pedestrian safety in parking areas.
- Connections between parking facilities and building entrances should be considered and enhanced to provide a pleasurable experience and transition from parking lot to building entrance. This includes the use of canopies or other shade structures, landscaping and protected pedestrian facilities.
- Pedestrian walkways should connect parking lots to main building entrances. Enhanced colors and texture materials should

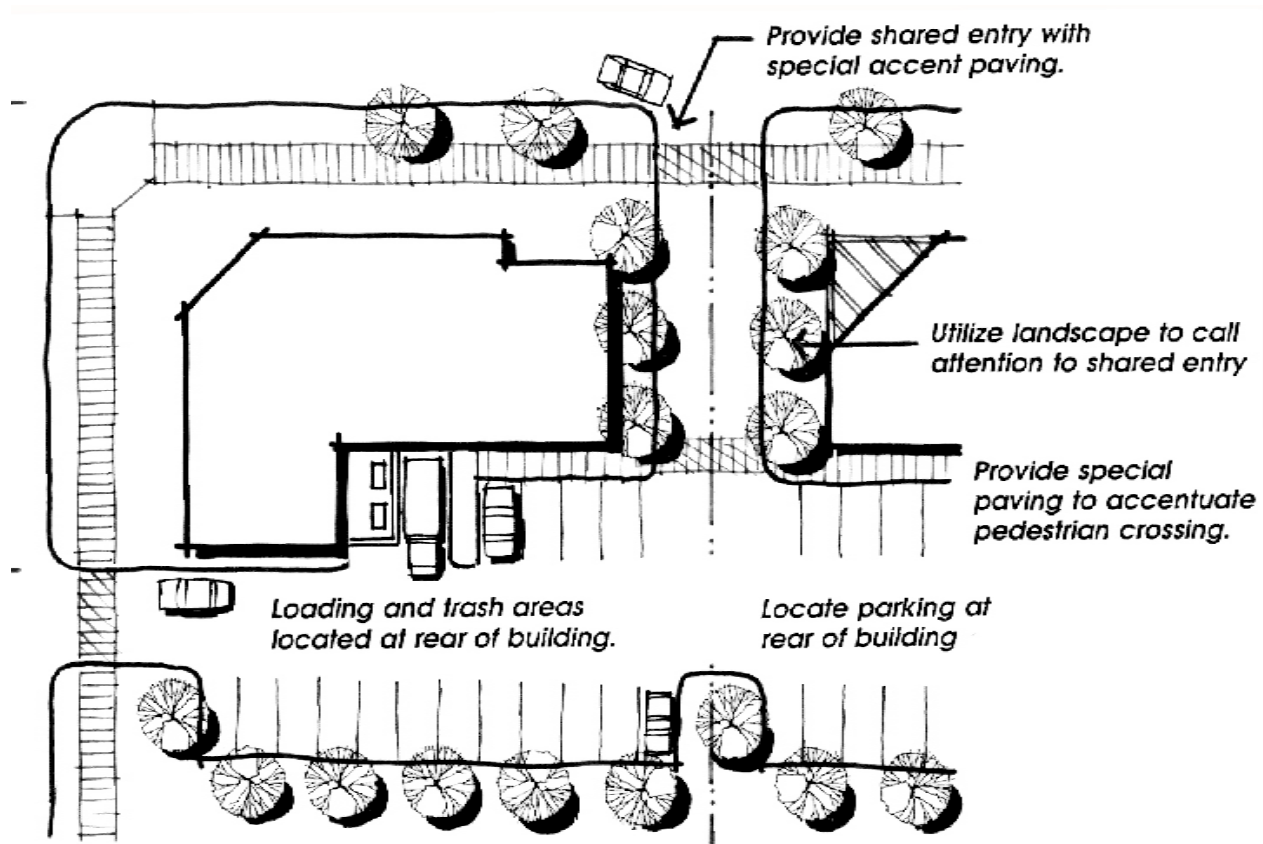
Well-designed Pedestrian Access in Parking Areas. SOURCE: Pi.1415926535



be incorporated into walkway or sidewalks within parking areas. Landscaping should be used to make the pedestrian experience pleasurable.

- Patterned pavement and integral colored concrete are encouraged, especially in areas that will have multiple uses, access from parking lots, and in other areas visible to the public.

Best Practices in Design of Parking Areas. SOURCE: Michael Baker International



3.5.10.2 Parking Garages

- Parking structure design must consider architectural compatibility size, scale, and bulk as it relates to adjacent properties and the overall design of the project.
- The first floor of parking garage should be programable if the garage abuts the primary street in the subarea so as to encourage pedestrian activity on the street.
- If the first floor cannot be programable the walls of first floor should be articulated per the architectural style of the primary structure and/or screened and landscaped to create a pedestrian-friendly environment.

Using Articulation and Landscaping of Non-Programmable First Floor. SOURCE: Top-MTA of the State of New York; Bottom-Nyttend



- Exterior elevations of parking garage shall draw architectural design cues from primary building and incorporate design components and materials utilized and compatible with the primary building(s). Long expanses of shear walls are not permitted.
- The exterior elevations of the parking structure should exhibit horizontal rather than sloping design elements.
- Landscaped planters are encouraged to be incorporated with the garage design
- Landscaping guidelines as described in Section 4.4 should be used to provide screening.

Programmable First Floor Space of Parking Garage. SOURCE: Michael Rivera



3.5.10.3 Bicycle Parking

- Bicycle parking may be placed as close to the entrance of a building as possible or may be organized into larger racks in an easily accessible location.
- It is highly encouraged that bicycle racks contribute to art installations. If unique bike racks that contribute to art installations are not provided, bicycle parking racks should be of the following types: Inverted “U”, “A”, and Post and Loop.
- The amount of bicycle parking facilities that are provided should be consistent with Section 12.40.050 of the City of Coachella Municipal Code.

Artistic Bike Racks- Creates District Identity. SOURCES: Left- Agremon; Right- Jim.henderson



3.5.10.4 Landscaping, Buffers, and Aesthetics

- Landscape buffering should be included as preferred in the landscaping guidelines in Section 4.4.
- Shade trees and drought-resistant planting should be planted in and around parking lots to reduce the heat island effect and offer visual relief in accordance with landscaping guidelines in Section 4.4.
- The landscaped islands are encouraged to use LID techniques/ bioswales to reduce storm runoff.

Shade Trees and Landscaping in Parking Areas. SOURCE: FASTILY



3.5.10.5 Parking Area Lighting

- Parking areas should be done in a consistent, attractive and unobtrusive manner that minimizes off-site impacts. The lighting fixtures should complement the street lighting fixtures.
- Lighting should use LED luminaires to reduce energy consumption and greenhouse gas emissions.
- Lighting should be dark skies compliant and use full or partial cutoffs to reduce light pollution.
- Adequate light should be provided for the safe movement of traffic and pedestrian but avoid glare on adjacent uses.

3.5.10.6 Pedestrian Safety

- Safe and highly visible pedestrian access should be provided from parking lots and garages to structures.
- The walkways should be minimum 6 feet wide and comply with ADA [Americans with Disabilities Act] regulations.
- Traffic calming features, including but not limited to speed humps and high visibility crosswalks may be used to enhance pedestrian safety in parking areas.

Painted Walkways and High Visibility Crosswalks. SOURCE: thisisbossi

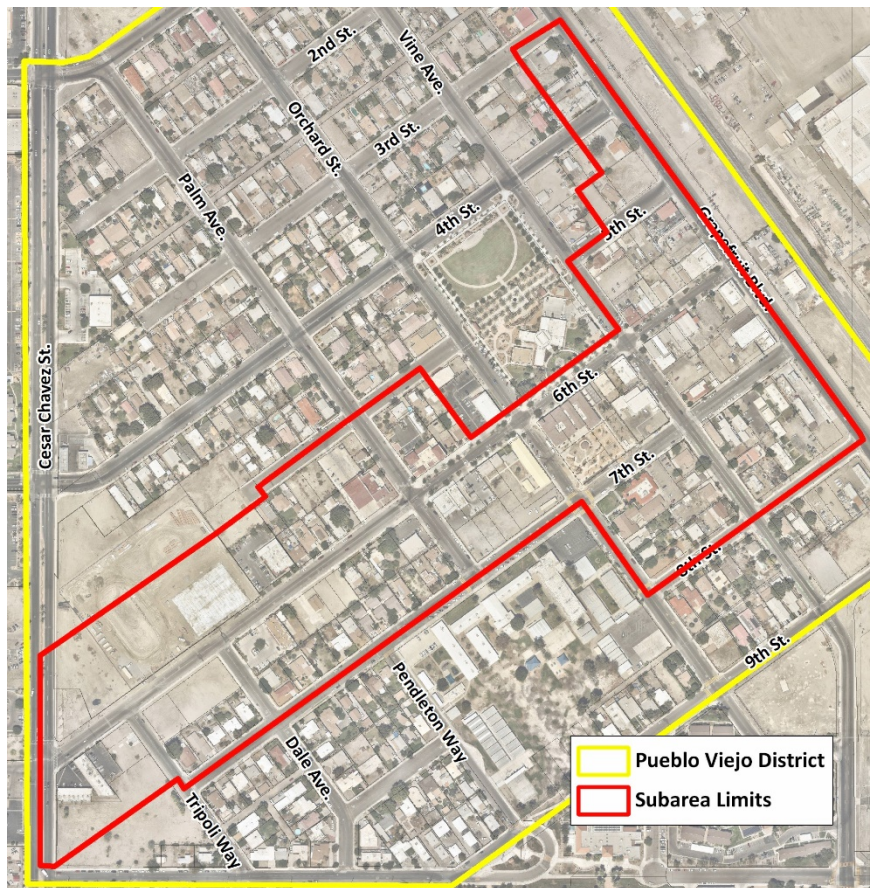


3.6 SIXTH STREET MIXED USE SUBAREA

The Sixth Street Subarea comprises parcels along Fifth, Sixth, and Seventh Streets and includes the Post Office area as well as the area surrounding intersections with Grapefruit Boulevard and Cesar Chavez Street (Refer to Map B).

Sixth Street is envisioned as a pedestrian-friendly horizontal and vertical mixed-use street with pedestrian-scale architecture. The interactive commercial uses on the ground floor and the articulation of buildings will make walking a pleasurable experience. Colonnades and arcades are encouraged as an extension of the building while maintaining harmony with adjacent buildings. These structures also provide much-needed shade. Plaza spaces that create mini gathering areas boost pedestrian activity and are encouraged. Buildings are expected to follow the architectural styles explained in Section 3.3. Architectural styles other than those described in the Section 3.3 may be allowed but are subject to more scrutiny. The City will require visualizations.

Map B Sixth Street Subarea Extents. SOURCE: Michael Baker International, Riverside County GIS



The buildings on Fifth and Seventh Streets also promote horizontal and vertical mixed-use development, though multifamily residential structures with no ground-floor commercial are also allowed.

Unless specified, the Sixth Street mixed use guidelines apply to structures oriented on Fifth, Sixth, and Seventh Streets.

3.6.1 Existing Buildings

Certain existing buildings along Sixth street enforce the feeling of a downtown district with small setbacks and pedestrian-oriented entrances. Preservation, restoration and adaptive reuse of these buildings is especially encouraged, where existing buildings. Refer to Section 3.5.1 for guidelines related to existing buildings

Preserve Existing Structures that Enforce a Feeling of Historic Downtown. SOURCE: Michael Baker International

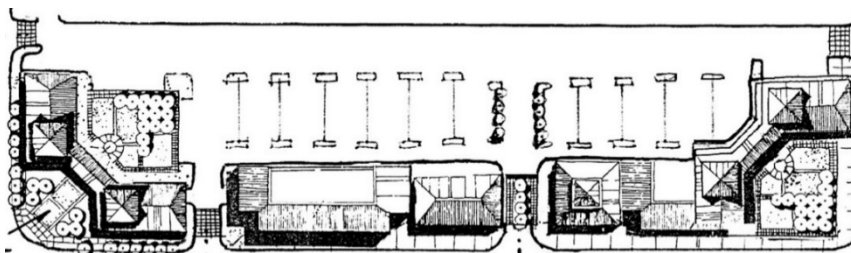


3.6.2 Site Design

3.6.2.1 Building Orientation and Setting

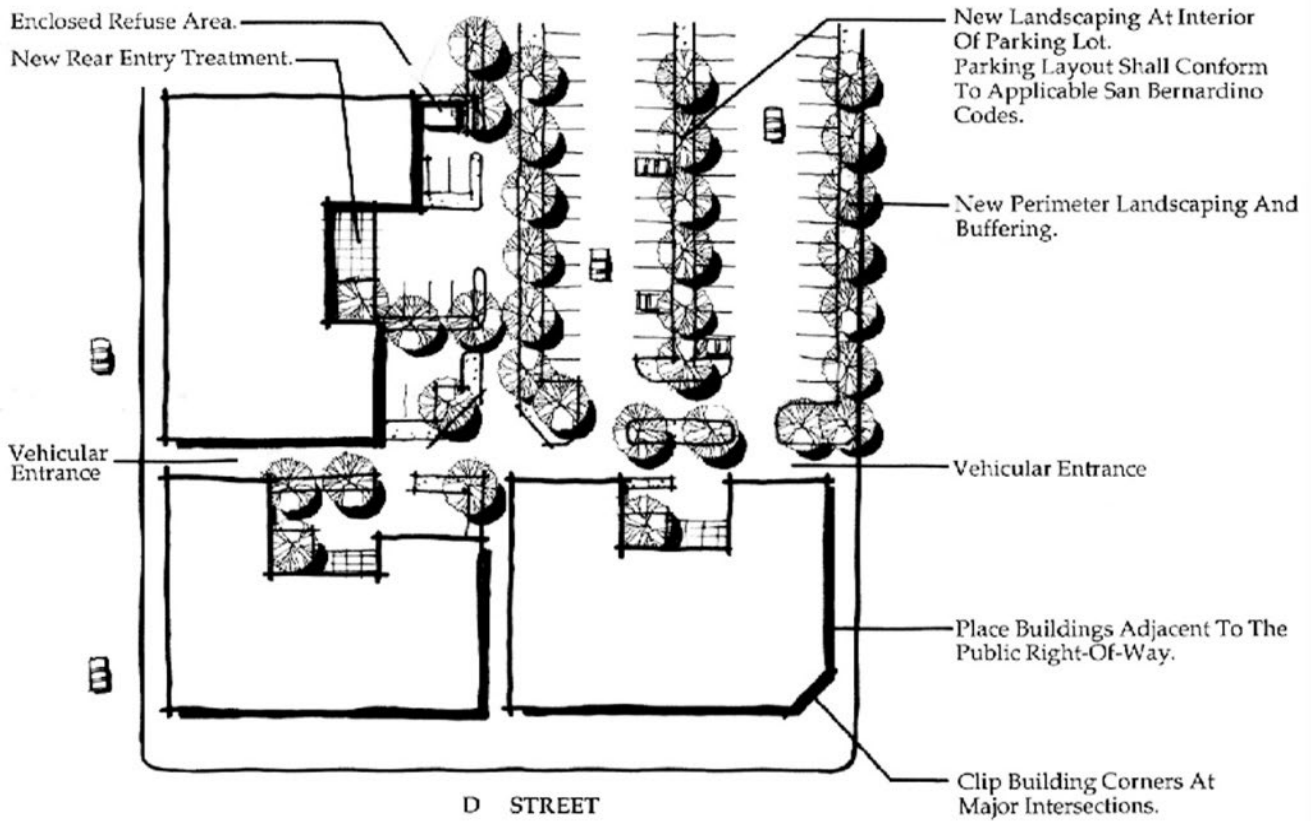
- Building orientation facing the primary street is important for creating street appeal, enhancing aesthetics, and drainage considerations and encouraged.

Building Orientation Towards the Street. SOURCE: Michael Baker International

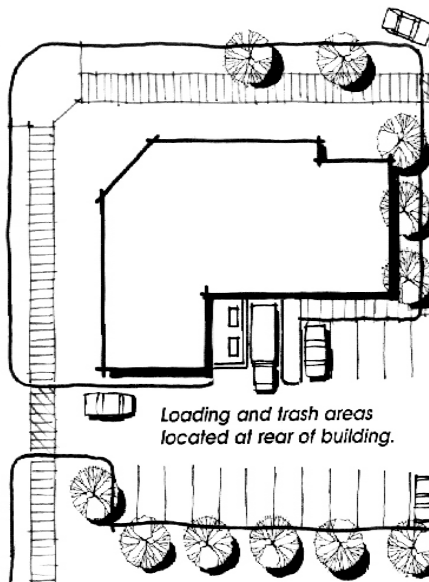


- When possible, the entry to parking and loading areas should be from the side streets. This orientation will help reduce the number of driveways on the primary street and create a pedestrian-friendly environment.

Typical Building Site Setting for Sixth Street Subarea. SOURCE: Michael Baker International

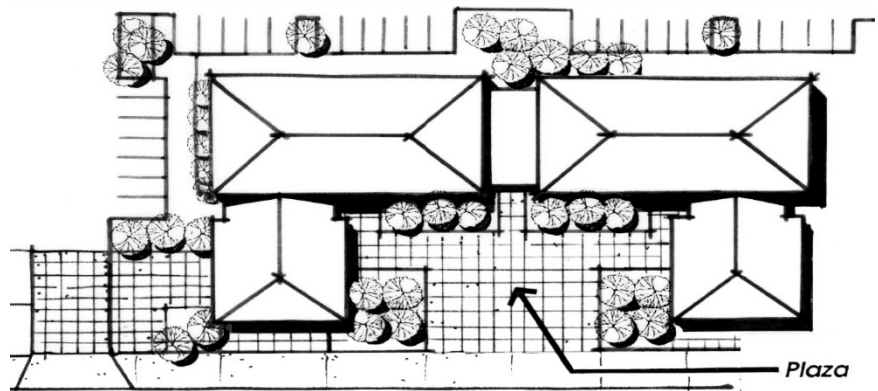


Loading Areas Bend the Buildings. SOURCE: Michael Baker International



- Parking and loading areas should be located at the rear of the buildings.
- Plazas, outdoor dining, or other open spaces should be oriented toward the street (see Section 3.6.4.1, Plazas, Open Space, Paseos, and Outdoor Dining).

Public Gathering Areas Orientation Towards the Street. SOURCE: Michael Baker International



3.6.3 Architecture

3.6.3.1 Massing and Articulation

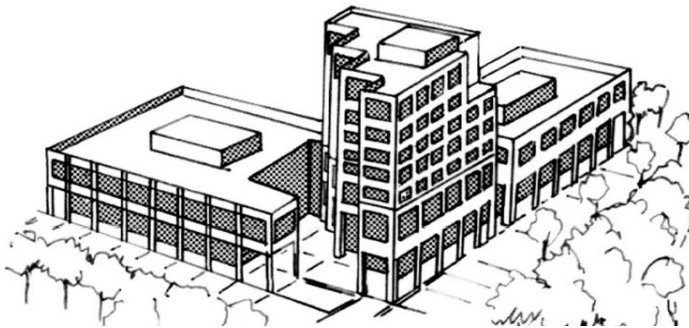
- Buildings should use four-sided architecture, meaning all visible sides of the building should have façade treatment. However, the primary entry will be on the primary street.

Use of four-sided Architecture. SOURCE: M.O. Stevens



- Where the side walls are required to be blank for architectural or structural reasons, and are visible from the street, the developer should partner with the City for mural installation in accordance with the City’s mural program. See Section 3.10, Signs, Murals, and Public Art.
- Changes in height, materials, articulation style, massing, rooflines, and so on break the monotony of the façade and create interest for pedestrians. These changes should occur often (every 25 to 35 feet) along a building façade to retain pedestrian interest. Larger projects should be designed to reflect a small-scale street frontage.

Changes in height. SOURCE: Michael Baker International



Painted murals on blank walls help with placemaking. SOURCE: Thomas R Machnitzki



Changes in articulation. SOURCE: Michael Baker International



- Any articulation should comply with the maximum setback limits provided in the Zoning Ordinance.

Pedestrians Observe Details. SOURCES: Top-Tim Kiser; Bottom- Stanley Walker



3.6.3.2 Articulation and Detailing

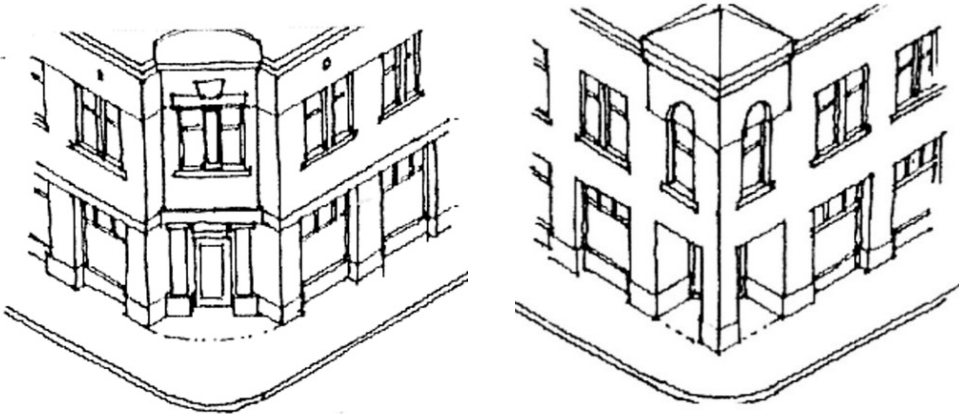
Pedestrians move slower than vehicular traffic and are more likely to observe articulation and smaller details. In order to retain pedestrians’ interest and make walking a pleasurable experience, buildings should have architectural elements that are detailed to be appreciated by the pedestrians and authentic representations of architectural styles. Variation in materials and colors and/or use of cornices, decorative columns, and other features are different ways of providing articulation.

- Depending on the architectural style, articulation or framing around the windows and doors using varied materials or colors can create interest in building façades.
- On façades that do not include entrances, windows, or display windows, articulation using vertical and/or horizontal elements, should be used on the façade to add visual interest. Applicant should refer to the Articulation and Decorative Elements and Windows and Doors subsection under Allowed and Encouraged Architectural Styles in Section 3.3 depending on the architectural style of the project
- All facades should emphasize three-dimensional detailing depending on the architectural style used. Such detailing provides depth and play of shadows creating visual interest on the façade.

3.6.3.3 Entry Orientation

- Primary entries should be on the primary street. The entry should be accentuated to make it visible from the street, depending on the architectural style.
- Entries oriented toward Sixth Street should be integrated with the colonnade or arcade structures.
- Entries providing access to uses on the upper levels should clearly visible from the street and integrated with the ground-floor façade to highlight the access location.
- Corner lots at the intersection of Streets are encouraged to have corner entries thus enabling entry from both streets while maintaining single point of entry.
- Development on corner lots should make a strong architectural statement and place more emphasis on the building corner.

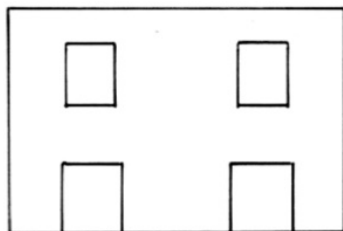
Corner Buildings, Strong Architectural Statement, Corner Entries. SOURCE: Michael Baker International



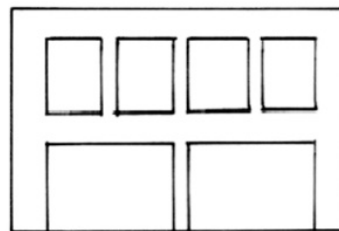
3.6.3.4 Window Design

- Windows should be used as architectural elements that add relief to the façade and wall surface while being true to the architectural style.
- The window design can be modified on upper levels to allow for the functionality of the space as intended. However, it should follow the rhythm established by the ground floor articulation.

Windows as Architectural Element. SOURCE: Michael Baker International



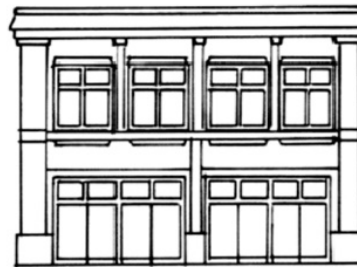
Proportion of opening sizes to building mass is too small



Increase opening sizes



Articulate openings

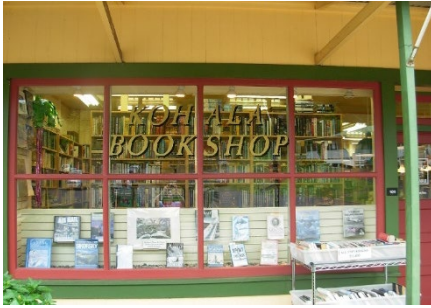


Break up building mass

Modified Upper Level Windows. SOURCE: Michael Baker International



Display Window- Providing Visual Access to the Interior. SOURCE: Brewbooks from near Seattle



Display Window- Providing No Visual Access to Interior. SOURCE: Editor5807



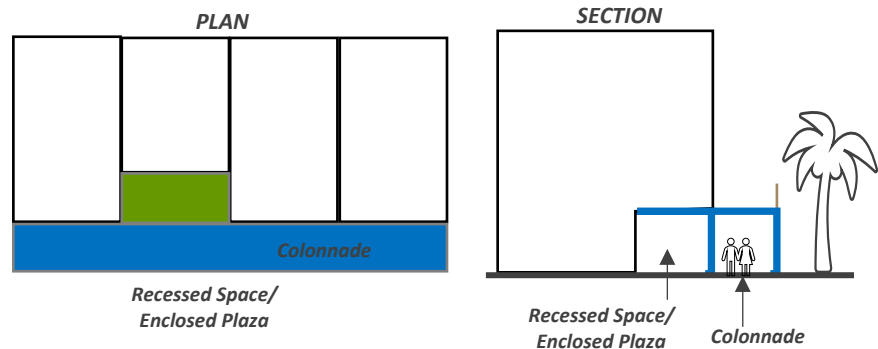
- Commercial storefronts should include street-oriented display windows in order to engage pedestrians. These windows should provide visual access to the building interior while also serving as an area for merchandise display.
- Enclosed display windows that provide minimal to no visual access to storefront interiors should be included where actual windows cannot be provided due to the nature of the building’s use.
- Exceptions to window display requirements can be made where display windows (open and enclosed) are not feasible. In this case, exterior walls should be designed to offer architectural relief or be screened by landscaping and designed to provide pedestrian amenities such as wider sidewalks or benches.
- Clear glass windows are encouraged especially for retail use to promote interaction between pedestrians and the businesses and to provide light and views into store interiors. The tinting of glass should be no more than 20% on all windows. Highly reflective window film is prohibited as the application can interfere with moving traffic.
- For both commercial and mixed-use buildings, real windows that face the street should be provided on the second floor. In mixed-use buildings, the second-floor windows should be fully functional and able to be opened.

3.6.3.5 Porches, Balconies, and Decks

3.6.3.5.1 Porches

- Porches are not conducive to the Main Street style of the Sixth Street Subarea. Instead, a continuous colonnade structure spanning entire blocks is desired. See Section 3.6.3.6 for Shade Structures. However, enclosed plaza spaces or recessed porches are allowed.

Commercial Uses Oriented Toward Sixth Street. SOURCE: Michael Baker International



- Recessed spaces are encouraged for buildings on Fifth and Seventh Streets as well as for buildings oriented on minor streets. Such a space should be designed to be consistent with the architectural style of the building to which they are attached and have sufficient depth and width to be occupiable spaces.

3.6.3.5.2 Balconies

- Window balconies if compatible with architectural style are encouraged on the upper floors if they do not interfere with street uses or detract from the common building line or patterns.

Window Balconies on Upper Floor. SOURCE: Pixie



- Faux balconies with windows are acceptable provided they integrate features from the architectural style of the building.
- Refer to Zoning Ordinance for setback considerations related to Balconies.
- When balconies on the upper floor run the entire length of the façade, they should be integrated with a colonnade structure on the ground floor. The depth of such balconies should not be more than the depth of the colonnade structure and should be made of lighter materials to give a feel of openness and maintain the visual continuity of the main building structure.
- Where possible, balconies should be oriented toward the street and not toward internal block parking areas.

*Lower and Upper Level Deck Examples.
SOURCES: Top- Jonathan Billinger;
Bottom- Aleniromswit*



*Examples of shade structure for decks.
SOURCE: Top- MB298; Bottom- Dan
Rademacher*



3.6.3.5.3 Decks and Patios

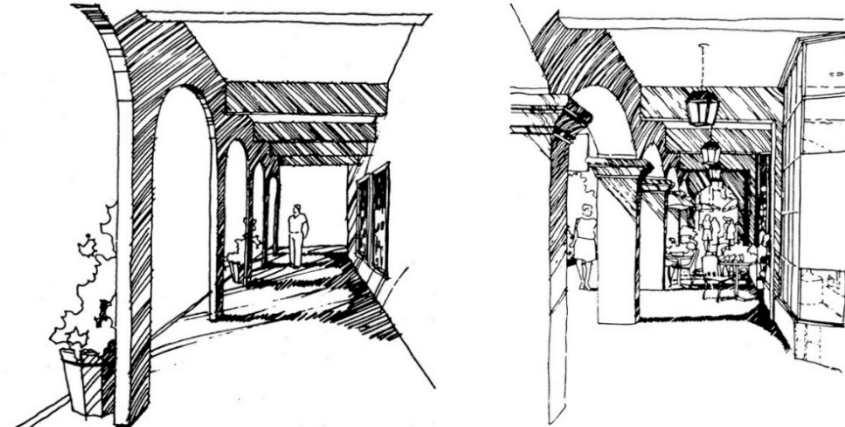
- Decks and patios are encouraged as they provide outdoor gathering/ dining spaces and activate the street life. Refer to Zoning Ordinance for allowable size, setback, and location of patios.
- Decks and patios should be ADA accessible
- Decks should have sufficient depth and width to be occupiable spaces.
- Upper level decks should have a minimum parapet of 36 inches. Though encouraged, parapets are not required if decks and patios are 30 inches above grade or lower.
- The parapet and other articulation of the deck structure should draw from the architectural style of the building or should be designed in away so as to not interfere with the character of the building.
- Decks should be made with durable materials with low heat absorption and should take climatic conditions into consideration. Some suggested materials are natural wood such as redwood, cedar and pressure-treated wood, Composites composed primarily of wood fibers and recycled plastic and Aluminum.
- Decks should be maintained in usable condition free from chipping, cracks, and tripping hazards. Staining and coloring should be done per manufacturer’s instructions on a regular basis.
- Decks and patios are encouraged to use shade structures in order to provide relief from harsh desert climate and rain. These structures can be such as Pergolas, Trellis, Tension membrane structures (TMS) or shade sails, retractable devices, such as canvas awnings or louvres.
- The shade structures should be maintained regularly. If such structures become unsightly, they should be replaced immediately.

3.6.3.6 Shade Structures

3.6.3.6.1 Colonnades and Arcades

- Shade structures such as colonnades and arcades are encouraged, especially on Sixth Street. Such structures should incorporate architectural features from the building’s style while ensuring the visual continuity of the walkway.

Colonnade Should Complement Architectural Style. SOURCE: Michael Baker International



These columns are much too thin. Columns with mass and integrity.

- On corners, colonnades or arcades may wrap around the side of the building facing the side street.
- The space above colonnades can be used as occupiable space.

3.6.3.6.2 Awnings

Awnings are not permitted on the first floor of buildings on Sixth Street Subarea. Awnings may be used on the upper floors to provide shade. Refer to Section 3.5.6 Shade Structure- Awnings in General Provisions for materials, design and maintenance guidelines.

3.6.4 Outdoor Gathering Areas

The outdoor gathering areas include paseos plazas/open spaces, courtyards, and outdoor dining areas. These areas give vitality and vibrancy to the place.

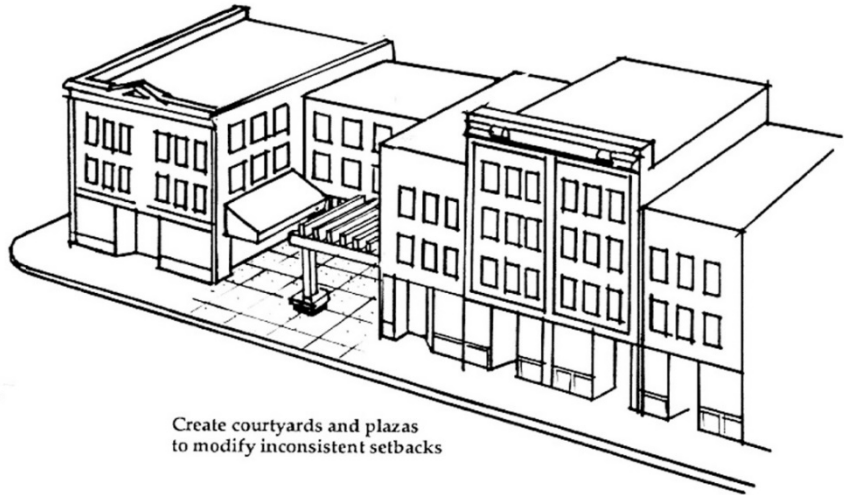
- **Paseos** are outdoor passages dedicated to pedestrian use. They establish connection between streets, parking areas, plazas and courtyards, and building entrances. These areas can be used for spillover dining and gathering spaces.
- **Plazas** are common open spaces that are readily accessible from the street. They provide for large gathering spaces. They can be integrated within single building design or can be created with the placement of 3 or more buildings to form a shared open space.
- **Courtyards** are similar to plazas but at smaller scale.
- **Outdoor dining area** are generally within the setback of the building and separated from sidewalks using a divider such as a

fence. They are generally used by people patronizing the business.

3.6.4.1 Plazas, Open Spaces, Gathering Areas

- Plazas, Open Spaces, Gathering Areas usable by the general public should be physically and visually connected to the public sidewalk.

Plaza Oriented Towards the Street. SOURCE: Michael Baker International



Placemaking Elements Example for Plaza Area. SOURCE: Charvex



Shade Structure Example for Plaza Area. SOURCE: Sekretärin



- Plazas, Open Spaces, Gathering Areas are should incorporate attractive landscaping.
- Plazas are encouraged to be designed to give an inviting feel using high-quality hardscape and focal elements such as water features public art, and murals.
- Use of Interesting shade structures is encouraged but such structures should not prohibit the penetration of sunlight to the ground floor or obstruct view of the storefronts.
- Escalators or elevators should not dominate the function and appearance of the plaza.
- Provision of seating in form of fixed furniture and seating walls is encouraged.
- Plazas developed as a part of residential development on the ground floor can remain private.

3.6.4.2 Paseos

- Paseos should connect interior spaces such as courtyards and parking to the primary street also in order to provide a pedestrian the ability to pass through the block without walking around it.

- Vehicular access, loading, or parking uses are prohibited within the paseo except during restricted hours as determined by the City.
- Paseos should be wide enough to feel safe for a pedestrian. Narrow spaces, even when lit, will tend to not be utilized. Increasing a Paseo's size can also allow for multiple uses such as dining and gathering spaces.
Paseos may be used as outdoor dining areas. However, they should be wide enough to accommodate a clear walking path for pedestrians and provide for ADA accessibility.
- Entries to the paseo, and storefront entries within the paseo, shall be designed and lighted so they do not create hiding places.
- Lighting should be provided from overhead fixtures, either mounted on the building facade or suspended from cables spanning the paseo so they do not obstruct the free-flow of pedestrians.
- Murals, sculptures, artwork, archways, water features, landscaping and other features that help with placemaking by creating interest for the users and thereby activating space in paseo areas are encouraged. These placemaking elements should be compatible with the architectural style of the building.

Active Paseo Space. SOURCE: Martin Falbisoner



Example of Active Paseo Area Connecting Two Streets. SOURCE: Comercio y Justicia



Example of Sidewalk Dining Areas in Frontage Zone. SOURCE:



Example of Sidewalk Dining Areas in Furnishings Zone, SOURCE: Runner1928



3.6.4.3 Sidewalk Dining Areas Fences and Barriers

- Fences and barriers are required to demarcate the outdoor gathering areas when they are in Frontage Zone (area between pedestrian walkway and the property line), so they do not encroach cause obstruction for pedestrians and provide a clear walkway for passersby.
- Fences may not be needed on pedestrian side if the outdoor dining is in the Furnishings Zone (area between pedestrian walkway and street. However, fence will be required on the street side of dining area.
- Fences and barriers shall be removable at any time if required; however, they shall be properly secured when the corresponding business is closed or not in use.
- This fencing or barrier should be semitransparent and architecturally compatible with the building’s architectural style. Refer to Zoning Ordinance for maximum allowable height of the fence or barrier.
- Fencing materials should be of good quality and durable, such as wrought iron, steel, or wood and be compatible with the architectural style of the building.
- No fence or barrier shall have any sharp or pointed pickets that could cause injury. All finishes should be clean and free of any exposed screws or other fasteners.
- Fences are encouraged to incorporate planter boxes that are easily removable. The planters should be and should be maintained at all free of dead plants. The plants should not exceed the height of the fence. Artificial plants are discouraged.

Example of Sidewalk Dining Areas in Frontage Zone. SOURCE: Will Buckner

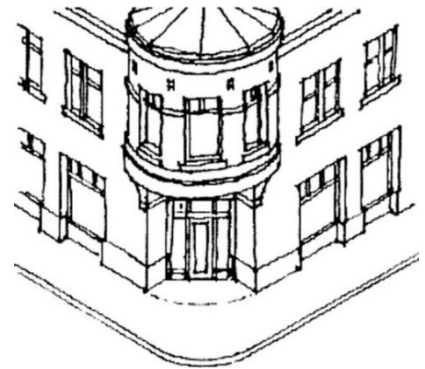


3.6.5 Buildings at Intersections

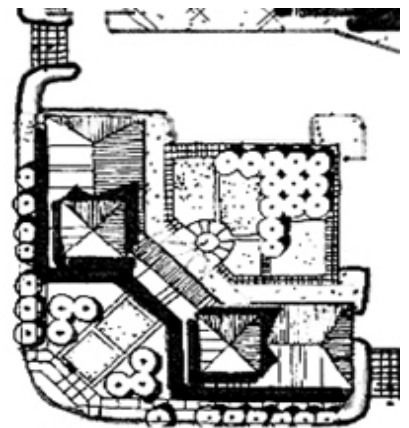
The buildings at the intersections of two streets are the most visible and have a greater effect on passersby. They provide entry to the street; thus, more articulation is desired. The architectural and other guidelines above are also applicable to the buildings at the intersections of Sixth Street with other major or minor streets. However, it is expected that these buildings will be treated as gateway buildings, especially at major intersections—Cesar Chavez Street and Grapefruit Boulevard.

- Several ideas are listed below for the corner buildings that will help create the gateway effect.
 - Buildings can be higher than the rest of the buildings on the street/ block.
 - Buildings can be recessed to create a small plaza space and include art, a fountain, or a focused landscaping feature.
 - Buildings can have a corner element appropriate to the architectural style, such as a bell tower.
 - Buildings can creatively use projections on the façade at the corners to emphasize the effect of the entryway.
 - Materials or colors could be varied at the corner, depending on the architectural style.
- A greater degree of ornamentation or articulation is desired appropriate to the architectural style compared to the rest of the structure.
- The corner buildings are encouraged to have corner entries to enable entering the building from both streets.

Greater Degree of Ornamentation and Projection of Corner Element. SOURCE: Michael Baker International



Create a Small Plaza Space at the Corner Entrance. SOURCE: Michael Baker International



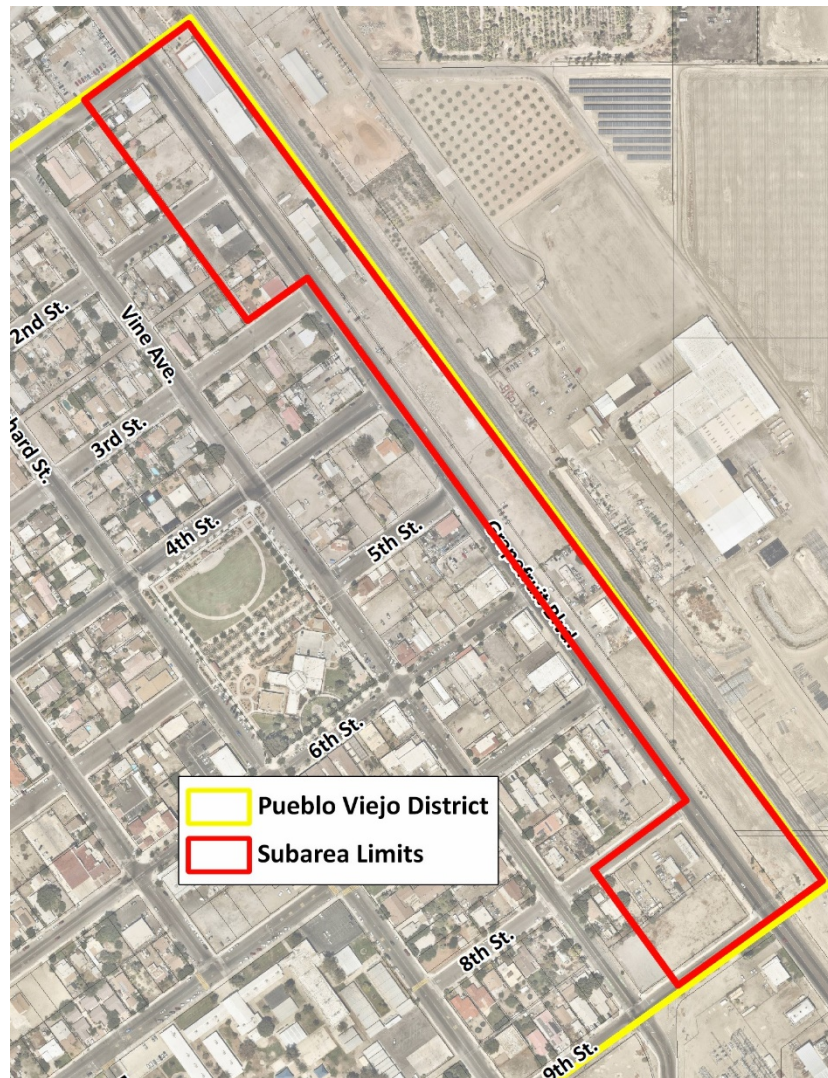
Greater Degree of Ornamentation and Projection of Corner Element. SOURCE: M.O. Stevens



3.7 GRAPEFRUIT BOULEVARD SUBAREA

Grapefruit Boulevard Subarea includes the parcels along highway 111 in the Pueblo Viejo District. These parcels have highway oriented light industrial businesses ranging from automobile repair, auto sales, gas stations, small stores and offices. Most of these businesses are expected to remain in place. The new buildings are also envisioned to be industrial or highway commercial making Grapefruit Boulevard a vehicle-oriented commercial corridor with automobile-scale buildings. Hence, the guidelines related to scale, massing, articulation and signage will be suited to an auto centric setting. However, it is also expected that the development of out-lots, vacant parcels and adaptive reuse of the structures could lead to development of uses such as bars and restaurants and hence, pedestrian-friendliness is a necessary part of the

Map C Grapefruit Boulevard Subarea Extents. SOURCE: Michael Baker International, Riverside County GIS



guidelines. The goal of design guideless for this area focus on creating a cohesive appearance using certain common elements and a unique look by incorporating colloquial architectural elements and art and history that relates to Coachella.

Grapefruit Boulevard between First and Ninth Streets should stand out from the majority of this thoroughfare and encourage passing automobiles to stop and enjoy all that Pueblo Viejo District has to offer. The intersection of Sixth Street and Grapefruit Boulevard is one of the three Gateways to Pueblo Viejo District and hence should be designed to reflect that. Refer Section 3.6 for Sixth Street Subarea guidelines. Buildings are expected to follow the architectural style guidance as described below.

3.7.1 Existing Buildings

Certain buildings on Grapefruit Boulevard though automobile oriented, have unique historic industrial characteristics and are encouraged to be restored and/or adapted to newer uses and/or additional uses. Some of the existing buildings if adapted to newer uses can result in outlots that can be developed with additional compatible uses. Preservation, restoration and adaptive reuse of these buildings is encouraged. Refer to Section 3.5.1 for guidelines related to existing buildings.

3.7.2 Site Design

3.7.2.1 Building Orientation and Setting

The larger lot sizes and automobile-oriented uses along Grapefruit Boulevard should be designed in a manner that fits in with the surrounding development pattern and context. However, ensuring that the new buildings establish a more appropriate development pattern is also necessary.

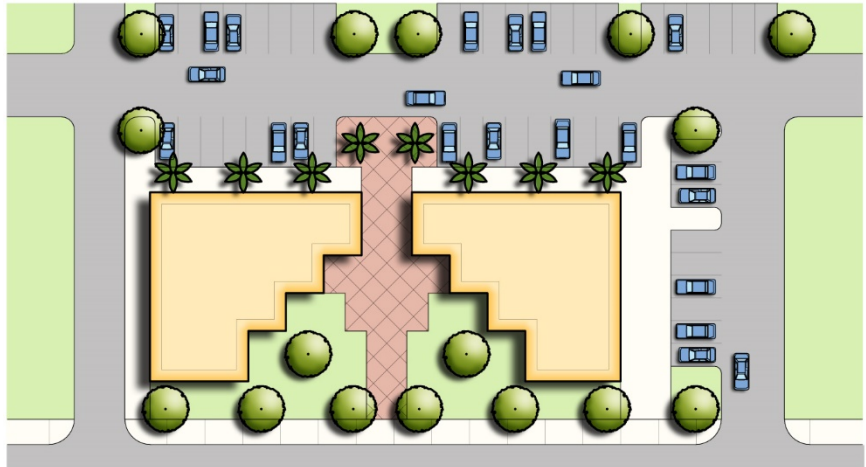
- Primary façade of buildings should be oriented toward Grapefruit Boulevard; Developers are encouraged to treat side facades visible from the street with similar design principles as they treat primary facade.
- Buildings should not be located in a manner that make them appear like “islands” surrounded by paved areas.
- As much as possible the office portions or pedestrian oriented uses should be oriented toward Grapefruit boulevard to create a pedestrian friendly environment.
- Developments should include usable outdoor open space (courtyards, plazas, shaded arcades and functional landscaped areas) whether located in setbacks or other areas. These could also be used as outdoor employee break/lunch areas.

Unique Architectural Features. SOURCE: Michael Baker International



- Mature trees and tree groupings, and significant vegetation should be preserved and incorporated into development’s usable open space where possible. Such features should be considered as strong site design determinants.
- Where industrial uses are adjacent to non-industrial uses, appropriate buffering techniques such as setbacks, screening, effects of industrial operations.

Pedestrian Friendly Building with Parking Behind the Building. SOURCE: Michael Baker International



Pedestrian oriented building parking in the front



Examples of Pedestrian Friendly Automobile Oriented Commercial/ Light Industrial Buildings. SOURCE: Michael Baker International



3.7.3 Architecture

3.7.3.1 Massing and Articulation

The automobile-oriented commercial uses and light industrial structures along Grapefruit Boulevard are expected to have larger building footprints. The larger the mass of a building with unbroken building walls and rooflines, the larger and bulkier it will appear. While faster moving traffic might not need building mass to be broken to the extent that a pedestrian oriented corridor such as Sixth Street might need, it should

not be forgotten that the Grapefruit Boulevard is also envisioned to house pedestrian oriented uses such as bars and restaurants and hence, there should be a balance between automobile-centric and pedestrian centric design with respect to massing and articulation. Bulkiness of buildings can be reduced through several methods that break massing and incorporating articulation. Guidelines include, but are not limited to:

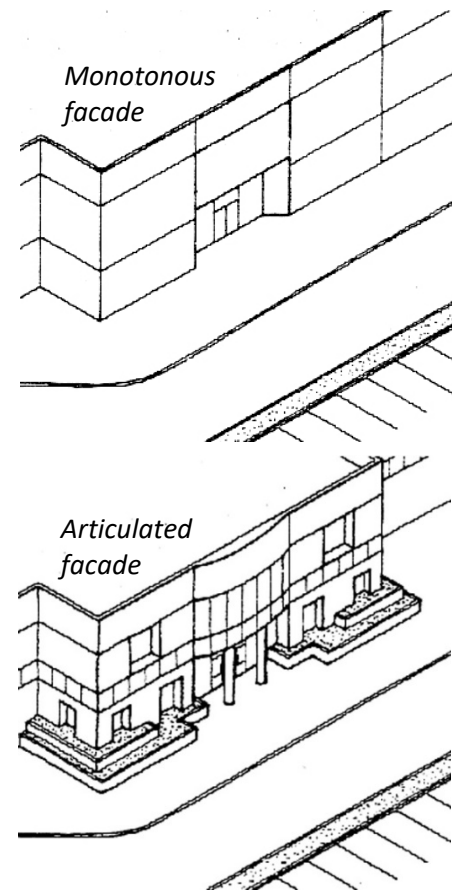
- At an intersection with a minor street, the side of the building facing the minor street should have a façade treatment.
- Developers are encouraged to use techniques such as recessing building floors and changing rooflines to break the mass of the building
- Breaks in building mass should be used to provide visual relief for long building facades. Vertical or horizontal offsets in the wall surfaces at regular intervals, including columns, projections, and recesses, depending on the architectural style of the building helps with breaking the monotony as well as mass of the building. Such treatments are encouraged every 30 to 50 feet
- Applicants are encouraged to incorporate other structures on the site instead of a single dominant building
- Articulating details around doors, windows, balconies and other openings help with breaking the monotony of the façade and are encouraged
- Building façade design oriented towards Grapefruit Boulevard should avoid long, repetitive, monotonous facades – particularly those that repeat the same design element several times along the same elevation
- Façade design should incorporate play of colors and materials depending on the architectural style of the building to break the monotony of façade

The collective architecture of buildings along a street defines the character of the corridor and gives it an identity. Developers are encouraged to draw cues from existing light industrial buildings, automobile repair shops and other existing structures along Grapefruit Boulevard that have distinctive architectural elements and style and in cohesion can create a unique industrial architecture inspired corridor.

3.7.3.2 Entry Orientation

- Primary entrances should be oriented toward Grapefruit Boulevard. Warehousing and industrial uses may have secondary entrances on the sides or rear of the building.
- Primary building entries should be readily identifiable by the moving traffic and well defined through the use of projections, recesses, columns, roof structures, or other design elements

Changes in Materials/ Colors and Using Articulation Interest and Breaks the Monotony of Façade. SOURCE: Michael Baker International



Prominent Entry Feature. SOURCE: Dfadden



based on the architectural style. The single massing break provided by the entry in long blank façade is not acceptable.

- Entries to the upper floors if used by different business offering should be readily visible by the moving traffic.

3.7.3.3 Door and Window Design

- Windows and doors are key elements of any structure's form and should relate to the scale of the elevation on which they appear and the Architectural style of the building. Windows and doors can establish character by their rhythm and variety.
- Clear glass windows are encouraged unless they are not feasible due to the nature of functional use.
- The tinting of glass should be no more than 20% on all doors and windows. Highly reflective film is prohibited as the application can interfere with moving traffic.

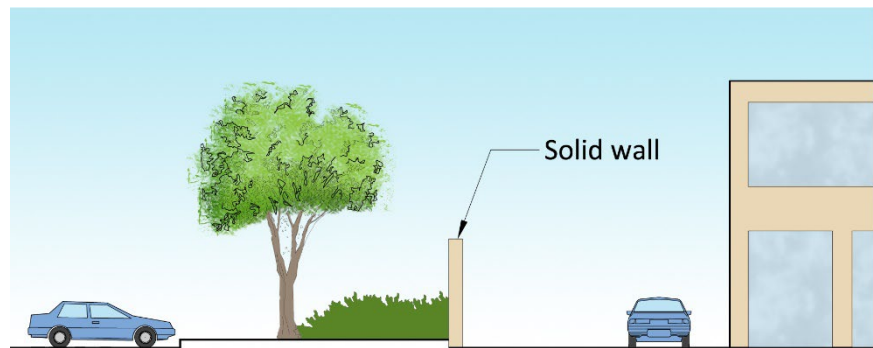
3.7.3.4 Awnings

- Awnings are permitted both on first as well as upper floors. Refer to Section 3.5.6 Shade Structure- Awnings in General Provisions for materials, design and maintenance guidelines.

3.7.3.5 On-site Screening

- Less visually aesthetic components necessary for industrial/commercial development such as loading areas, trash enclosures, mechanical equipment, and noise and odor producing functions should be located at the sides and/or rear of

Screening- Solid wall and Landscaping. SOURCE: Michael Baker International



main buildings, and screened with compatible architectural features and walls, and/or landscaping.

- Solid walls if necessary for screening should be no more than 6 feet high and made of solid masonry or concrete tilt-up with an

exterior finish compatible to the main structure. Landscaping should be used to reduce the starkness of such walls

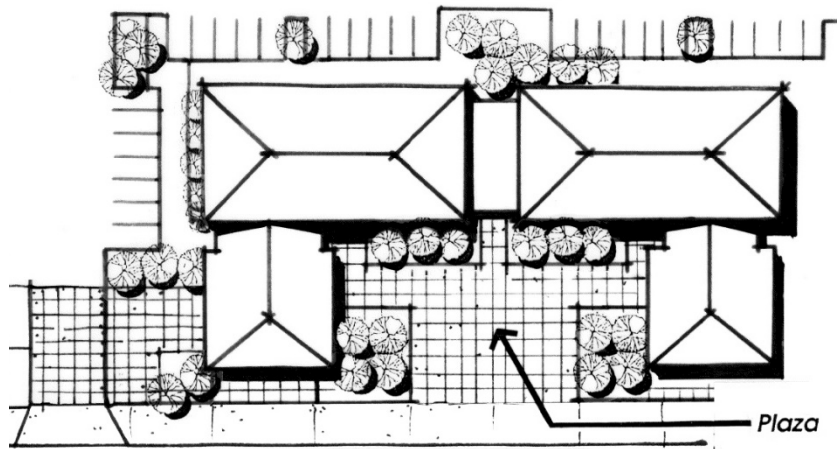
3.7.4 Off-Street Parking

- Parking lots should not be the dominant visual element of the site. Refer to Section 3.5.11 Off Street Parking guidelines in the General Provisions
- Developers are encouraged to locate parking on the sides or rear of the buildings.

3.7.5 Outdoor Gathering Areas

- Required setback areas are encouraged to be used as public open space, plazas, and other public amenities like sidewalks.
- The plazas are encouraged to be integrated with building design and to give an inviting feel using elements such as public art, landscaping, interesting shade structures, murals, arched walls creating gateways, and so on.

Plaza Integrated with Building. SOURCE: Michael Baker International



- Provision of seating in form of fixed furniture and seating walls is encouraged.
- Pedestrian walkways should be provided to connect building entrances with parking areas. It is strongly encouraged that these walkway areas provide pedestrian improvements including but not limited to shade structures, benches and furniture, and dining areas, if appropriate.
- Plazas, open spaces, and dining areas should be buffered from the high-speed traffic by a landscape buffer, architectural feature, or building.

Plaza Area Seating. SOURCE: Andy C



Pedestrian Walkways Connecting Parking to Entrance. SOURCE: Øyvind Holmstad



- Fences or barriers are encouraged to separate dining areas from open space and plazas where there is pedestrian traffic.
- Fences should be of good quality and made of wrought iron, steel, or wood. Other materials may be permitted if they are compatible with the architectural style of the adjacent building and the style of the dining furniture.

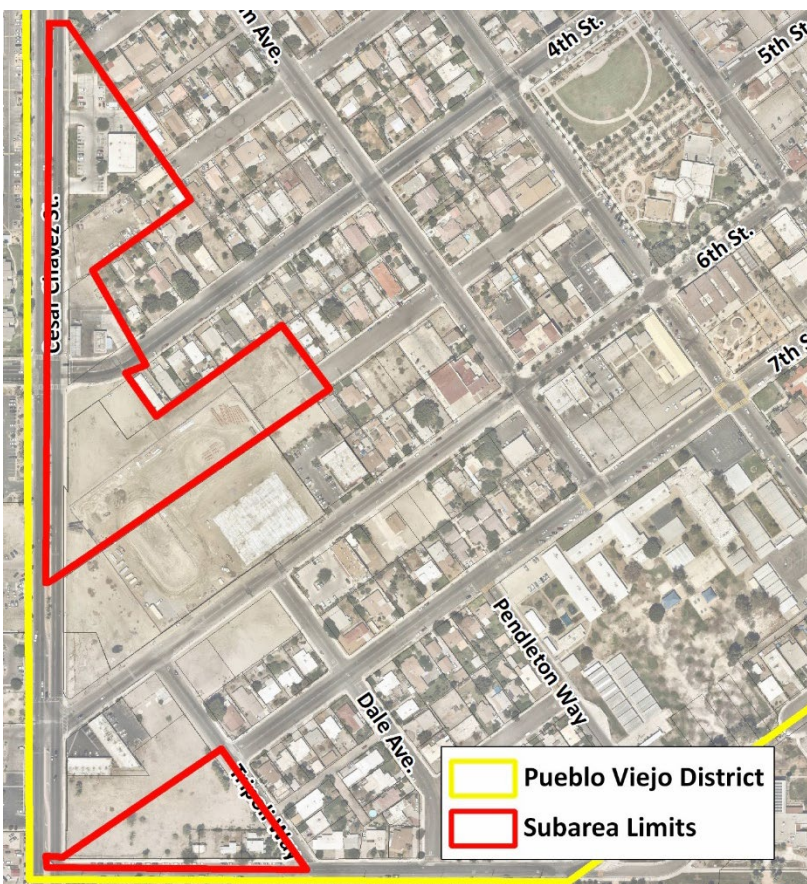
Plaza Space in Commercial Area. SOURCE: Payton Chung



3.8 CESAR CHAVEZ STREET SUBAREA

Cesar Chavez Street Subarea consist of parcels on the east side of Cesar Chavez Street as shown on Map D. The parcels on the southern side of the street are currently vacant while northern side houses auto-related uses such as gas station, AutoZone and so on. The rest of the corridor though not in the study area is developed in a conventional strip commercial style with single storied franchise stores and large expanse of parking. If unchecked the development of vacant parcels in this subarea as well as any redevelopment opportunity of existing built areas could follow the similar development patterns that is not conducive to pedestrian and bike traffic and will cater largely to automobile users. While uses along this fast-moving corridor are expected to remain primarily auto-centric, the areas are in immediate vicinity of Sixth Street and is positions for spillover effects and hence should be planned as a pedestrian and bike friendly corridor. Site design, massing, articulation, open space development and other placemaking elements play a role in creating such an environment that encourages pedestrian activity while catering to automobile users.

Map D Cesar Chavez Street Subarea Extents. SOURCE: Michael Baker International, Riverside County GIS



3.8.1 Existing Buildings

- Renovations and additions to existing structures in order to introduce new uses should be done appear as an originally conceived part of the design. New additions should match the current scale and proportions of the existing buildings. However, they need not replicate the existing structures. construction. Refer to Section 3.5.1 for guidelines related to existing buildings.

3.8.2 Site Design

3.8.2.1 Building Orientation and Setting

Buildings Oriented Towards Primary Street.
 SOURCE: Michael Baker International



- Primary façade of buildings should be oriented toward Cesar Chavez Street; Developers are encouraged to treat side facades visible from the street with similar design principles as they treat primary facade.
- Development design should not lead to creation of superblocks. A finer pedestrian friendly block design no greater than 400 feet in length is preferred. These internal routes should be designed to connect to existing street grid. Such blocks also provide increased street frontages on the internal through-routes of these centers create more walkable setting for new buildings and public open spaces.
- Internal streets should contain no more than two through-lanes and sidewalks leading to parking areas
- Buildings should not be located in a manner that make them appear like “islands” surrounded by paved areas. Shared parking concepts and developments encouraging “park-once-and-walk” experience is encouraged.

Orientation and Articulation. SOURCE Michael Baker International



- Organize buildings and the spaces between them to cultivate street life. The arrangement of the buildings should entice customers out into the public spaces between the buildings.
- Where surface parking is required for development feasibility, surface lots should be to the rear of primary building frontages

and configured to allow their conversion to structured parking as land values rise and development intensity increases. Walkways connecting these parking spaces to front of buildings can be doubled as open spaces or paseos that are strategically located to be visible from large portions of development as well as street.

- Developments should include usable and attractive outdoor open space (courtyards, plazas, shaded arcades and functional landscaped areas) located in strategic locations to attract and promote pedestrian activity. These locations can that connect parking to the front of the buildings and can also be used as outdoor dining areas, mini playground spaces and other activity generating uses.
- Mature trees and tree groupings, and significant vegetation should be preserved and incorporated into development’s usable open space where possible. Such features should be considered as strong site design determinants.

3.8.3 Architecture

3.8.3.1 Massing and Articulation

- At an intersection with a minor street, the side of the building facing the minor street should have a façade treatment.
- Other facades directly visible from the street should have façade treatment.
- The central portion of development as well as bookends should be designed to be visibly taller and denser than rest of the development as these are most visible from the street.

Dominant Central Portion and Bookends. SOURCE: Michael Baker International

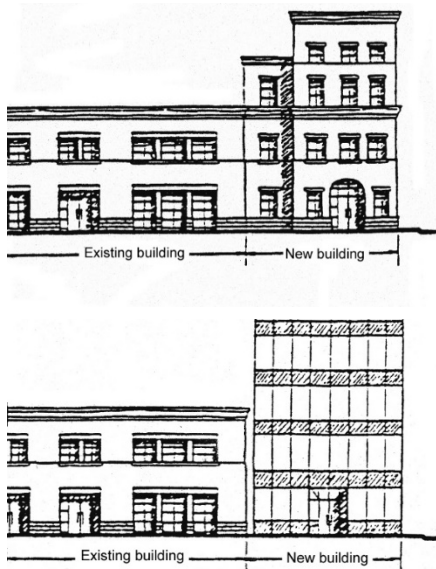


- Building design should avoid blank façades. A single, dominant building mass should not be used for larger buildings. Breaks in building mass should be used to provide visual relief for long building facades.
- In order to break the mass of a large store, consider expressing internal functions (i.e. bank, deli, and florist) as minor storefronts.

Articulation and Breaking the Facade. SOURCE: Michael Baker International



New Expansion to Compliment Old Structure. (Image on the top shows addition using same architectural style. Bottom image shows different style but follows the banding thus complementing the existing building architecture). SOURCE: Michael Baker International



Franchise store Architecture Using Prescribed Architectural Guidelines.



- Incorporate two (or more) entrances along the front of large stores. If two entrances are not possible, consider partially wrapping the front of a large store with smaller stores. Wall planes should not run in one continuous direction for more than 30 feet without an offset or some form of articulation dependent on architectural style of the building.
- Substantial variations at massing breaks should include changes in height and the horizontal plane.
- Changes in materials, textures and the utilization of other architectural enhancements by use of vertical or horizontal architectural elements based on architectural style of the building that help break massing and create interest for passersby are also encouraged.

Frequent Changes in Façade. SOURCE: Michael Baker International



- Murals and landscape screening are greatly encouraged as they help in breaking mass of the building.
- Expansions to existing buildings should provide for continuity between the old building and the new addition. It is not necessary to match the existing building but should include prominent design elements of the old building to provide architectural compatibility between old and new.
- When using architecture of a franchise business such as fast food, it should be compatible with the neighboring buildings and should integrate features from architectural styles prescribed in Section 3.3. These franchise businesses should not be standalone structures but a part of overall development and should incorporate the architectural style of the overall development in their design.
- Buildings that derive their image primarily from applied treatments that express corporate identity are discouraged.

3.8.3.2 Entry Orientation

- Primary building entries should be readily identifiable by the moving traffic and well defined through the use of projections, recesses, columns, roof structures, or other design elements based on the architectural style. The single massing break provided by the entry in long blank façade is not acceptable.
- Entries to the upper floors if used by different business offering should be placed on the front of the building.

3.8.3.3 Door and Window Design

- Windows and doors are key elements of any structure’s form and should relate to the scale of the elevation on which they appear. Windows and doors can establish character by their rhythm and variety.
- Windows set back from the exterior wall surface to create variety and shadows in the wall planes are encouraged though the architectural style used will dominate the appearance, rhythm and design of these elements.
- Clear glass windows are encouraged unless they are not feasible due to the nature of functional use.
- Reflective glazing is prohibited.

3.8.3.4 Shade Structures

3.8.3.4.1 Colonnades and Arcades

- Shade structures such as colonnades and arcades are encouraged as part of internal site design to provide shaded walkways within the development or from parking lots or structures to building entrances.
- Developers should explore the possibility of connecting different buildings in large developments using colonnades and arcades and integrate them with open spaces such as courtyards, plazas, outdoor dining spaces and so on.

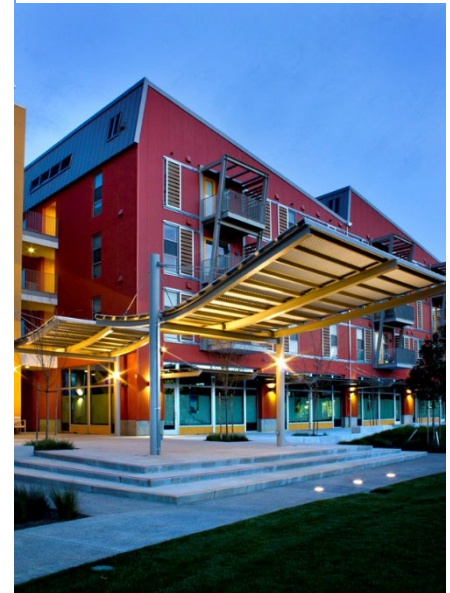
3.8.3.4.2 Awnings

- Awnings are permitted both on first as well as upper floors. Refer to Section 3.5.6 Shade Structure- Awnings in General Provisions for materials, design and maintenance guidelines.

3.8.3.5 On-site Screening

- Less visually aesthetic components necessary for large commercial developments such as loading areas, trash

Identifiable Entry. SOURCE: Michael Baker International



Awnings. SOURCE: Michael Baker International



enclosures, mechanical equipment, and noise and odor producing functions should be located at the sides and/or rear of main buildings, and screened with compatible architectural features and walls, and/or landscaping.

- Solid walls if necessary for screening should be no more than 6 feet high and made of solid masonry or concrete tilt-up with an exterior finish compatible to the main structure. Landscaping should be used to reduce the starkness of such walls

3.8.4 Off-Street Parking

- Parking lots should not be the dominant visual element of the site. Refer to Section 3.5.11 Off Street Parking Guidelines in the General Provisions
- Developers are encouraged to locate parking on the sides or rear of the buildings.

3.8.5 Outdoor Gathering Areas

- Required setback areas are encouraged to be used as public open space, plazas, and other public amenities like sidewalks.
- The plazas are encouraged to be integrated with building design and to give an inviting feel using elements such as public art, landscaping, interesting shade structures, murals, arched walls creating gateways, and so on.
- Provision of seating in form of fixed furniture and seating walls is encouraged.
- Pedestrian walkways should be provided to connect building entrances with parking areas. It is strongly encouraged that these walkway areas provide pedestrian improvements including but not limited to shade structures, benches and furniture, and dining areas, if appropriate.
- Plazas, open spaces, and dining areas should be buffered from the high-speed traffic by a landscape buffer, architectural feature, or building.
- Fences or barriers are encouraged to separate dining areas from open space and plazas where there is pedestrian traffic.
- Fences should be of good quality and made of wrought iron, steel, or wood. Other materials may be permitted if they are

Buffering Plaza Space. SOURCE Michael Baker International



compatible with the architectural style of the adjacent building and the style of the dining furniture.

Examples of Outdoor Gathering Areas. SOURCE: Michael Baker International



3.9 TRANSITION AREA

The Transition area comprises of parcels surrounding the Veterans’ park areas a block south of Seventh Street and east of Orchard Street. As the name suggest this is an areas in between residential and commercial areas. Design of site and structures are expected to integrate the surrounding characteristics of the land and the residential development pattern though used for commercial land uses. Most existing structures in this area are expected to remain with infill development occurring in the vacant parcels. The existing structures are expected to be converted to low traffic producing commercial uses as directed by zoning. The area is expected to have spillover effect from Sixth Street development with uses that are geared towards providing accommodations for tourists as well as locals and lighter commercial uses. The existing and new developments will have a view of newly redesigned Veterans’ Park and streetscapes. Similar to the Sixth Street Subarea, this area is envisioned as a pedestrian and bike friendly area. Horizontal and vertical mix of uses- residential, institutional, commercial, offices- is encouraged. However, building design should consider pedestrian scaled buildings and balancing it with need for privacy especially if there are residential uses on the ground floor. New buildings are expected to follow the architectural styles explained in Section 3.3. Other architectural styles other than the ones stated in the Section 3.3 may be allowed but subject more scrutiny and will require visualizations.

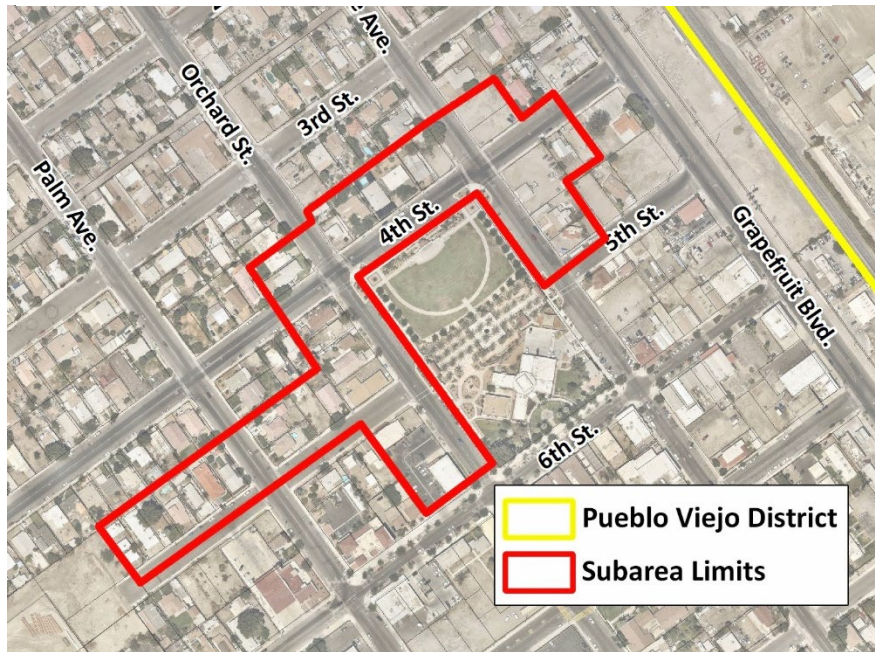
Existing Buildings Converted to Medical Office. SOURCE: Sardaka





Existing Buildings converted to artist's studio. SOURCE: Ildar Sagdejev



Map E Transition Area Subarea Extents. SOURCE: Michael Baker International, Riverside County GIS



	Pueblo Viejo District
	Subarea Limits

Existing Buildings Converted to Medical Office. SOURCE: Rept0n1x



Existing Building Converted to an Inn. SOURCE: Roger Kidd



3.9.1 Existing Buildings

- Any additions/ renovations to existing structures, conversion of uses to commercial, changes in parking area, and addition of signage should be done in a manner that it does not change the residential character of the area and compatibility with the surrounding neighborhood.
- Additions should respect the scale and massing of existing structure and should not become dominant part of the development.

Existing Buildings. SOURCE: Michael Baker International



- The architectural and articulation of new addition should draw from style of the existing building. However, they need not replicate the existing structures.
- Refer to Section 3.5.1 for additional guidelines related to existing buildings.

3.9.2 Site Design

3.9.2.1 Building Orientation, and Setting

- Building orientation facing the primary street is important for creating street appeal, enhancing aesthetics, and drainage considerations and encouraged.
- Parcels surrounding the park should be oriented to take maximum advantage of visual and physical access to the park.
- Buildings should use four-sided architecture, meaning all visible sides of the building should have façade treatment. However, the primary entry will be on the primary street.
- Open spaces should be oriented toward the street (see Section 3.6.4.1 and 3.6.4.2 in Outdoor Gathering Areas Section).

3.9.3 Architecture

3.9.3.1 Massing and Articulation

Due to spillover effects of Sixth Street development and existence of Veterans’ Park, pedestrian activity is expected in this area. Hence, the massing and articulation should be designed to appeal to the slower moving pedestrian traffic rather than vehicular traffic. Emphasis on detailing and use of quality materials will be important since pedestrians move slower than vehicular traffic and are more likely to observe articulation and smaller details.

- In order to retain pedestrians’ interest and make walking a pleasurable experience, buildings should incorporate changes in massing as well as articulation and detailing that are authentic representations of architectural styles. Changes in the architectural style, variation in materials and colors materials, horizontal and vertical articulation elements, framing around the windows and doors, changes rooflines and wall planes and so on break the monotony of the façade and create interest for pedestrians.
- All facades should emphasize three-dimensional detailing depending on the architectural style used. Some examples are use of elements such as cornices, window moldings and reveals to cast shadows and create visual interest on the facade.

- Tile artwork, plaques, decorative glass, and lighting fixtures are encouraged to provide visual relief to façades. These elements should reflect the building’s architectural style.
- Artwork and architectural relief integrated into building design is encouraged and should be appropriate to the setting.

3.9.3.2 *Entry Orientation*

- Primary entries should be on primary street. The entry should be accentuated to make it visible from the street depending on the architectural style.
- Direct and visible ground floor access to uses located in upper levels should be noticeable from the street and integrated with ground floor façade to highlight their location. Entries should be further emphasized if the uses are located on the front facade.

3.9.3.3 *Door and Window Design*

- Doors and Windows should be used as architectural elements that add relief to the facade and wall surface while being true to the architectural style.
- Commercial storefronts (if on ground floor) should include street-oriented display windows in order to engage pedestrians.
- Windows for residential uses on ground floor should be designed to provide privacy to the residents while creating a pedestrian scale environment. An example would be to provide windows above pedestrian sight level with articulation is provided at the pedestrian level.
- Exceptions to window covering requirements can be made where display windows (open and enclosed) are not feasible. In this case, exterior walls should be designed to provide architectural relief or be screened by landscaping and designed to provide pedestrian amenities such as wider sidewalks or benches.
- Clear glass windows are encouraged unless they are not feasible due to the nature of functional use.
- Clear glass windows are encouraged especially for retail use to promote interaction between the pedestrians and the businesses. Highly reflective window film is prohibited as they can interfere with moving traffic.

3.9.3.4 *Porches, Balconies, and Decks*

3.9.3.4.1 Porches

- Porches are encouraged and should be designed to be consistent with the architectural style of the building to which they are attached and provide sufficient depth and width to be occupiable spaces.

Porch Example. SOURCE: Stalane



3.9.3.4.2 Balconies

- Window balconies if compatible with architectural style are encouraged at the upper floors. Faux balconies with windows are acceptable provided they integrate features from the architectural style of the building.
- Where possible, balconies should be oriented towards the street/ park area and not towards internal block or parking areas.

Deck Example. SOURCE: Acabashi



3.9.3.4.3 Decks

- Decks should be designed to be aesthetically unobtrusive and should provide sufficient depth and width to be occupiable spaces.
- The parapet and other articulation of the deck structure should draw from the architectural style of the building.

3.9.3.5 Awnings and Canopies

- Awnings and canopies may be used on first floor as well as upper floors of the building. Refer to Section 3.5.6 Shade Structure-Awnings in General Provisions for materials, design and maintenance guidelines.

3.9.4 Outdoor Gathering Areas

3.9.4.1 Plazas, Open Spaces, Gathering Areas

Due to transitional and residential nature of the area large plazas or paseos are not expected in this area. Rather front yard space converted to small plaza, open space or gathering areas is envisioned.

- Required setback areas are encouraged to be used as public open space, plazas, and other public amenities like sidewalks.
- Open spaces, gathering area and dining areas should be buffered from the high-speed traffic by a landscape buffer, architectural feature, or building.
- Pedestrian walkways should be provided to connect building entrances with sidewalks. These should be separate from vehicular access.
- Attractive landscaping and use of elements such as water fountains, art work, playground equipment and so on make smaller gathering spaces lively and are encouraged. However, installation of such elements should respect the scale of the building.
- Perimeter fencing separating sidewalks from gathering areas is generally discouraged unless used for outdoor dining areas.

3.9.4.2 Sidewalk Dining Areas Fences and Barriers

- If open spaces oriented towards sidewalks are used for dining, fencing or barriers should be provided to separate dining areas from sidewalks. Refer to Section 3.6.4.3 under Sixth Street Subarea for guidelines related to Sidewalk Dining Area Fences and Barriers

3.10 SIGNS AND PUBLIC ART

3.10.1 Signs

Signs are important element of commercial/ mixed use corridors and play an important role in establishing the visual character of a place. They are vital component for many businesses because they serve as the primary identification tool for an establishment and often advertise goods or services that businesses may provide. However, a sign that has a cluttered look will undermine the character of the building and overall street appearance, while clean, attractive, and aesthetically pleasing signage can give a unique identity to the street and contribute to the overall visual appeal of the area. The general guidelines in this section provide guidelines that are applicable to all sign type that are further detailed in this section. A matrix is also provided that suggests permitted sign type by subarea.

3.10.1.1 General Sign Guidelines

- Repetitious signage should be avoided.
- No more than three signs are allowed for each building.
- The signs should not be placed closer together than 5 feet. A-frame signs can be placed closer than 5 feet from another sign for the same business.
- The design of each sign should incorporate a maximum of four colors. All signs of a single business should be of a similar color palette.

3.10.1.1.1 Sign Message

- The primary sign should only display the business's name. Secondary signs may include information regarding products provided.
- Sign message should be aesthetical pleasing and should not give a cluttered appearance.

3.10.1.1.2 Sign Lighting

- Any external spot or flood lighting should be directed at the sign and away from pedestrian walkways and/or roadways. Lighting should not create a glare for motorists or pedestrians.
- External spot or flood lighting fixtures should be simple in design, should blend into the appearance of the sign, and should not obscure the sign's graphics.

- Back-lit signs are discouraged unless back lighting is used for individually cut letters
- Halo-lit illumination or reverse channel letters are encouraged.
- Cabinet lit signs are prohibited.
- Blinking or flashing lights are prohibited.
- All mechanical equipment for the lighting of a sign should be hidden from view.
- Where signs face residential areas, lighting should be minimal.

Backlit Lighting for Individual Letters.
SOURCE: Nina Stössinger



3.10.1.2 Sign Types by Subarea

The following text describes the various types of signs that can be used in each subarea. However, the choice of sign type should be based on the subarea’s character, land uses, traffic, and the audience the sign attracts. For example, Sixth Street is expected to be transformed into a pedestrian-friendly environment with restaurants and ground-floor retail. Signs in this subarea should cater to slower-moving traffic and even slower pedestrians that will appreciate detailing and articulation. As such, a pedestrian scale is appropriate for projecting signs, hanging signs, and other sign types. In contrast, motorists in the faster-moving vehicles on Grapefruit Boulevard or Cesar Chavez Street appreciate larger signs with illumination and larger lettering that makes signs easy to read while driving. While other pedestrian-level signs will give character to these two subareas, the signs may not be as frequent as those seen on Sixth Street. Table AA shows various signs and the suggested subareas in which they may be used.

Table AA: Sign type by subarea

Sign Type	Suggested Subarea (Frequent Use)	Remarks
Projecting	<ul style="list-style-type: none"> ■ Sixth Street Subarea ■ Transition Area ■ Grapefruit Boulevard 	These signs are to be used frequently in the Sixth Street Subarea and in the Transition Area, both of which are expected to attract pedestrians. Projecting signs can be used by businesses along Grapefruit Boulevard that may cater to pedestrian traffic.
Hanging	<ul style="list-style-type: none"> ■ Sixth Street Subarea ■ Transition Area ■ Grapefruit Boulevard 	These signs are to be used frequently in the Sixth Street Subarea and in the Transition Area, both of which are expected to attract pedestrians and have shade structures.

<p>Window</p>	<ul style="list-style-type: none"> ■ Sixth Street Subarea ■ Transition Area ■ Grapefruit Boulevard ■ Cesar Chavez Street 	<p>Window signs cater to pedestrian traffic and are expected to be used in pedestrian-oriented areas such as the Sixth Street Subarea and the Transition Area. They are expected to be used less frequently along Grapefruit Boulevard and Cesar Chavez Street.</p>
<p>A-Frame Sign</p>	<ul style="list-style-type: none"> ■ Sixth Street Subarea ■ Transition Area 	<p>Window signs cater to pedestrian traffic and are expected to be used in pedestrian-oriented areas such as the Sixth Street Subarea and the Transition Area. They are not expected to be used on Grapefruit Boulevard and Cesar Chavez Street.</p>
<p>Wall Sign</p>	<ul style="list-style-type: none"> ■ Transition Area ■ Grapefruit Boulevard ■ Cesar Chavez Street 	<p>These signs are effective in areas with faster-moving traffic where the signs are to be read in a relatively shorter amount of time. Larger signs with bigger text facilitate quick reading and are to be used along Grapefruit Boulevard and Cesar Chavez Street. They can be used in the Transition Area though are not encouraged. Wall signs are not to be used in the Sixth Street Subarea.</p>
<p>Monument Sign</p>	<ul style="list-style-type: none"> ■ Grapefruit Boulevard ■ Cesar Chavez Street 	<p>These signs are effective in areas with faster-moving traffic where the signs are to be read in a relatively shorter amount of time. Larger signs with bigger text facilitate quick reading and are to be used along Grapefruit Boulevard and Cesar Chavez Street. Monument signs are not to be used in the Sixth Street Subarea and the Transition Area.</p>

3.10.1.3 Sign Types

This section describes various sign types and includes guidelines for their use.

3.10.1.3.1 Projecting Signs

Projecting signs are attached to a building façade and project at a 90-degree angle from the face of the building. Projecting signs should be oriented toward pedestrians and should only be used when a shade structure is not located on the building’s façade. Along Grapefruit Boulevard, projecting signs should only be used along pedestrian walkways that connect parking areas with building entrances and on storefronts adjacent to plazas or open space.

- Projecting signs should be flat in nature, with signage details embossed into or projecting slightly from the sign material.
- Sign design, supports, and brackets should be compatible with the architectural style of the building from which the sign projects.
- Projecting signs should be mounted to the ground-floor façade, with a vertical clearance of 8 feet.
- Signs should be of a visually interesting design that may include irregular outlines, internal cutouts, and/or two- or three-dimensional symbols or icons.
- Projecting signs (excluding supports or brackets) should fit within a rectangle with a maximum area of 6 square feet.
- A minimum clearance of 12 inches is required between the building façade and the beginning of the sign, with a minimum projection of 36 inches.

3.10.1.3.2 Hanging Signs

Hanging signs are attached to the underside of a shade structure (for example, a colonnade, arcade, or canopy) and should be placed at a 90-degree angle from the building façade. Hanging signs should be oriented toward pedestrians and should only be used under shade structures. If shade structures are included along pedestrian walkways between parking areas and building entrances, hanging signs may be appropriate for businesses along Grapefruit Boulevard.

- Hanging signs should not project farther than the shade structure to which they are attached.
- Hanging signs should not be attached to shade structures whose height is greater than that of the ground floor.
- A minimum vertical clearance of 8 feet is required.

Projecting Signs. SOURCE: Michael Baker International



Hanging Signs. SOURCE: Michael Baker International



- Hanging signs (excluding supports or hangers) should fit within a rectangle with a maximum size of 4 square feet.
- A minimum clearance of 12 inches is required between the building façade and the beginning of the sign, with a minimum projection of 36 inches.
- Signs should be of a visually interesting design that may include irregular outlines, internal cutouts, and/or two- or three-dimensional symbols or icons.

3.10.1.3.3 Window Signs

Window Sign. SOURCES: Top- Michael Baker International; Bottom- Brewbooks from near Seattle



Window signs are signs that are painted, etched, or adhered to windows or doors within the façade of a building. These signs can be only text or a combination of text with graphic items.

- Window signs should not occupy more than 15 percent of the available window area of a building façade.
- Sign text should not be more than 8 inches in height.
- Window signs should be applied directly to the interior of the window or hung on the inside of the window as close to the glass as possible. Any blank space should be transparent.
- Window signs should be made of high-quality materials, including but not limited to paint, gold leaf, and/or neon. Etching or sandblasting are also appropriate methods of window sign installation.
- Window signs and associated graphics should complement the architectural style of the building and the design of the storefront interior.
- Neon signs should only be hung in windows.

3.10.1.3.4 A-Frame Signs

A-frame signs are temporary signs that stand on their own and are usually placed on the sidewalk in front of the business they advertise.

- A-frame signs should be made of durable, sturdy materials, including but not limited to wood or metal, with open bases that can withstand a variety of weather conditions and will not be easily sun-bleached.
- Glass, breakable materials, paper or laminated paper, PVC pipes, or illumination are prohibited for A-frame signs.
- Signs should be designed to look organized, with minimal text and large graphic elements.

- A-frame signs should be easily movable and should not be installed permanently.
- A-frame signs should have a maximum sign area of 8 square feet and can have a maximum of two sides of this size.
- All text on A-frame signs should be between 2 and 4 inches high. Text associated with the business’s logo should be exempted from this guideline.
- Rectangular A-frame signs should be no more than 4 feet in height and 2 feet in width. These signs should have an open base.
- Shaped silhouette A-frame signs should be no more than 5 feet in height and 3 feet in width. These signs should not exceed the maximum sign area of 8 square feet.
- A-frame signs should be placed on the sidewalk in such a way that 5 feet of sidewalk area is still available and doorway access is not impeded.
- A-frame signs should be placed only along the business’s frontage.
- A-frame signs should be placed on the sidewalk no sooner than half an hour before the business’s posted opening time and should be taken in from the sidewalk no later than half an hour after the business’s posted closing time.
- A-frame signs should be placed along the curb on the street side of the sidewalk.
- Each business should only have one A-frame sign.

A-frame Signs. SOURCE: Infrogmation



3.10.1.3.5 Wall Signs

Wall signs are attached directly to the façade of a building and are of a three-dimensional shape.

Wall Signs. SOURCE: Michael Baker International

- Wall signs should project no more than 12 inches from the wall of the façade to which they are attached.
- Wall signs should not extend beyond the edges of the building façade to which they are attached and should not cover doorways or windows.
- Wall signs should not cover more than 15 percent of the building façade.
- Wall signs should complement the architectural style of the building, in style and proportion.
- Signs should be designed to provide shadow relief and a well-designed substantial appearance.



3.10.1.3.6 Monument Signs

Monument signs are freestanding signs that primarily draw the attention of motorists. These signs are often used for developments in which the main building is set back from the street right-of-way.

Monumental Sign. SOURCE: Dj1997



Monumental Sign. SOURCE: ReptOn1



- Monument signs should be installed on two posts or a substantial base.
- Monument signs should be designed to be architecturally compatible with the building for which they provide signage.
- The base should consist of stone, brick, stucco, or other sturdy, permanent, and durable material and should be at least 18 inches in height.
- The sign face should consist of durable materials, including but not limited to metal, aluminum, and/or wood.
- Landscaping should be integrated into the sign and should surround the entirety of the base. The landscaping should be designed to ensure sign legibility for the life of the sign.
- Monument signs should avoid identifying multiple tenants. However, monument signs should be no closer than 100 feet from each other. If lot width and building design do not permit multiple monument signs on the property, the sign should be designed to incorporate the development’s anchor tenants.
- Monument signs should be externally lit. Internally lit monument signs are prohibited. In the case of neon, exceptions may be made.
- Site addresses should appear on the monument signs and will not count as part of the sign area. Addresses should be no less than 9 inches in height.
- Monument signs should be perpendicular to the adjacent street and sidewalk and should have not more than two parallel sign faces.
- Monument sign face should be no more than 50 square feet. The height of the sign’s side should be no more than 6 feet.

3.10.2 **Public Art**

Public art refers to murals, statuary, and installations in public realm and areas of private realm accessible to general public. Public art projects come in many forms and can be permanent or temporary art installations with varied viewpoints. They create a vibrant environment and give a uniquely identity and placemaking aspect to an area. However, establishing clearly defined criteria for art installations is necessary to avoid conflicts and tensions between various sects of people using a place

and providing a cohesive look. Along with artwork itself, its siting and maintenance should as be clearly defined.

Murals are categorized as public art and are an important part of the urban fabric of Pueblo Viejo. They represent not only the residents of Coachella but also the zeitgeist of the time at which they were installed. New development offers an opportunity to plan for mural locations in a way that will complement the intended architectural style of the buildings. Murals should be consistent with Chapter 5.70, Art Murals, of the City of Coachella’s Municipal Code.

3.10.2.1 Site Selection and Placement

- Public art is encouraged to be installed in areas experiencing or expected to experience high levels of pedestrian traffic such as sidewalks and plaza areas or places that create entrance to the area such as medians, and circles.
- Public art should be easily visible and accessible to the public and serve as gathering places for people
- Public art should be part of the City's circulation system and enhance the overall public environment and pedestrian streetscape experience
- Public art should establish landmarks and neighborhood gateways.
- The placement of public art should not block entrances, windows, signage, and pedestrian circulation.
- The placement of public art should not interfere with the line of sight of the oncoming traffic

3.10.2.2 Content

- Public art should not include commercial signage, logos, or advertising. However, they may include dedications.
- Public art projects are encouraged to be diverse and derive content from local history, community, geography, and environment.
- Public art should be expected to integrate into the overall design of the larger
- context in which it is placed. It should not be conceived as an afterthought.

Public Art in Sidewalk Area. SOURCE: Géza Stremeny (sculpture) / Yoav Dothan (photo)



Temporary Art Installation. SOURCE: Martin Falbisoner



Public Art in Plaza Area. SOURCE: Beyond My Ken



3.10.2.3 Materials and Construction

- Public Art should be constructed with durable materials that will withstand the elements and can be washed to remove dirt and graffiti.
- Public art projects are encouraged to be made of environmentally sustainable materials.
- Temporary installations may be allowed for no more than two weeks, using non-durable materials based on with City's approval. However, such installations should be firmly anchored to ground or structure depending on the type of installation. The City may ask for its removal prior to time limit if it suffers significant damage/ deterioration.

3.10.2.4 Maintenance

- The maintenance of public art on private property will the responsibility of property owner.
- The artwork should be presentable at all times and devoid of unpleasant look due to conditions such as discoloration, rust, and so on.

Existing Mural in Pueblo Viejo District. SOURCE: Michael Baker International



4 PUBLIC REALM DESIGN GUIDELINES

4

The public realm refers to the street right-of-way and includes sidewalk space and roadway space. Planners and designers are focusing on a variety of ways to integrate multiple modes of travel—not just motorized vehicles—with the land uses they serve. Alongside traditional transportation by car, new developments are rediscovering multimodal transportation by improving transit, pedestrian, and bike connectivity that lead to amenity-rich, walkable, bikeable, and sustainable communities. These approaches are designed to provide transportation choice, reduce air pollution and other environmental impacts, enhance public health, and support amenities. The public realm guidelines concentrate on the commercial/mixed-use streets in the Pueblo Viejo District although traffic calming features are suggested for the residential streets. The chapter begins with explanation of streetscape concepts for each of the commercial street along with an illustrative section. This section is followed with General Guidelines that are applicable to all commercial streets. Traffic Calming section lays out various ways of calming traffic in both commercial and residential areas of the District and Landscaping Guidelines provides plant palette for various streets.

4.1 STREETScape CONCEPTS

In 2015, the City implemented new streetscape design along three blocks of Sixth Street from Palm Avenue to Grapefruit Boulevard. This streetscape will inspire the streetscape design on the remaining commercial streets. However, all streets are different and should be designed per the space available, context, pedestrian activity, and land uses to which they cater. Each street should have its own distinct character, though some elements such as street furnishings, lighting, and signage act as tying elements that give uniformity and order to the whole District. Streetscapes should continue the design principles, materials, planting, lighting, furnishings, and general streetscape design per the newly built streetscape on Sixth Street.

While detailed streetscapes are to be designed as and when the City is ready to implement these projects, shown below are the typical streetscapes that are suggested to create a multimodal public realm along the commercial/mixed-use streets of Pueblo Viejo.

- Sixth Street, from Grapefruit Boulevard to Cesar Chavez Street
- Fifth Street, from Grapefruit Boulevard to Cesar Chavez Street
- Seventh Street, from Grapefruit Boulevard to Tripoli Avenue

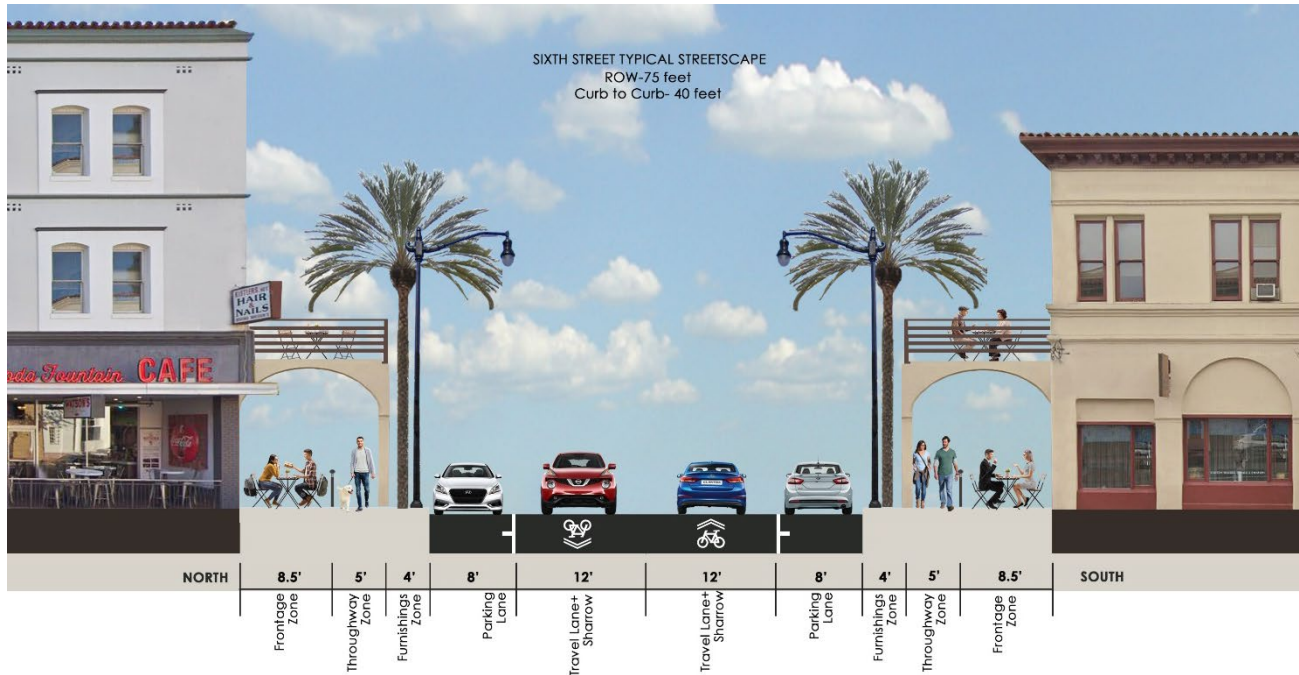
- Grapefruit Boulevard, from Bagdad Avenue to Park Lane
- Cesar Chavez Street, from Bagdad Avenue to Park Lane
- Orchard Street, from Sixth Street to Fourth Street
- Vine Avenue, from Sixth Street to Fourth Street

4.1.1 Sixth Street

From Grapefruit Boulevard to Cesar Chavez Street

The streetscape of Sixth Street will be a continuation of the recently implemented streetscape in the three blocks of Sixth Street from Grapefruit boulevard to Palm Avenue. As depicted in the street section below, it consists of one lane on each side of the street and parking lane. Due to restriction of space and need for enhanced pedestrian area, a shared bicycle facility is suggested on this street. The sidewalk area consists of a 4 feet Furnishings Zone that will accommodate plantings, light poles, bike racks, way finding maps, and other utilities. As an urban corridor the Furnishings Zone is preferred to be hard paved with tree grates and planter pots and can be used as an extension of sidewalk area (Throughway Zone). Dependent on City’s permission this area can also be for outdoor dining space. The Throughway Zone and the Frontage Zone (spillover space between 5 feet clear walking space and parcel line) are approximately 13.5 feet wide and shaded with a colonnade/ Arcade structure. Frontage Zone will house street furniture (sidewalk dining, benches and trash cans).

Sixth Street Streetscape. Source: Michael Baker International



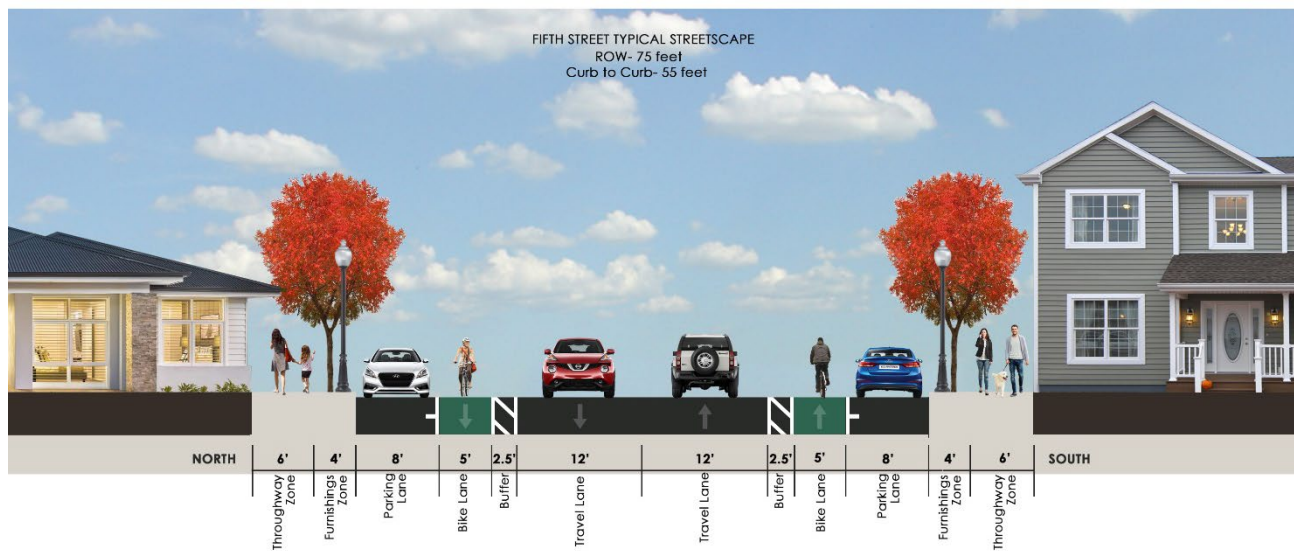
The private realm guidelines will help in making walking a pleasurable experience for pedestrians but details in public realm such as treatment of sidewalk with inlays, markings, and patterns in paving can add to this experience. The place making features such as plazas or gathering spaces that may a part of public realm will follow guidelines listed under this category in private realm as applicable (see Section 3.6.4). Traffic calming and landscaping on this pedestrian oriented corridor will play a large part in attracting foot traffic.

4.1.2 Fifth Street

From Grapefruit Boulevard to Cesar Chavez Street

Fifth Street is a part of Sixth Street Subarea and a parallel street to Sixth Street to the north. The street is expected to see spillover effects of development along Sixth Street on the south side of the Street and have transitional land uses on the north side. The street is also expected to cut through the development in Block XX and intersect with the Cesar Chavez Boulevard. While there will be pedestrian activity, it is not expected to be as intense as Sixth Street leading to minimum area (4 feet) for Furnishings Zone and a comfortable Throughway and Frontage Zone (approximately 6 feet). The proposed roadway has one travel lane in each direction, a buffered bike lane, and on-street parallel parking. Being a parallel street to Sixth Street and having ample right of way, this street is a good candidate for a bike facility. Hence a buffered bike lane is proposed.

Fifth Street Streetscape. Source: Michael Baker International

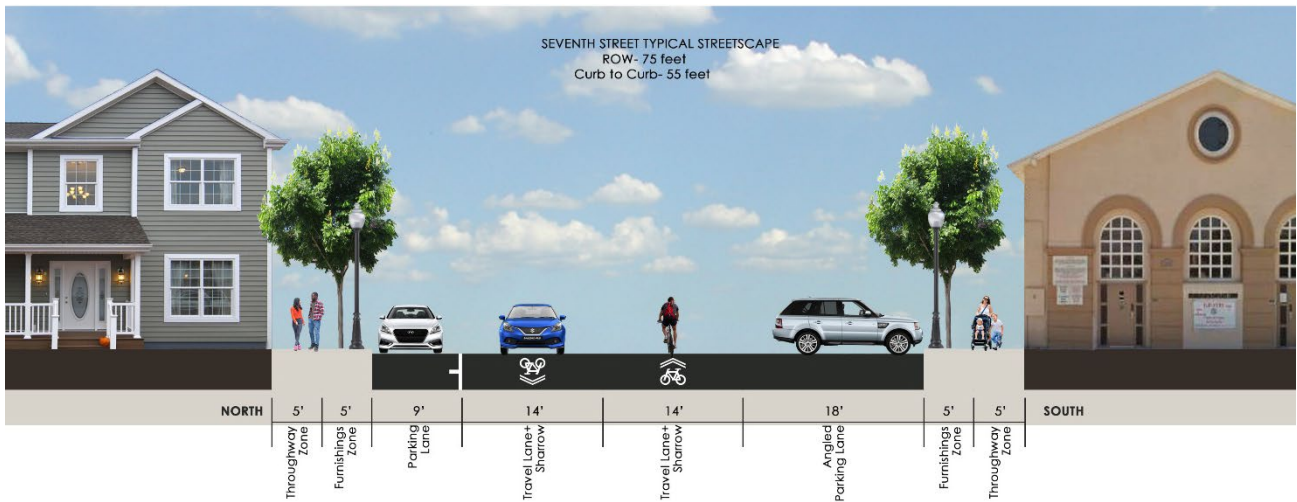


4.1.3 Seventh Street

From Grapefruit Boulevard to Tripoli Avenue

The Seventh Street is also a part of Sixth Street Subarea and a parallel street to Sixth Street towards the southside. The street is also expected to see spillover effects of development along Sixth Street in certain blocks. The school block and residential uses on the south side of the street are not expected to change. The configuration of Street includes 5 feet Furnishings Zone and 5 feet of Throughway and Frontage Zone. The ample right of way allows for accommodation of angled parking on south side and parallel parking on the north side of the street. In addition to parking, the proposed facility includes one travel lane with Sharrows to accommodate bikes.

Seventh Street Streetscape. Source: Michael Baker International



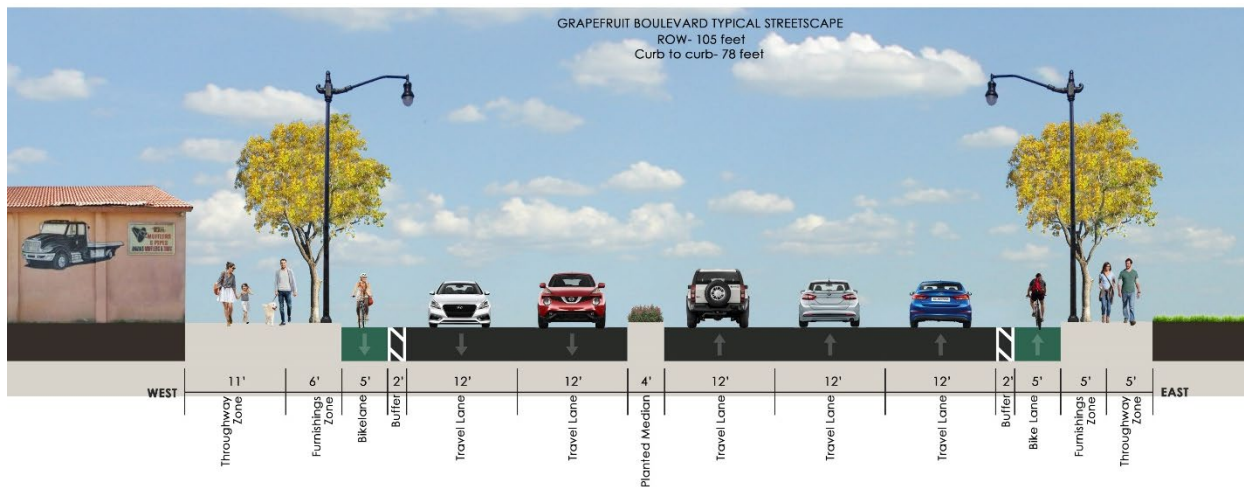
4.1.4 Grapefruit Boulevard

From Bagdad Avenue to Park Lane

Grapefruit Boulevard is a State Route 111, with a speed limit of 45 mph and provides connection to State Route 86. It caters to commercial and light industrial uses in the Pueblo Viejo District and it is expected to be that way in the future. The east side of the street has vacant land bordered by railway tracks. Currently, there is one travel lane in each direction, on street parking and a turn lane. Though being a gateway to the City and to the Pueblo Viejo District, the street does not have an appeal and does not look unique to Coachella. It has ample right of way and is in need of an upliftment.

Based on Cities current General Plan Update’s Mobility Element, Grapefruit Boulevard falls in “Primary Arterial with Enhanced Bicycle Facilities” category. The suggested configuration calls for two 12 feet lanes in north side of the street and three 12 feet lanes on the south side of the street. There is no street parking provided on this street. The proposed concept also calls for buffered bike lanes in both directions. A 4 feet planted median is also proposed. It will not only create an aesthetically pleasing corridor with a gateway effect but will also help with access management and traffic flow. The walkway area in the north is larger than the south side of the corridor as it is expected to get more

Grapefruit Boulevard Streetscape. Source: Michael Baker International



pedestrian activity.

4.1.5 Cesar Chavez Street

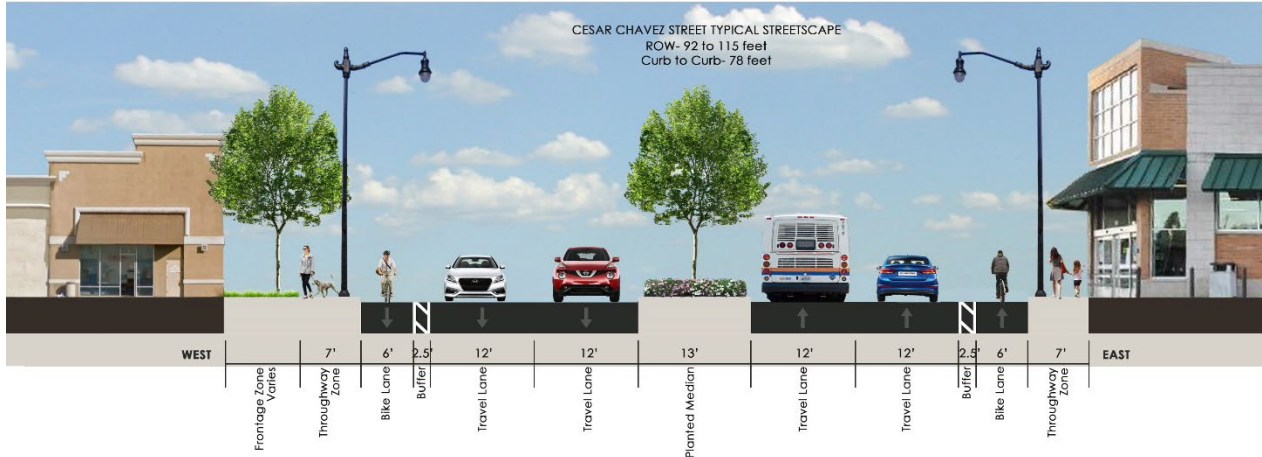
From Bagdad Avenue to Park Lane

Cesar Chavez Street is a commercial corridor with big box stores, strip malls, apartment complex, and other commercial uses. The vacant land at the intersection with Sixth Street is proposed to house a transit center. Currently, there are two lanes in each direction and a planted median in parts of the corridor and turn lane in the rest. While there are pedestrian facilities, the streetscape design does not encourage walking. Being a major gateway to the District the street will need upliftment and incorporation of multimodal facilities.

The proposed configuration has two 12 feet travel lanes in each direction, no parking, a 13 feet wide planted median, and a buffered bike lane. The Throughway/furnishing zone has a combined 7 feet of space based on the existing curb lines restrictions leaving no room for street trees. However, smaller potted plant arrangements can enhance the aesthetics of the

walkway. The Frontage Zone varies in the North based on the development and parcel lines. This space can be used for planting trees and other placemaking opportunities such as installation of gateway features, art and so on. The planted median will also give much needed gateway effect.

Cesar Chavez Street Streetscape. Source: Michael Baker International



4.1.6 Orchard Street

From Sixth Street to Fourth Street

The Orchard Street has recently been reconfigured with angled parking on the park side and parallel parking on the other side of the Street. It has one lane in each direction and enhanced pedestrian realm. However, the street lacks bicycle facility. The Street has right of way to accommodate bicycle lanes or can be marked as a share the road facility by sharrow markings on the street. This will provide bike connections from the Fifth street and to the Park.

4.1.7 Vine Avenue

From Sixth Street to Fourth Street

Vine Avenue has recently been reconfigured similar to the Orchard Street and has no bike facilities. It too has right of way to accommodate bicycle lanes or can be marked as a share the road facility by sharrow markings on the street to provide bike connections from the Fifth street and to the Park.

4.2 GENERAL GUIDELINES

4.2.1 Active Living Components

Active living components—pedestrian, bicycle, and transit facilities—should be integral to the streetscape design in Pueblo Viejo District.

4.2.1.1 Pedestrian Facilities

Walking is the most basic form of transportation. The creation of an attractive and safe pedestrian environment is a critical part of developing more livable communities. Pedestrian facilities should be safe and accessible to all types of users, connect to places where people want to go, encourage interaction, be attractive and of pedestrian scale, easy to use, economical to build, and easily maintainable. The main forms of pedestrian facilities are sidewalks and trails including street crossings and curb ramps. Enhancements include pedestrian signals (including countdown signals and lights embedded in crosswalks), raised crosswalks, and street furniture, lighting, and landscaping.

Sidewalks

Connectivity between buildings, land use areas, and adjacent development, both existing and new, can be achieved through the site-sensitive design of pedestrian areas such as sidewalks. These elements may be integrated into the street design and cohesively designed to connect residential, office and retail/commercial areas, and transit stops.

Sidewalks are divided into three zones:

1. **Furnishings Zone**
 2. **Throughway Zone**
 3. **Frontage Zone**
- The **Furnishings Zone** is defined as the area of the sidewalk appropriate for planting trees and the placement of street furniture, bike racks, trash receptacles, and light poles.
 - The width of Furnishings Zone is to be a minimum of 6 feet.
 - See related guidelines for landscaping, active transportation, lighting, and street furniture in this section.
 - The **Throughway Zone** is defined as the area of the sidewalk that is utilized for moving pedestrians along the corridor.
 - The width of Throughway Zone is to be a minimum of 5 feet.
 - Throughway Zone must be clear without any obstacles.

- The Throughway Zone should be covered with a colonnade or arcade structure on Sixth Street.
 - A shaded structure is an optional for the rest of the streets in Pueblo Viejo District.
- The **Frontage Zone** is defined as the area of the sidewalk between the property line and the Throughway Zone.
 - The width of this area varies and may be combined with setbacks to provide additional area for outdoor activities.
 - Frontage Zone may be used in a variety of ways such as outdoor dining areas, locations for public art, fountains, planter boxes, or for pedestrian space for window shopping, festive decorations, or small kiosk areas.

Sidewalk Zones. SOURCE Michael Baker International



- All sidewalks should be accessible pursuant to ADA standards.
- Ramps at corners, midblock, or at driveways should be pursuant to the California Department of Transportation’s (Caltrans) ADA standards.
- Truncated domes (dark gray color) should be used at the ramps per Caltrans’ standards.

- The transition between ramps and gutters to the road surface should be smooth.
- Curb ramps are required at every corner of the intersection.

Materials and Construction

- Sidewalks should be constructed of concrete that meet City standards.
- Any color addition to the concrete must approved by the City.
- Sidewalks should be constructed to promote the safety of the users, meaning they should be smooth and free of uneven surfaces that may cause a pedestrian to trip and fall.
- Elements such as engravings, inlays, embeddings, mosaic work, and other types of public art elements are encouraged to project the unique character of the community. These are placemaking elements and create interest for pedestrians. However, these elements should not cause obstructions to ADA accessibility or other safety concerns and should be approved by the City.
- Sidewalks should be built with appropriately spaced expansion and control joints so as to not cause cracking in the concrete.
- If used, tree grates should be flush with the sidewalks.

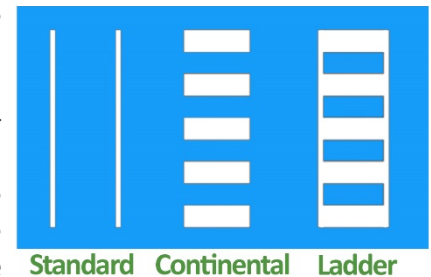
Public Art and Pavers. SOURCE: Jeangagnon



Pedestrian and Bicycle Crossings

- Marked crosswalks are most effective when they can be identified easily by motorists and pedestrians. They should present a visual contrast with the surface of the street. Marked crosswalks may be provided at all major-minor and minor-minor intersections as well as at midblock crossings.
- The use of longitudinal stripes in addition to or in place of the standard transverse markings can significantly increase the visibility of a marked crosswalk to oncoming traffic and are encouraged.
- Thermoplastic pavement marking material should be used for pavement markings including crosswalks.
- Decorative crosswalks typically made with bricks or other specialty paving are encouraged. They make the crosswalk more visible to oncoming traffic. These should be provided at all major intersections such as Sixth Street intersections with Grapefruit Boulevard and Cesar Chavez Street. Decorative crosswalks may also be used at other intersections and midblock crossings. The FHWA Memorandum “MUTCD - Official Ruling 3(09)-24(l) – Application of Colored Pavement” dated August 15, 2013 provides the following clarification on acceptable decorative treatment at marked crosswalks and should be followed in Pueblo Viejo District:

Marked Crosswalks. SOURCE: Michael Baker International



Decorative Crosswalk. SOURCE: Piotrus



“Examples of acceptable treatments include brick lattice patterns, paving bricks, paving stones, setts, cobbles, or other resources designed to simulate such paving. Acceptable colors for these materials would be red, rust, brown, burgundy, clay, tan or similar earth tone equivalents. All elements of pattern and color for these treatments are to be uniform, consistent, repetitive, and expected so as not to be a source of distraction. No element of the aesthetic interior treatment is to be random or unsystematic. No element of the aesthetic interior treatment can implement pictographs, symbols, multiple color arrangements, etc., or can otherwise attempt to communicate with any roadway user.”

Raised Crosswalk. SOURCE: Richard Drdul



- Raised crosswalks are encouraged to be used at locations where speeding is prevalent.
- Midblock crossings should be highly visible and installed in areas with large neighborhood blocks, typically more than 400 feet. Use of frequent midblock crossings can disrupt traffic flow and create safety concerns for pedestrians.
- A refuge island is a cut in a median island or median (refer to Section 4.3 Traffic Calming Features) to provide refuge for pedestrians and bicyclists. It is an effective tool in increasing overall comfort and facilitates pedestrian and bicycle crossing. Refuge islands offer a protected space for pedestrians and bicyclists to wait for an acceptable gap in traffic, especially at unsignalized crossings, and reduce overall crossing length.

4.2.1.2 Bicycle Facilities

Bicycle facilities are very important in creating a multimodal transportation Pueblo Viejo District. The Streetscape Concepts in Section 4.1 suggests various facilities that can be incorporated in the available right of way in the commercial areas of Pueblo Viejo District. It is suggested that a bicycle connectivity plan be created for the Pueblo Viejo District area to assess the feasibility of various bikeway types on the streets of the District and ensure connectivity to businesses in the commercial areas. Low speed and low traffic residential streets should be looked at to provide traffic calming features and cover them as bike boulevards. As far as possible, analysis should assess the feasibility of the bike facility from the safest to the least safe option in the order listed below.

- Separated trail
- Curb/median-separated cycle track
- Post-separated cycle track
- Elevated cycle track
- Buffered green bike lane
- Buffered bike lane
- Green bike lane
- Striped bike lane

- Bike boulevards
 - Sharrows with signs
 - Share the road signs
 - No facility
- Caltrans standards and National Association of City Transportation Officials (NACTO) guidelines should be followed in the design of these facilities along the roadway and at crossings.
 - The bike facilities should be connected to transit and pedestrian facilities.

Bicycle Parking

- Bicycle racks should be placed in the Furnishings Zone at strategic locations specified by the City. The racks should be installed between street trees, planter boxes, and other street furnishings.
- Racks should be located so that an average of two bicycles may be parked every 200 feet.
- Maximum distance between any two rack locations is not to exceed 600 feet.
- Bicycle stands should complement the streetscape design, materials, and color palette in the area.

Separated Bike Lane. SOURCE: Paul Krueger



Buffered Bike Lane. SOURCE: Paul Sableman



4.2.1.3 Transit Facilities

Bus Stop Placement

Bus stops should be placed in strategic locations as specified by the Sunline Transit Agency and as determined by a traffic/transit study of the roadway. Many factors influence the location of stops, such as site-specific safety considerations, traffic patterns, intersection geometry, passenger origins and destinations, pedestrian accessibility, route design, frequency of use, density and land use, service type (express vs. local), and available space. In general, pedestrian-oriented areas have closely placed bus stops (approximately 0.25 miles) compared to areas with less pedestrian focus.

The guidelines below apply to the placement of bus stops.

- It is preferred that bus stops be located on the far side of a roadway (right after an intersection) because that location is the safest for passengers exiting the bus and minimizes conflicts with other vehicles. Midblock or near-side stop (just before an intersection) may be used in some situations, depending on Bus pull-out bays are preferred over curbside stops and should be integrated into streetscape design where possible.
- The bus stop should be placed in the Furnishings Zone and should not spill into the Throughway Zone.
- Bus stops should be placed in such a way that they are visible to transit users.
- The bus stop boarding, and alighting area should have a firm, stable surface and should be handicap accessible pursuant to ADA standards.

Sunline Transit Agency Bus Stop. SOURCE: Michael Baker International



- It is advised that all bus stops in the Pueblo Viejo District incorporate a shelter per Sunline Transit Agency’s standards.

4.2.2 Parking

On street parking is encouraged in the commercial areas as it has several advantages. Besides providing convenient parking for businesses, on street parking can reduce the need for provision of off street parking depending on city’s zoning code and thereby reducing impervious surface required for development. It also acts as a traffic management tool by providing a safety barrier for pedestrians and reduces motor vehicle speeds.

- On-street parking is encouraged in the commercial areas.
- Multi-space parking meters are encouraged compared to individual pole mounted meters as they incorporate more customer-friendly features such as on-screen instructions and acceptance of credit cards for payment.
- Parallel or angle parking may be based on street configuration as determined by a traffic study and the streetscape design.
- The on-street parking stall dimensions should follow City’s parking standards.
- Each parking stall should be individually marked.

*Multi-space Parking Meter Example.
SOURCE: Zorro2212*



4.2.3 Access Management

Access management is the practice of properly locating and designing access to adjoining properties to reduce conflicts and improve safety while maintaining reasonable property access and traffic flow on the public street system.

- Minimize curb cuts to reduce conflicts between vehicles, pedestrians, and bicyclists; locate driveways and major entrances away from intersections and away from each other to minimize effects on traffic operations; minimize potential for crashes; provide for adequate storage lengths for turning vehicles; and reduce conflicts with pedestrians.
- Consider using curbed medians and locating median openings to manage access and minimize conflicts.
- Use turn lanes where medians are not possible to avoid delays in traffic flow.
- Use cross streets and alleys to provide access to parking and loading areas behind buildings.

4.2.4 Street Furnishings

Street furnishings have a significant aesthetic impact on the streetscape. Therefore, attention should be paid to all street furnishings to maintain

consistency along the corridor. Examples of street furniture include benches, trash bins, telephone booths, water fountains, and recycling/trash containers.

- Commercial area street furnishings should follow the palette established by the Sixth Street streetscape design. The City will create a developer contribution program to fund the street furnishings as properties are developed. The City will install all the street furnishings, which will ensure the consistency of the streetscape.
- The selected designs for street furnishings not described in the Sixth Street streetscape design should be compatible with the established palette and be simple, functional, easily maintained, sturdy, and of good commercial-grade quality.
- The materials should be nonreflective and be able to withstand weather conditions.
- The materials that come in contact with pedestrians should have a low heat absorption index.
- **Benches**
 - Benches in commercial areas should follow the palette established by the Sixth Street streetscape design: 96-inch Classic series bench, Model C-196, bronze powder-coat color with IPE wood seat, manufactured by Victor Stanley. Benches should be attached to the ground per the manufacturer's recommendation.
 - Benches should be placed in the Frontage Zone similar to the current Sixth Street design.
 - The frequency of benches will vary by subarea, with the pedestrian-friendly Sixth Street Subarea having at least two benches per block compared to Grapefruit

Bench Used in Sixth Street Streetscape. SOURCE: Victor Stanley



Boulevard or Cesar Chavez Street with one bench per block.

- The City will determine the location of the benches based on detailed streetscape plans.
 - Once installed, the benches are to be maintained by property owners.
- **Recycling and Trash Containers**

- Trash containers in commercial areas should follow palette established by the Sixth Street streetscape design: Dynasty Series litter receptacle, Model DYN-36, bronze powder-coat color with side opening, manufactured by Victor Stanley. Containers should be attached to the ground per the manufacturer’s recommendation.
- It is recommended that the new streetscape design use trash and recycling containers. Recommended model: Dynasty Series litter receptacle, Model DYN-242, bronze powder-coat color with side opening, manufactured by Victor Stanley. Containers should be attached to the ground per the manufacturer’s recommendation.
- Recycling and trash containers should be placed in the Frontage Zone, similar to the current Sixth Street design, and in line with the benches.
- The placement of the containers will follow the design of the streetscape. However, it is advised that one set of containers be placed every 200 feet in areas with heavy pedestrian traffic such as the Sixth Street Subarea.
- Containers should be placed so that they are easily visible.
- The containers should have clear and visible symbols for trash and recycling.
- Recycling and trash containers should be maintained by the property owner.

Trash Can Used in Sixth Street Streetscape. SOURCE: Victor Stanley



Trash and Recycling Can Used in Sixth Street Streetscape. SOURCE: Victor Stanley



■ **Public Art**

- Apart from regular street design, streetscapes should provide for a periodic “surprise element” in the form of public art. These elements may be stand-alone structures or interactive art pieces, gateway elements, or artistic gathering places. Surprise elements not only play an important role in attracting the public, they also help create an identity within each subarea.

Use of Public Art as Surprise Element. SOURCE: Jeangagnon



■ **Utilities**

- Utility connections and support should be located in the Furnishings Zone to avoid conflict with pedestrian movement in the rights-of-way.

Example of Tree Grate. SOURCE: David Schott from Redmond



- New utility upgrades and service to development parcels should be installed belowground and should be easily accessible. These should be approved by the City’s Public Works Department in coordination with other regulating agencies.
- Existing utility boxes such as telephone boxes could be converted into pieces of art to add character to the neighborhood.

■ **Tree Grates**

- Tree grates may be used in the commercial areas based on the streetscape design.
- Tree grate design and material should complement the street furnishings.
- Tree grates should be a minimum of 5 feet square.
- Tree grate should flush well with the sidewalk in order to prevent any injury to pedestrians related to uneven surface

4.2.5 Lighting

- The City has an established palette for lighting along Sixth Street. This palette should be continued in all commercial areas.
- The City will create a developer contribution program to fund the lighting as properties are developed. The City will install all the street lighting, which will ensure the consistency of the streetscape.

4.2.6 Gateways and Entries

According to the Vision Plan for Pueblo Viejo, major gateway features are suggested at three intersections.

- Cesar Chavez Street and Grapefruit Boulevard
- Sixth Street and Grapefruit Boulevard
- Sixth Street and Cesar Chavez Street

Following guidelines are suggested for the design of the Gateways and Entry structures

- To create a "sense of arrival" or entry, there must be a significant change in the spatial arrangement to the scale of which the pedestrian and the vehicle driver can perceive the change.
- The size and scale of an entry is important as it must be bold enough to capture the attention and memory of the user.
- To create a visual change, contrast of spatial elements and design features are required. To strengthen the entry statement, a

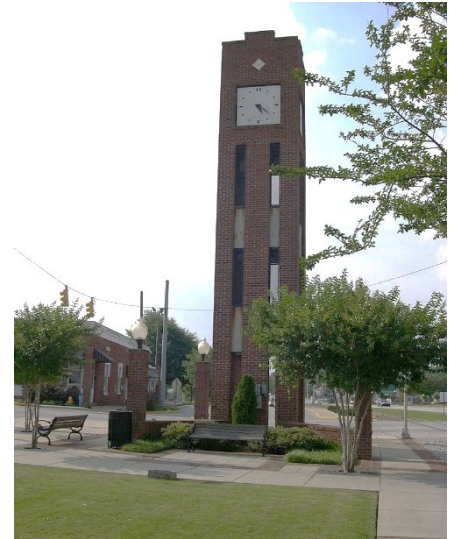
combination of a vertical support structure with an overhead horizontal structure is recommended.

- The Gateways should be designed in a way that its materials and design reflect the Spanish colonial revival or Mission Revival architectural style.
- Gateways and Entry structure should be placed in a way to not cause obstructions to the line of sight.

Example of a Gateway. SOURCE: Charvex



Gateway Tower Example: SOURCE: Jerry Stevens



4.2.7 Roadway Signage

- The City has an established palette for roadway signs along Sixth Street. This palette should be continued in all commercial areas.
- Signage location and installation should be per MUTCD standards or as specified by the City based on detailed streetscape design.
- Signage should be clearly visible to the intended traffic.
- The signage installation should be in the Furnishings Zone and should not create obstructions in the Throughway Zone.

4.2.8 Wayfinding

Exterior wayfinding signs help locals and visitors orient to a place and easily find shops or restaurants.

- Wayfinding signs may be placed near landscaped areas in the Furnishings Zones at major intersections. They can also be placed at strategic locations that can also double as plaza spaces in commercial areas.

- The design of wayfinding signs should complement the distinctive characteristics of the subareas and the street furniture in the commercial areas.
- Maps may be designed as interactive or static.
- Maps should be designed as easy to read, and graphics should be clear and easily understood.
- The maximum height permitted is 6 feet, or as approved by the City.
- The maximum width permitted is 4 feet, or as approved by the City.
- Illumination should be subdued and not garish.

Wayfinding Signage Example. SOURCE: Stevekeiretsu Wayfinding Signage Example. SOURCE: Seattle City Council from Seattle



4.3 TRAFFIC CALMING FEATURES

Traffic calming features are used to reduce vehicle speeds and make it safer and more comfortable for other modes to coexist with the automobiles. A variety of traffic calming features are available for use in streetscape design. Commercial streets are expected to attract pedestrians and bicyclists and hence streetscape design with strategically incorporated traffic calming features will help maintain speed limit while encouraging walkability.

Residential streets are well established in the Pueblo Viejo District. However, speeding and cut-throughs are an issue. Further, because of the lack of space on the commercial streets to accommodate facilities for bicyclists, the residential neighborhood streets are an option for creating alternative routes in the form of bike boulevards. These accommodations and general safety involve a reduction in the speed of motor vehicles moving along the residential neighborhood streets.

Every street is different, and further study is needed to implement one or more suitable methods for each street and purpose it serves. Described below are some methods that can calm traffic and increase the real as well as perceived sense of safety for pedestrians and bicyclists in the residential areas of Pueblo Viejo. The Table B contains a matrix that shows the applicability of these features by streets.

- **Medians.** A median is the portion of the roadway separating opposing directions of the roadway or separating local lanes from through travel lanes. Medians may be depressed, raised, or flush with the road surface. Medians are generally linear and continuous through a block and allow vehicles to travel efficiently. They can also encourage pedestrians to cross away from crosswalks. The presence of a median (especially a raised planted median) gives the perception of narrower lanes, in turn causing vehicles to slow down. Medians also reduce the length of the crossing if accompanied with a median island, making crossing safer for pedestrians and bicyclists.
- **Median Islands.** A median island is an island in the middle of the roadway, typically with landscaping for aesthetic reasons, in order to narrow the vehicle travel lanes. They are generally located midblock and can be used as a gateway to the street or community. A median island can be combined with a refuge island (a cut created to provide refuge for pedestrians) when midblock crossings are warranted.
- **Reduced Lane Widths.** According to the AASHTO Green Book, for rural and urban arterials, lane widths may vary from 10 to 12 feet.

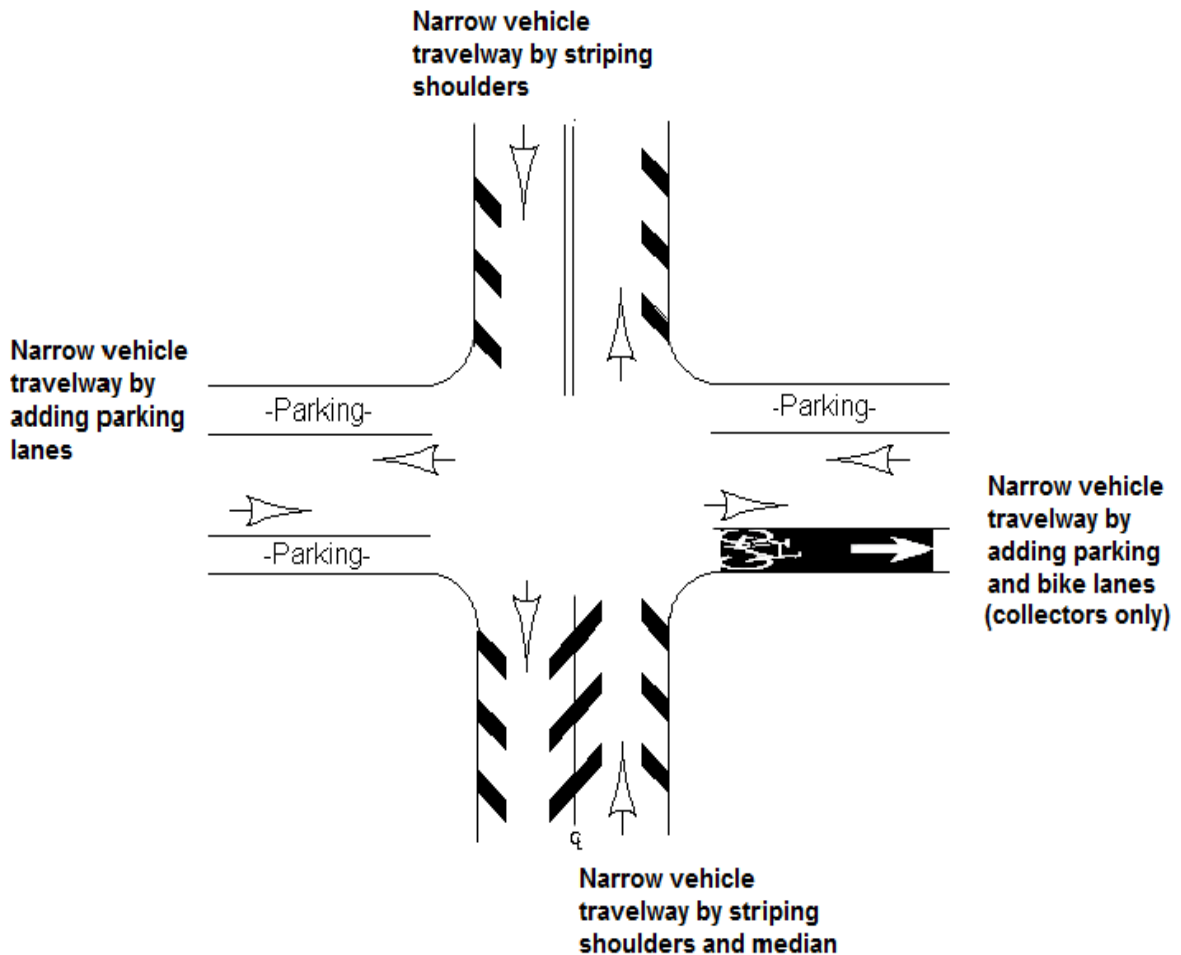
Median Island. SOURCE: Richard Drdul



For roadways at low speeds (45 mph or less) narrower lane widths are normally quite adequate and have some advantages. When there is excess space available, it can be repurposed to provide features such as bike lanes, enhanced pedestrian facilities, and on-street parking without the need to physically widen the roadway. All roadways in Pueblo Viejo District have speed limit of 45 mph or less and hence narrow roadways can be applied as a traffic calming feature.

- Pavement Markings.** Most streets in the Pueblo Viejo District have no pavement markings, leaving the street wide open for speeding vehicles. Pavement markings allocate limited space for moving traffic, keeping vehicles in their lanes and helping motorists slow down. The perception of narrower streets tends to make motorists drive slower. Various options for pavement markings are shown in figure below

Pavement Marking. SOURCE: Traffic Calming Guide for Neighborhood Streets, Virginia Department of Transportation, 2017



- Curb Extensions/ Bulb-outs.** Curb extensions/bulb-outs are provided at driveways and at intersections. They can be effective at intersections in reducing crossing length and increasing visibility, making crossing safer for pedestrians and bicyclists. The presence of curb extensions/bulb-outs gives the perception of narrower lanes, in turn causing vehicles to slow down. They may result in a loss of parking but do include space for landscaping, creating community character and a pedestrian-friendly environment. The curb extensions must be carefully planned and designed to work with the existing driveway locations and the function of existing drainage and street sweeping systems.

Landscaping in Curb Extensions. SOURCE: Richard Drdul

Curb Extensions in Commercial Area. SOURCE: Andrew Bossi



- Chokers.** Chokers are curb extensions that reduce the overall width of the roadway. They create a pinch point along the street. Chokers can be created by bringing both curbs in, or they can be done by more dramatically widening one side at a midblock location. They should be clearly visible to oncoming traffic and should incorporate landscaping.
- Chicanes.** Chicanes create a horizontal diversion of traffic and can be gentler or more restrictive depending on the design. Shifting a travel lane has an effect on speeds as long as the taper is not so gradual that motorists can maintain speeds. For traffic calming, the taper lengths may be as much as half of what is suggested in traditional highway engineering.
- Speed Humps.** Speed humps can serve to slow traffic and are especially effective on residential roadways and other low-volume roadways. However, they can be discouraged by fire departments due to potential reductions in response times. While speed humps may be effective at reducing speed at the location of the hump itself, vehicles may speed up between the humps depending on the distance between them and other

Speed Hump. SOURCE: Richard Drdul



factors such as stop signs and signalized intersections. Noise impacts should also be considered as a factor due to an increase in the noise level from vehicles passing over the humps, as well as the additional noise from vehicles braking in advance of the humps and accelerating after crossing them.

Speed Lump. SOURCE: Whatlep



- **Speed Lumps.** A speed lump is a modified speed hump where openings are added to accommodate emergency or other large vehicles so that they can use the openings without traversing the raised portion to minimize speed reduction. However, the size of the speed lumps ensures that passenger vehicles cannot likewise avoid traveling over at least one set of lumps. Like speed humps, speed lumps should be clearly visible and are generally placed midblock.
- **Speed Tables.** Speed tables provide an overall gentler transition than speed humps because they incorporate a larger flat area. They improve noise levels compared to speed humps. Speed tables should be clearly visible and are generally placed midblock.

Raised Crosswalk. SOURCE: Scott Batson



- **Raised Crosswalks.** In addition to marked or decorative treatments, raised crosswalks make pedestrians more visible to oncoming traffic, in addition to serving as a speed reduction facility for vehicles due to the horizontal deflection of the roadway. The elevation of the crosswalk serves to reduce speeds almost like a speed hump and textured materials. Raised crosswalks should be designed pursuant to ADA standards. Since they increase visibility, it is suggested that midblock crossings be treated as raised crosswalks.

PMSD Sign. SOURCE: Richard Drdul



- **Raised Intersections.** Raised intersections make for an overall gentler transition compared to speed humps and can provide visually attractive traffic calming on two or more streets at once.
- **High Visibility Crosswalks.** Refer to the discussion of crosswalks in Section 4.2.1.1, Pedestrian Facilities.
- **Pole-Mounted Speed Display (PMSD) Signs.** This sign combines a speed limit sign with a radar speed feedback sign that displays the real-time speed of an approaching vehicle, which tends to make motorists reduce their speed. The signs are highly effective on roadways identified for traffic calming and should be placed at the beginning of a street section. A 200-foot visibility distance should be provided.
- **Traffic Calming Circles and Mini-Roundabouts.** Traffic calming circles are smaller than roundabouts and can fit within the area available for area intersections. They are most appropriate for

residential neighborhoods where speeding might be an issue. When left-turning traffic is heavier, an alternative solution is a mini-roundabout. The primary difference between a traffic circle and a mini-roundabout is that with the mini-roundabout, the central area must be traversable by long vehicles and cannot be landscaped. Instead, it is often raised slightly and paved with a special treatment to discourage traffic. Automobiles will generally circulate properly around the mini-roundabout, while trucks will turn over the raised area slowly.

Mini Roundabout. SOURCE: Stephen Sweeney



- **On Street Parking.** On-street parking, in effect, reduces the width of the street, leading to slower driving. Parking also separates traveling cars from the sidewalk, helping to improve pedestrian safety.

On Street Parking and Lane Making Giving Perception of Reduced Lane Width. SOURCE: Andrew Bossi



Table B: Traffic calming feature by street

Type of Traffic Calming Feature	Suggested Streets for Application
Medians	<ul style="list-style-type: none"> ■ Grapefruit Boulevard ■ Cesar Chavez Street
Median Islands	<ul style="list-style-type: none"> ■ Fifth Street ■ Sixth Street ■ Seventh Street ■ Grapefruit Boulevard ■ Cesar Chavez Street ■ Orchard Street ■ Vine Avenue ■ Residential Streets
Reduced Lane Widths	<ul style="list-style-type: none"> ■ Fifth Street ■ Sixth Street ■ Seventh Street ■ Grapefruit Boulevard ■ Cesar Chavez Street ■ Orchard Street ■ Vine Avenue
Pavement Markings	<ul style="list-style-type: none"> ■ Fifth Street ■ Sixth Street ■ Seventh Street ■ Grapefruit Boulevard ■ Cesar Chavez Street ■ Orchard Street ■ Vine Avenue ■ Residential Streets
Curb Extensions/Bulb-outs	<ul style="list-style-type: none"> ■ Fifth Street ■ Sixth Street ■ Seventh Street ■ Orchard Street ■ Vine Avenue ■ Residential Streets
Chokers	<ul style="list-style-type: none"> ■ Residential Streets
Chicanes	<ul style="list-style-type: none"> ■ Residential Streets
Speed Humps	<ul style="list-style-type: none"> ■ Fifth Street ■ Sixth Street ■ Seventh Street ■ Orchard Street ■ Vine Avenue ■ Residential Streets

Speed Lumps	<ul style="list-style-type: none"> ■ Residential Streets
Speed Tables	<ul style="list-style-type: none"> ■ Residential Streets
Raised Crosswalks	<ul style="list-style-type: none"> ■ Fifth Street ■ Sixth Street ■ Seventh Street
Raised Intersections	<ul style="list-style-type: none"> ■ Fifth Street ■ Sixth Street ■ Seventh Street ■ Grapefruit Boulevard ■ Cesar Chavez Street
High Visibility Crosswalks	<ul style="list-style-type: none"> ■ Fifth Street ■ Sixth Street ■ Seventh Street ■ Grapefruit Boulevard ■ Cesar Chavez Street ■ Orchard Street ■ Vine Avenue ■ Residential Streets
Pole-Mounted Speed Display (PMSD) Signs	<ul style="list-style-type: none"> ■ Fifth Street ■ Sixth Street ■ Seventh Street ■ Grapefruit Boulevard ■ Cesar Chavez Street ■ Orchard Street ■ Vine Avenue ■ Residential Streets
Traffic Calming Circles and Mini-Roundabouts	<ul style="list-style-type: none"> ■ Residential Streets
On Street Parking	<ul style="list-style-type: none"> ■ Fifth Street ■ Sixth Street ■ Seventh Street ■ Orchard Street ■ Vine Avenue ■ Residential Streets

4.4 LANDSCAPING GUIDELINES AND PLANTING PALETTE

4.4.1 Shade and Tree Canopy

- The palette of plant materials should create an aesthetically pleasing space through a mix of colors, heights, and types of plants.
 - Trees should provide vertical interest.
 - Shrubs should be used as a natural fence that shields vehicles from the pedestrian sidewalk.
 - Flowering plant material in a variety of colors at the ground level or in planters is encouraged.
- Creative design and the use of native and drought-tolerant plants are required. The latest editions of *Western Garden Book* as well as Coachella Valley Water District Publication *Lush and Efficient-Landscape Gardening in the Coachella Valley*, should be referred to when finalizing the tree palette.
- Landscaping can be incorporated into open tree wells, planter pots, and fences.
- Ample space should be provided for the growth of trees, so the roots do not damage sidewalks as they grow.

4.4.2 Planter Boxes/Containers

There are opportunities along some of the commercial corridors such as Sixth street to incorporate planter boxes in the design. The main aim of this planter boxes will be to provide physical as well as perceived separation from the moving traffic as well as to add color splash to the streetscape. Below are suggested guidelines.

- The City planter boxes should be located in the Furnishings Zone of the streetscape and be consistent for the entire Streetscape in order to provide continuity of design.
- The City planter boxes should be made of durable material that can be easily maintained and can withstand wear and tear due to rough handling. Some materials used for City provided planter boxes are concrete, cast stone, Glass fiber reinforced concrete ay.
- Planter boxes provided by private property owner in Frontage Zone can be of varying materials and shapes as long as they are complementary to building architecture and streetscape and well maintained at all times.
- The planter boxes both in Furnishings Zone and Frontage Zone should not encroach on clear space allocated for pedestrian walkway.

- Planter boxes should be placed in such a way to provide easy access to underground utilities lids
- Planter boxes should be placed in manner to not interfere with drop off zones, disabled parking and loading zones.
- Planter boxes should be made heavy enough to not be lifted by couple individuals in order to avoid theft or easy movement of the planter boxes.
- Planter boxes should be at least 30 inches in height.
- The shape and sized of planter boxes should be complementary to the streetscape design and function.



4.4.3 Planting Palette

4.4.3.1 *Street and Ornamental Trees*

The street and ornamental tree palette proposed here are suggestive purposes and may be changed based on the design of the streetscape. However, it is suggested that the trees should be chosen to thrive in the harsh desert climate of Coachella. The latest edition of Western Garden Book should be referred to when finalizing the tree palette for the streetscapes for Pueblo Viejo District.

Street trees and ornamental trees are suggested for each street to provide its own unique identity. There are a variety of ways to configure these trees and will depend on the design of streetscape. Below is the suggested matrix and pictures by the street.

Sixth Street

Street Tree- Date Palm - Phoenix dactylifera



Characteristics

- Height: 80 - 100 feet
- Width: 20 - 40 feet
- Tree Shape: Feather Palm
- Foliage: Evergreen; Gray Green
- Leaves Shape: Frond and Pinnatifid
- Flowers: Spring or Summer; Yellow
- Fruit: Fall; Brown Drupe
- Shading Capacity: Moderate

Ornamental Tree- Pink Dawn Chitalpa - x Chitalpa tashkentensis 'Pink Dawn'



Characteristics

- Height: 25 - 35 feet
- Tree Shape: Rounded, Umbrella or Vase Shape
- Foliage: Deciduous; Gray Green, Gold
- Leaves Shape: Lanceolate to Ovate
- Flowers: Spring, Summer or Fall; Showy and Pink
- Shading Capacity: Moderate

Seventh Street

Street Tree- Chinese Flame Tree - *Koelreuteria bipinnata*



Characteristics

- Height: 20 - 40 feet
- Width: 15- 30 feet
- Tree Shape: Rounded, Umbrella or Vase Shape
- Foliage: Deciduous
- Leaves Shape: Bipinnately Compound, Medium Green, Bronze or Gold
- Flowers: Summer or Fall; Yellow
- Fruit: Fall; Prolific, Orange, Red or Rose Capsule

Ornamental Tree (Option-1) - Palo Verde - *Cercidium x 'Desert Museum'*



Characteristics

- Height: 20 - 25 feet
- Width: 12 - 15 feet
- Tree Shape: Rounded or Umbrella
- Foliage: Deciduous; Blue Green or Medium Green
- Leaves Shape: Pinnately Compound Odd
- Flowers: Spring; Bright Yellow
- Shading Capacity: Moderately Low

Ornamental Tree (Option-2) - Crepe Myrtle - *Lagerstroemia indica*



Characteristics

- Height: 20 - 25 feet
- Width: 20 - 25 feet
- Tree Shape: Oval, Rounded, Umbrella or Vase Shape
- Foliage: Deciduous; Bronze, Dark Green, Red, Gold, Orange Multicolored
- Leaves Shape: Oval
- Flowers: Summer; Showy. Lavender, Pink, Red, Rose or White
- Fruit: Fall; Brown Capsule
- Shading Capacity: Moderately Dense

Fifth Street

Street Tree- Chinese Pistache - <i>Pistacia chinensis</i>	
	<p>Characteristics</p> <ul style="list-style-type: none"> ■ Height: 25 - 35 feet ■ Width: 25 - 35 feet ■ Tree Shape: Oval, Rounded or Umbrella ■ Foliage: Deciduous; Medium Green, Red, Gold, Orange or Multicolored ■ Leaves Shape: Pinnately Compound Even with Oblong to Elliptic Leaflets ■ Flowers: Spring; Inconspicuous ■ Fruit: Summer or Fall; Prolific- Red or Mostly Blue Drupe ■ Shading Capacity: Moderately Dense
Ornamental Tree (Option-1) - Crepe Myrtle - <i>Lagerstroemia indica</i>	
	<p>Characteristics</p> <ul style="list-style-type: none"> ■ Height: 20 - 25 feet ■ Width: 20 - 25 feet ■ Tree Shape: Oval, Rounded, Umbrella or Vase Shape ■ Foliage: Deciduous; Bronze, Dark Green, Red, Gold, Orange Multicolored ■ Leaves Shape: Oval ■ Flowers: Summer; Showy. Lavender, Pink, Red, Rose or White ■ Fruit: Fall; Brown Capsule ■ Shading Capacity: Moderately Dense
Ornamental Tree (Option-2) - Buddhist Bauhinia - <i>Bauhinia variegata 'Candida'</i>	
	<p>Characteristics</p> <ul style="list-style-type: none"> ■ Height: 20 - 35feet ■ Width: 20 - 30 feet ■ Tree Shape: Rounded or Umbrella Shape ■ Foliage: Deciduous to Partly Deciduous, Light Green ■ Leaves Shape: Lobed and Palmate ■ Flowers: Spring or Summer; Showy-Fragrant White ■ Fruit: Summer; Brown Pod ■ Shading Capacity: Moderate

Grapefruit Boulevard

Street Tree- Arizona Ash - Fraxinus velutina 'Bonita



Characteristics

- Height: 30 - 50 feet
- Width: 30 - 40 feet
- Tree Shape: Conical or Oval Shape
- Foliage: Deciduous ; Medium to Light Green, Gold
- Leaves Shape: Pinnately Compound Odd
- Flowers: Spring; Inconspicuous
- Fruit: Summer or Fall; Yellow or Mostly Green Winged Seed
- Shading Capacity: Moderate

Ornamental Tree - Honey Mesquite - Prosopis glandulosa



Characteristics

- Height: 25 - 35 feet
- Width: 25 - 35 feet
- Tree Shape: Rounded
- Foliage: Deciduous ; Green
- Leaves Shape: Bipinnately Compound
- Flowers: Spring or Summer; Inconspicuous- Yellow
- Fruit: Spring; Brown Pod
- Shading Capacity: Low to Moderately Low

Cesar Chavez Street

Street Tree- California Fan Palm - *Washingtonia filifera*



Characteristics

- Height: 50 - 70 feet
- Width: 10 - 20 feet
- Tree Shape: Fan Palm
- Foliage: Evergreen; Dark Green
- Leaves Shape: Palmate
- Flowers: Year Round; Inconspicuous
- Fruit: Year Round; Black Drupe
- Shading Capacity: Dense

Ornamental Tree (Option-1) - Hong Kong Orchid - *Bauhinia blakeana*



Characteristics

- Height: 20 - 40 feet
- Width: 20 - 25 feet
- Tree Shape: Umbrella
- Foliage: Partly Deciduous; Gray Green
- Leaves Shape: Lobed and Palmate
- Flowers: Fall or Winter; Fragrant Pink, Purple or Rose
- Fruit: Fruitless
- Shading Capacity: Moderate

Ornamental Tree (Option-2) - Chinese Elm- *Ulmus parvifolia*



Characteristics

- Height: 40 - 60 feet
- Width: 50 - 70 feet
- Tree Shape: Oval, Rounded or Umbrella
- Foliage: Evergreen to Partly Deciduous; Glossy Dark Green, Bronze or Gold
- Leaves Shape: Elliptic to Ovate
- Flowers: Summer or Fall; Inconspicuous
- Fruit: Fall; Brown or Mostly Green Winged Seed
- Shading Capacity: Moderate

Other Options

Ornamental Tree (Option-1) - Desert Willow - *Chilopsis linearis*



Characteristics

- Height: 15 - 30 feet
- Width: 10 - 20 feet
- Tree Shape: Rounded or Umbrella
- Foliage: Deciduous; Blue Green or Medium to Dark Green, Gold
- Leaves Shape: Linear
- Flowers: Spring or Summer; Showy Fragrant- Lavender, Pink, Rose or White
- Fruit: Fall; Brown Capsule
- Shading Capacity: Low

Ornamental Tree (Option- 2) - Ocotillo - *Fouquieria splendens*



Characteristics

- Height: 18 - 20 feet
- Width: 8 - 10 feet
- Tree Shape: Vase Shape
- Foliage: Type; Color
- Leaves Shape: Ovate Leaves on Spiny Stems
- Flowers: Spring or Summer; Tubular- Red
- Shading Capacity: Low

Ornamental Tree (Option-3) - Pomegranate - *Punica granatum*






Characteristics

- Height: 18 - 20 feet
- Width: 12 - 15 feet
- Tree Shape: Oval, Umbrella or Vase Shape
- Foliage: Deciduous; Glossy Light to Medium Green, Gold
- Leaves Shape: Oblong
- Flowers: Spring or Summer; Showy- Orange, Pink or Red
- Fruit: Fall; Prolific- Brown or Red Berry Shading Capacity: Moderate to Dense

4.4.3.2 Container Plantings

Various varieties of grasses, dwarf shrubs, flowering perennials and succulents are suggested here planting in containers in the public realm. These plants are drought tolerant and well suited to Coachella region. The palette is suggested for Pueblo Viejo District as a whole. These plants can be mixed and matched to create unique combination along each streetscape that have planter boxes. The suggested plants can be used by private property owners for planting in Frontage zone of they choose. The latest edition of Western Garden Book should be consulted to determine any planting that is not suggested in the palette below for container planting in public realm as well as areas open to public in private realm.

Grasses and Grass-like Plants

Carex		<u>Blooming</u>
		Early Summer
Festuca		<u>Blooming</u>
		Summer
Stipa		<u>Blooming</u>
		Non-flowering

Dwarf Shrubs

Nandina



Blooming

Late Spring, Early Summer

Calliandra



Blooming

Seasonal Bloomer

Dwarf Rose



Blooming

Spring to Frost




Convolvulus



Blooming

Late Summer

Flowering Perennials

Teucrium		Blooming Summer
		
Origanum		Blooming Summer to Fall
		
Salvia		Blooming Summer to Fall
		

Succulents

Sedum	
	<p><u>Blooming</u></p> <p>Summer to Fall depending on the type on the species</p>
Echeveria	
	<p><u>Blooming</u></p> <p>Summer to Fall depending on the type on the species</p>
Aloe	
	<p><u>Blooming</u></p> <p>Seasonal Bloomer</p>
Agave	
	<p><u>Blooming</u></p> <p>Rarely Blooms</p>



Coachella Civic Center, Hearing Room
53-462 Enterprise Way, Coachella, California
(760) 398-3502 ♦ www.coachella.org

MINUTES

OF A REGULAR MEETING
OF THE
CITY OF COACHELLA
PLANNING COMMISSION

January 19, 2022
6:00 PM

CALL TO ORDER: 6:01 P.M.

PLEDGE OF ALLEGIANCE:

ROLL CALL:

Commissioners Present: Commissioner Figueroa, Commissioner Gonzalez, Commissioner Leal, Vice Chair Navarrete, Chair Virgen (All Planning Commissioners participated via teleconference)

Staff Present:

- *Gabriel Perez, Development Services Director
- *Nikki Gomez, Associate Planner
- *Rosa Montoya, Planning Technician
- *Celina Jimenez, Grants Manager
- *Andrew Simmons, City Engineer
- *Participated in meeting via teleconference

SPECIAL ORDER OF BUSINESS

Selection of Planning Commission Chair and Vice-Chair

Commissioner Figueroa nominated Vice Chair Navarrete as Chair. Leal made an alternate motion that Vice Chair Navarrete and Chair Virgen continue in their roles as Vice Chair and Chair. Chair Virgen asked if there was a second to the original nomination of Vice Chair Navarrete as Chair. There was no second to the motion and the motion failed due to a lack of a second to the motion. Commissioner Gonzalez seconded the alternate motion made by Commissioner Leal that Vice Chair Navarrete and Chair Virgen continue in their roles as Vice Chair and Chair. Separate Roll Call votes were taken for the consideration of Vice Chair Navarrete and Chair Virgen to continue their roles as Vice Chair and Chair

IT WAS MOVED BY COMMISSIONER LEAL AND SECOND BY COMMISSION GONZALEZ TO SELECT CHAIR VIRGEN TO CONTINUE AS PLANNING COMMISSION CHAIR.

Approved Chair Stephanie Virgen as Chair by the following roll call vote:

AYES: Vice Chair Navarrete, Chair Virgen, Commissioner Gonzalez, Commissioner Leal, Commissioner Figueroa.

NOES: None.

ABSTAIN: None.

ABSENT: None.

IT WAS MOVED BY COMMISSIONER LEAL AND SECOND BY COMMISSION GONZALEZ TO SELECT VICE CHAIR NAVARRETE TO CONTINUE AS PLANNING COMMISSION VICE CHAIR.

Approved Vice Chair Miguel Navarrete as Vice Chair by the following roll call vote:

AYES: Vice Chair Navarrete, Chair Virgen, Commissioner Gonzalez, Commissioner Leal, Commissioner Figueroa.

NOES: None.

ABSTAIN: None.

ABSENT: None.

APPROVAL OF AGENDA:

“At this time the Commission may announce any items being pulled from the agenda or continued to another date or request the moving of an item on the agenda.”

IT WAS MOVED BY VICE CHAIR NAVARRETE AND SECOND BY COMMISSION GONZALEZ TO APPROVE THE AGENDA.

Approved agenda on a roll call vote:

AYES: Vice Chair Navarrete, Chair Virgen, Commissioner Gonzalez, Commissioner Leal, Commissioner Figueroa.

NOES: None.

ABSTAIN: None.

ABSENT: None.

APPROVAL OF THE MINUTES:

1. Draft Planning Commission Minutes - January 5, 2022

IT WAS MOVED BY COMMISSIONER FIGUEROA AND SECOND BY VICE CHAIR NAVARRETE TO APPROVE THE MINUTES.

Approved minutes on a roll call vote:

AYES: Vice Chair Navarrete, Chair Virgen, Commissioner Gonzalez, Commissioner Leal, Commissioner Figueroa.

NOES: None.

ABSTAIN: None.

ABSENT: None.

WRITTEN COMMUNICATIONS:

None

PUBLIC COMMENTS (NON-AGENDA ITEMS):

“The public may address the Commission on any item of interest to the public that is not on the agenda, but is within the subject matter jurisdiction thereof. Please limit your comments to three (3) minutes.”

REPORTS AND REQUESTS:

None

NON-HEARING ITEMS:

2. Coachella Sunline Transportation Hub (Architectural Review No 21-13)

The Sunline Transit Hub will serve as a transit center for Sunline Transit Agency services (Line 111, Line 91, Line 92, Line 95) and will include a 540 sq. ft. breakroom/office building for the use of Sunline Transit Agency staff, five bus shelters, landscape improvements, and a corner focal point for a future public art installation located at the Southeast corner of Cesar Chavez Street and 4th Street.

Gabriel Perez, Development Services Director, narrated a PowerPoint Presentation for the item. A copy of the Presentation is on file in the Planning Division.

Harman Singh, Sunline representative, indicated that micro transit “sun ride” for first mile, last mile rides would be provided from the proposed transportation hub. He stated that Planning Commission restroom feedback would be discussed with Sunline staff. He clarified that there is no security at bus stops and that surveillance cameras would monitor activity at the site.

Jesse Frescas, Sunline representative, indicated that the plans for the Veteran’s park restroom require a release by the architect of record. He stated construction costs of \$200,000 for a pre-manufactured 500 sq. ft. building such as the one proposed. He further stated that construction costs range to accommodate the aesthetic improvements would be \$250-350 sq. ft. for a 500 sq. ft. building and indicated the entire costs for the building would be a \$1 million building. Mr. Frescas stated that the construction budget is \$1.3 million and that there has been a 30% increase experienced in construction bids over the last 9 months. He indicated that the \$1.3 million budget for the transit hub would likely require \$1.6 million.

Brad Donais, P.E., project design engineer with Heptagon Seven, confirmed 4th Street and Cesar Chavez Street corner will be preserved for public art including electrical conduit and water service. He indicated surveillance cameras would be placed on light poles. He highlighted that there would be space for 4 buses concurrently, 2 buses off-site, and space adjacent to transit building for micro-routes or maintenance vehicles. He clarified that the \$1.8 million budget includes costs to purchase of the land. Mr. Donais stated some project features are understated due to the limited budget and they would do their best to work with staff to accommodate the Planning Commission requests.

During the ensuing discussion, the Commissioners, either individually or in agreement, provided the following commentary:

- Suggested searching for plans for Veteran’s Park for restroom building that would
- Inquired if services at transportation hub is available for other transportation options other than Sunline.
- Inquired if this would be a designated Uber and Lyft pick up area.

- Inquired if the modular building is proposed to save construction and maintenance costs.
- Expressed concerns about potential traffic congestion of buses that may affect Cesar Chavez Street.
- Recommended date palms be added to the corner of 4th Street and Cesar Chavez Street.
- Inquired if the proposed pedestrian path between 5th Street and transit hub would be lit.
- Recommended public restrooms for public convenience, even if costs were greater.
- Recommended security to assist in maintaining the public safety with the restroom.

Staff provided the following clarification to the Planning Commission:

- Various transportation options would be available from the transit hub such as van pooling services.
- Mario Lascano Street will be finished within the first 30 days of construction and is a 2-way street that has in-and-out access from 4th Street and an exit only at Cesar Chavez Street.
- Chelsea Investment Corporation constructed 20 parking stalls and the southern portion of Mario Lascano Street. The northern portion of the street is to be constructed by the City.
- Bollards and overhead lighting would be installed consistent with the Pueblo Viejo design theme between 5th Street and the transit hub.

IT WAS MOVED BY COMMISSIONER GONZALEZ AND SECONDED BY COMMISSIONER FIGUEROA TO:

- Bring back item and incorporate staff design recommendations, Planning Commission recommendations, including restrooms with incorporation of gender-neutral restrooms for cost savings.

Approved the motion to continue the item with modifications on a roll call vote:

- AYES: Vice Chair Navarrete, Chair Virgen, Commissioner Gonzalez, Commissioner Leal, Commissioner Figueroa.
- NOES: None.
- ABSTAIN: None.
- ABSENT: None.

PUBLIC HEARING CALENDAR (QUASI-JUDICIAL):

3. Coachella Valley Growers LLC Interim Outdoor Cannabis Cultivation

Conditional Use Permit 345 to allow interim outdoor cannabis cultivation on a 79.39 acre site located at 50501 Fillmore Street. (APN 763-070-012 & 763-070-010). Coachella Valley Growers, LLC.

Nikki Gomez, Associate Planner, narrated a PowerPoint Presentation for the item. A copy of the Presentation is on file in the Planning Division.

During the ensuing discussion, the Commissioners, either individually or in agreement, provided the following commentary:

- Inquired if Fillmore Street would be improved with this project due to the condition of Fillmore Street.
- Clarified that the applicant was subject to an approved special event permit for the outdoor cannabis cultivation operation previously and that the permit expired December of 2021.

Staff clarified that cost sharing agreement for street improvements and that the applicant would be responsible for removal and replacement of the Fillmore half-street along the project frontage for a 34 foot width.

Chair Virgen opened the public hearing at 7:25 P.M. and closed the public hearing at 7:26 P.M. There were no public comments

IT WAS MOVED BY COMMISSIONER GONZALEZ AND SECONDED BY VICE CHAIR NAVARRETE TO ADOPT RESOLUTION NO. PC2022-02, A RESOLUTION OF THE PLANNING COMMISSION OF THE CITY OF COACHELLA, CALIFORNIA APPROVING CONDITIONAL USE PERMIT 345 TO ALLOW INTERIM OUTDOOR CANNABIS CULTIVATION ON 79.39 ACRES SITE LOCATED AT 50501 FILLMORE STREET, COACHELLA, CA 92236 (APN 763-070-012 & 760-070-010).

Approved the item with modifications on a roll call vote:

- AYES: Vice Chair Navarrete, Chair Virgen, Commissioner Gonzalez, Commissioner Leal
- NOES: None.
- ABSTAIN: Commissioner Figueroa
- ABSENT: None.

INFORMATIONAL:

ADJOURNMENT: 7:30 P.M.

Respectfully Submitted by,

Gabriel Perez
Planning Commission Secretary

*Complete Agenda Packets are available for public inspection in the
Development Services Department at 53-990 Enterprise Way, Coachella, California, and on the
City's website www.coachella.org.*

THIS MEETING IS ACCESSIBLE TO PERSONS WITH DISABILITIES



STAFF REPORT
7/6/2022

TO: Planning Commission Chair and Commissioners

FROM: Nikki Gomez, Associate Planner

SUBJECT: Freestanding Identification Signs – Coachella Lakes RV Resort

SPECIFICS: Architecture Review No. 22-08 to install a freestanding identification sign at 25’ high and monument sign at 5’ high located at 44800 Dillon Road with the Coachella Lakes RV Resort in the C-T, PUD (Commercial Tourist Planned Unit Development) zone. Imagine Service Inc. (Applicant)

STAFF RECOMMENDATION:

Staff recommends that the Planning Commission adopt Resolution No. PC 2022-20 approving Architecture Review No. 22-08 pursuant to the findings and conditions of approval contained in the resolution.

BACKGROUND:

On March 6, 2019, the Planning Commission approved the Red Moon RV Park project on 78.3 acres of vacant land located within the existing Vineyards Luxury Motorcoach Resort at 44-790 Dillon Road. This project is a deviation from the existing Vineyards Luxury Motorcoach Resort that consisted of subdivided lots for both RV parking and “Casitas”. The Red Moon RV Park project was approved with Environmental Assessment (EA 18-03), Conditional Use Permit (CUP 308), and Architecture Review (AR 18-18) for a Recreational Vehicle (RV) Campground consisting of 469 RV parking rental spaces with no structures, clubhouse, laundry/restroom buildings with private streets and common area amenities. The development name “Red Moon RV Park” was recently renamed to “Coachella Lakes RV Resort” and is under construction. The proposed identification signs, which includes the freestanding pylon sign and monument entrance sign, is part of the site



development for the near anticipated completion of the Coachella Lakes RV Resort.

DISCUSSION/ANALYSIS:

The applicant, Image Service Inc, has submitted a request for an Architecture Review for freestanding identification signs. The two freestanding signs consist of a pylon sign at 25 feet in height located at the westerly portion of the RV Resort (S1) and a monument sign at 5’ in height (S2) located at the main entrance to the RV Resort along Dillon Road as shown in the Figure 2 below.

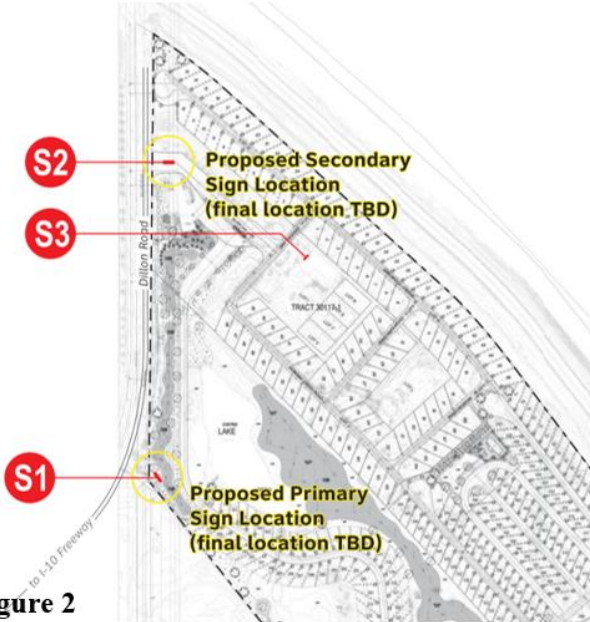


Figure 2

Table 1 shows the proposed sign’s consistency with the Municipal Code Chapter 17.56.010 Signs standards.

Table 1 – Development/Operational Standards

	Zoning Ordinance	Proposed	Complies with Code
Location	Main Signs for Commercial and Industrial Centers (More Than One Acre)	C-T, PUD (Commercial Tourist Planned Unit Development) zone 78.3 acre-site	Yes
Setbacks	Signs shall not extend beyond the property line	All the freestanding signs is within property line.	Yes
Utilization	Sign shall be used solely to identify the commercial or industrial center	The freestanding signs depicts the name of the development “Coachella Lakes RV Resort”	Yes

<p>Height</p>	<p>Signs shall not exceed a maximum height of six feet for monument sign or twenty-five (25) feet for a freestanding sign subject to architectural review and planning commission approval</p>	<p>The freestanding sign is at 25' high and the monument sign is 5' high.</p>	<p>Yes</p>
<p>Size of Signs</p>	<p>The allowable sign area shall be based on ten (10) square feet of sign per acre to a maximum area of seventy-five (75) square feet per face.</p>	<p>The 25' freestanding sign has a total of 71 square feet sign face and the monument sign is 52.5 square feet of sign face.</p>	<p>Yes</p>
<p>Illumination</p>	<p>A sign is defined as any identification, illustration or device illuminated or non-illuminated, which is visible from any public place or is located on private property and exposed to the public and which directs attention to a product, place, activity, person, institution, business or solicitation with exception of window display.</p>	<p>The 25' freestanding sign is internally illuminated and 5' the monument sign is externally illuminated.</p>	<p>Yes</p>

The proposed freestanding sign at 25' in height depicts the resort name "Coachella Lakes RV Resort" internally illuminated with project logo of three palm trees and color of yellow, orange and blue. The front face of the 25' freestanding sign is intended to be visible from Dillon Road and Vista Del Norte. The sign will consist of reverse pan channel letters mounted off cabinet faced, this allows for a halo illumination effect. The Figure 3 below shows night simulation of the sign with the halo illumination. The back face of the sign will be visible from the development; it will be non-illuminated dimensional logo.

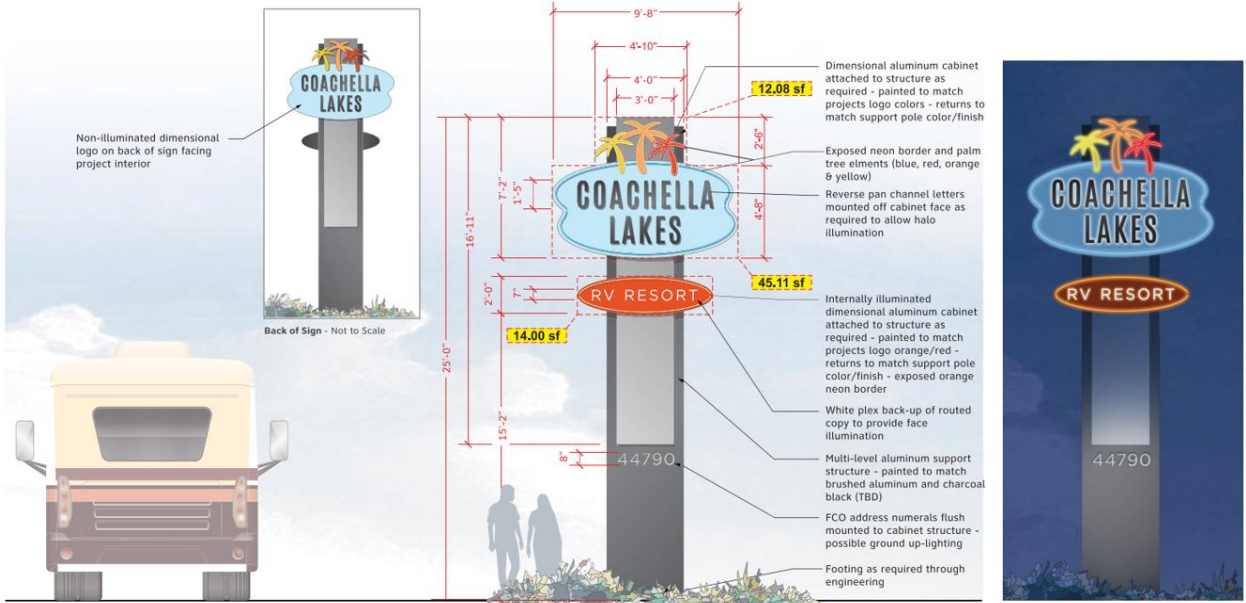
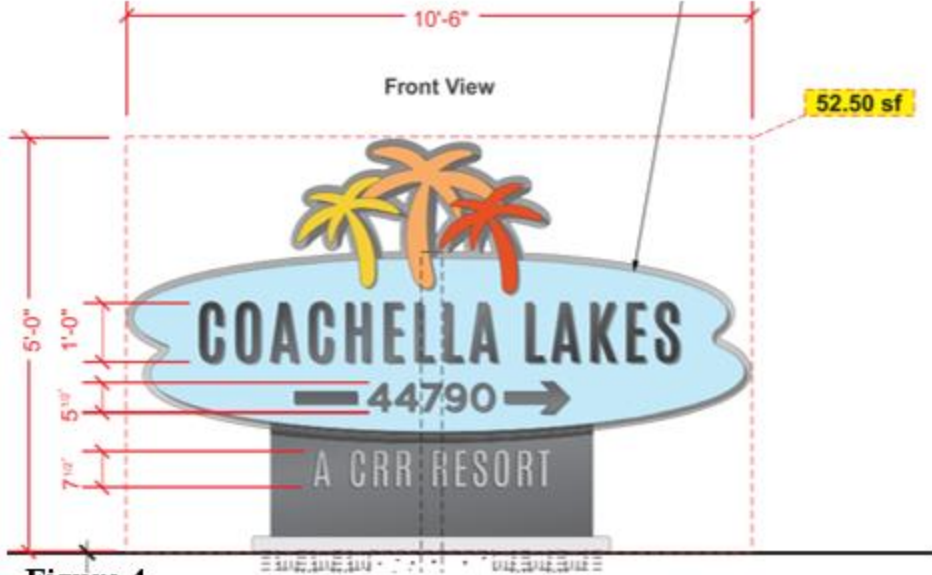


Figure 3

The proposed freestanding monument sign will be located near the gate entrance to the RV Resort along Dillon road show on Figure 4 below. The monument sign is 5’ in height with project logo and letters. The monument sign is externally illuminated using ground up lighting with FCO (flat out cut) letters, where each letters are individually cut and placed on an aluminum backing.



Environmental Setting:

The subject site is within an existing development at 44790 Dillon Road generally surrounded by urban uses, with adjoining zoning and land uses as follows:

- North: All-American Canal/Unincorporated County – Landfill (N-A County Zone)
- South: Vacant Land (MS Manufacturing Service/ CG General Commercial Zone)
- East: All-American Canal/Eastside Dike (N-A County Zone)
- West: Vacant Land (CT – Commercial Tourist/RMH – Residential Mobile Home)

Site Plan / Parking and Circulation:

The project site main entrance is located on Dillon Road with secondary access from Vista Del Norte. The entrance from Dillon Road will be shared between the Vineyards Luxury Motorcoach Resort and the Coachella Lakes RV Resort.

CONSISTENCY WITH THE GENERAL PLAN:

The proposed freestanding identification signs are intended to guide visitors to the Coachella Lakes RV Resort from Dillon Road. The location of the proposed sign is within the General Plan Subarea 12, which includes a policy to “*capitalize access to the freeway and seek out and encourage freeway, oriented uses, large format retail, lodging and regional entertainment and recreational*

uses that benefit for easy regional access.” The proposed sign is an appropriate structure for the RV Resort orientated towards Dillon Road and I-10 freeway to assist those visitors to access the RV Resort. In addition, the sign will provide advertising for those potential visitors along the freeway, with the benefit to the city as the closest entrance to the city through Dillon Road.

ENVIRONMENTAL REVIEW:

Staff has determined that the proposed project is categorically exempt from environmental review pursuant to the guidelines of the California Environmental Quality Act as “Accessory Structures-On premise signs” CEQA Guidelines, Section 15311, Class 11).

RECOMMENDATIONS:

1. Staff is recommending that the Planning Commission adopt Resolution No. PC 2022-20, approving Architecture Review No. 22-08 with the findings and conditions.
2. Deny the proposed project.
3. Continue this item and provide staff and the applicant with direction.

Attachments:

1. Resolution No. PC 2022-20
2. Vicinity Map
3. Project Plans

RESOLUTION NO. PC 2022-20

A RESOLUTION OF THE PLANNING COMMISSION OF THE CITY OF COACHELLA, CALIFORNIA APPROVING ARCHITECTURE REVIEW NO. 22-08 FOR FREESTANDING IDENTIFICATION SIGNS WITHIN THE COACHELLA LAKES RV RESORT LOCATED AT 44800 DILLON ROAD (APN 697-330-001, 005, 006, 007, 008, 009, 010, 011); IMAGE SERVICES, INC. APPLICANT.

WHEREAS, Image Services Inc, filed an application for Architecture Review No. 22-08 to install a freestanding identification sign at 25’ high and monument sign at 5’ high located at 44800 Dillon Road with the Coachella Lakes RV Resort; Assessor’s Parcel No. 697-330-001, 005, 006, 007, 008, 009, 010, 011 (“Project”); and,

WHEREAS, the Planning Commission conducted a duly noticed public hearing for AR No. 22-08 on July 6, 2022, at 1515 Sixth Street, Coachella, California regarding the proposed Project; and,

WHEREAS, at the Planning Commission hearing, the Applicant and members of the public were present and were afforded an opportunity to testify regarding the Project; and,

WHEREAS, the proposed freestanding identification signs is necessary or desirable for the development of the community, is consistent with the objectives of the City’s General Plan, and is not detrimental to the existing uses or the uses specifically permitted in the zone in which the proposed use is to be located; and,

WHEREAS, the proposed freestanding identification signs are within an existing development “Coachella Lakes RV Resort” providing identification to the resort as a necessary structures to identify the development name for visitors and public safety response and direct visitors from I-10 and Dillon Road to the development as the proposed freestanding identification signs are within an existing development utilized; and,

WHEREAS, the proposed project is exempt from the California Environmental Quality Act, as amended; and, The City of Coachella has determined that the proposed project is categorically exempt from environmental review pursuant to the guidelines of the California Environmental Quality Act as an “Accessory Structures-On premise signs” (CEQA Guidelines, Section 15311, Class 11).

WHEREAS, the conditions as stipulated by the City are necessary to protect the public health, safety and welfare of the community.

NOW, THEREFORE, BE IT RESOLVED, that the Planning Commission of the City of Coachella, California does hereby resolve as follows:

Section 1. Incorporation of Recitals

The Planning Commission hereby finds that all of the facts in the Recitals are true and correct and are incorporated and adopted as findings of the Planning Commission as fully set forth in this resolution.

Section 2. CEQA Findings

The City of Coachella has determined that the proposed project is categorically exempt from environmental review pursuant to the guidelines of the California Environmental Quality Act as “Accessory Structures-On premise signs” (CEQA Guidelines, Section 15311). The proposed freestanding identification signs is associated with a previously approved project.

Section 3. Architecture Review

With respect to the Architecture Review No. 22-08, the Planning Commission finds as follows for the proposed freestanding identification signs:

1. The proposed use will not be in conflict with, but will be in harmony with and in accordance with the objectives of the Coachella General Plan 2035. The site has a General Neighborhood and Open Space land use designation with Sub-Area No.12 (North Dillon) policies, which encourage a final designation buildout with mixture of uses that includes 25% of the sub-area for Resort uses that allows gated communities. In addition, the Sub-Area No. 12 identifies a policy direction to “*capitalize on the access to the freeway and seek and encourage freeway oriented uses ...regional entertainment and recreation uses that benefit from easy regional access*”. The proposed freestanding identification signs will enable the visitors and public safety responders to locate the development. The signs also reflects the logo of the resort, thus potentially attracting visitors from I-10 Freeway.
2. The proposed use will be located, designed, constructed, operated and maintained so as to be compatible with the existing character of the general vicinity and shall not change the essential character of the same area because the proposed use is within a zoning designation of C-T, PD (Commercial Tourist Planned Development Overlay) which permits facilities such as clubhouse and registration office subject to obtaining a conditional use permit to sell alcoholic beverages. The proposed signs in the C-T, PD (Commercial Tourist Planned Development Overlay) are compatible with the constructed structures within the resort.
3. Consideration has been given to harmony in scale, bulk, coverage and density, to the availability of public facilities and utilities, to harmful effect, if any, upon desirable neighborhood character, to the generation of traffic and the capacity of surrounding streets, and to any other relevant impact of development, because the Coachella Lakes RV Resort is currently being constructed incorporating all infrastructure improvements to support the amenities in the development. The Clubhouse and Registration Office in which the alcohol license will apply to was part of a previously approved entitlements by Planning Commission as Environmental Assessment (EA 18-03), Conditional Use Permit (CUP 308), and Architecture Review (AR 18-18).

4. Where the proposed use may be potentially hazardous or disturbing to existing or reasonably expected neighboring uses, it must be justified by the common public interest as a benefit to the community as a whole. As conditioned, the proposed use will provide for the sale of alcoholic beverages. The Riverside County Sheriff indicated that there no concerns about the operations of this establishment including service of on-sale beer. The Sherriff’s Department not recommend conditions related to public safety concerns. As such, there will be no hazardous or disturbing effects to the existing and neighboring uses.
5. The proposed signs will be installed with the property of the Coachella Lakes RV Resort on the parkway and entrance to the development, which does not affect vehicular approaches to the property. Existing vehicular approaches from Dillon Road and Vista Del Norte will be utilized for the project. Existing on-site vehicular parking is sufficient.
6. The City of Coachella has determined that the proposed project is categorically exempt from environmental review pursuant to the guidelines of the California Environmental Quality Act as “Accessory Structures-On premise signs” (CEQA Guidelines, Section 15311). The proposed freestanding identification signs is associated with a previously approved project.

Section 4. Planning Commission Approval

Based on the forgoing recitals and findings above, and the written and oral comments, facts and evidence presented, the City of Coachella Planning Commission approves Architecture Review (AR No. 22-08) to install a freestanding identification sign at 25’ high and monument sign at 5’ high located at 44800 Dillon Road with the Coachella Lakes RV Resort.

PASSED APPROVED and ADOPTED this 6th day of July 2022 by the following vote:

AYES:

NOES:

ABSENT:

ABSTAIN:

Stephanie Virgen
Planning Commission Chairperson

ATTEST:

Gabriel Perez
Planning Commission Secretary

APPROVED AS TO FORM:

Carlos Campos
City Attorney

STATE OF CALIFORNIA)
COUNTY OF RIVERSIDE) ss.
CITY OF COACHELLA)

I HEREBY CERTIFY that the foregoing Resolution No. PC2022-20 was duly adopted by the Planning Commission of the City of Coachella at a regular meeting thereof, held on this 6th day of July 2022 by the following vote of the Planning Commission:

AYES:

NOES:

ABSENT:

ABSTAIN:

Gabriel Perez
Planning Commission Secretary

1	CS Cover Sheet	
2	S1 Illuminated, Primary Monument Sign	71.19 SQ FT
3	S1a Primary Monument Sign Footing Detail	
4	S1b Primary Monument Sign Lighting Detail	
5	S2 Illuminated, Entry Monument Sign	52.50 SQ FT
6	S3 Illuminated, Office Wall Sign	24.80 SQ FT
7	LTR Detail View / Section	
8	SM Site Plan / Vicinity Map	

Design / Fabrication
Image Services, Inc.
CA. LIC. #978517 - C10 / C45
 2281 La Crosse Ave. Ste 501, Colton, CA. 90324
 PH - 909-370-4500
 Email: alfonsoa@imageservicesinc.com

EXTERIOR SIGNAGE PROPOSAL PLANS
 Design Plans and Specs - Sheets - 8

SUBMITTAL RECORD	
DATE	REMARKS

Updated 2021



IMAGE SERVICES, INC.
 SIGNS • LIGHTING • ELECTRICAL
 CA. LIC. #978517 - C10 - C45

Coachella Lakes RV Resort
44-800 Dillon Rd.
Coachella, CA 92236

EXTERIOR SIGNAGE

R2 06/30/2022

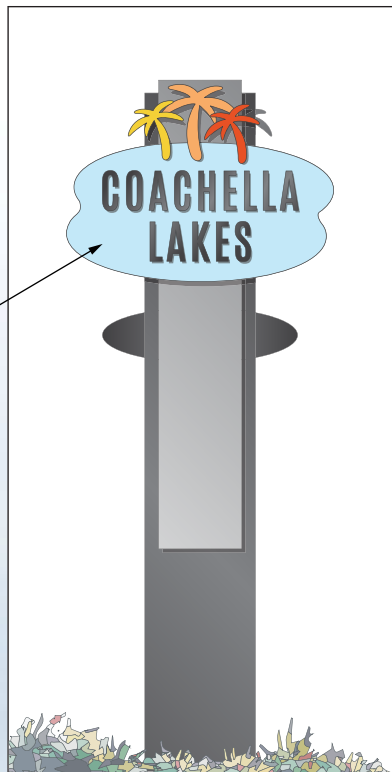
DESIGN APPROVAL:



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 Cost of providing necessary wiring to sign area is not included in this sign proposal.

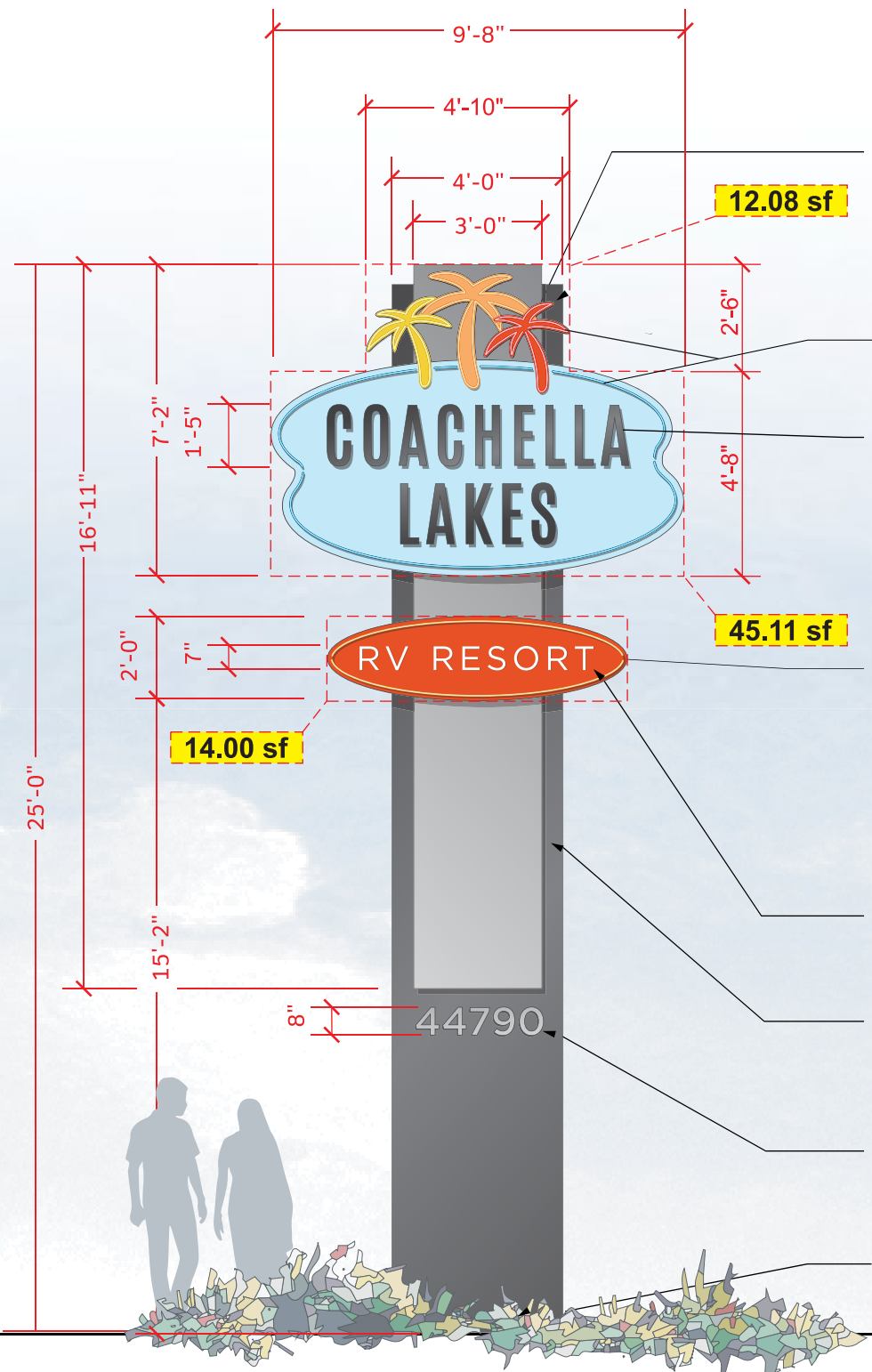
S1 Primary Monument Sign

Item 4.

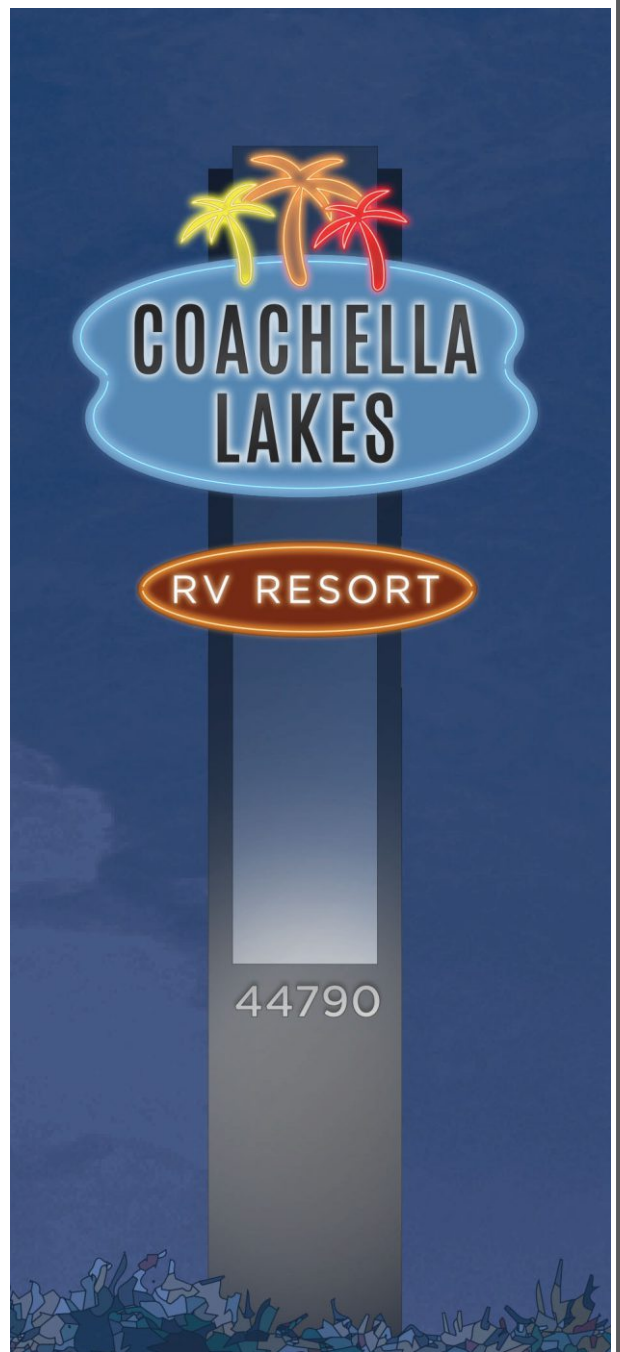


Non-illuminated dimensional logo on back of sign facing project interior

Back of Sign - Not to Scale



- Dimensional aluminum cabinet attached to structure as required - painted to match projects logo colors - returns to match support pole color/finish
- Exposed neon border and palm tree elements (blue, red, orange & yellow)
- Reverse pan channel letters mounted off cabinet face as required to allow halo illumination
- Internally illuminated dimensional aluminum cabinet attached to structure as required - painted to match projects logo orange/red - returns to match support pole color/finish - exposed orange neon border
- White plex back-up of routed copy to provide face illumination
- Multi-level aluminum support structure - painted to match brushed aluminum and charcoal black (TBD)
- FCO address numerals flush mounted to cabinet structure - possible ground up-lighting
- Footing as required through engineering



Conceptual Night View

S1 Coachella Lakes - Primary Monument
NTS

45.11 sf + 12.08 sf + 14.00 sf = 71.19 sf Total

Front View

IMAGE SERVICES, INC.
Signs • Lighting • Electrical
855-754-6243
CALIFORNIA CONTRACTOR LICENSE NUMBER 978517

Project Name: Coachella Lakes RV Resort
 Address: 44-800 Dillon Rd.
Coachella, CA 92236
 Drawing #: _____ Date: R2 06/30/2022

Revision Sequence:

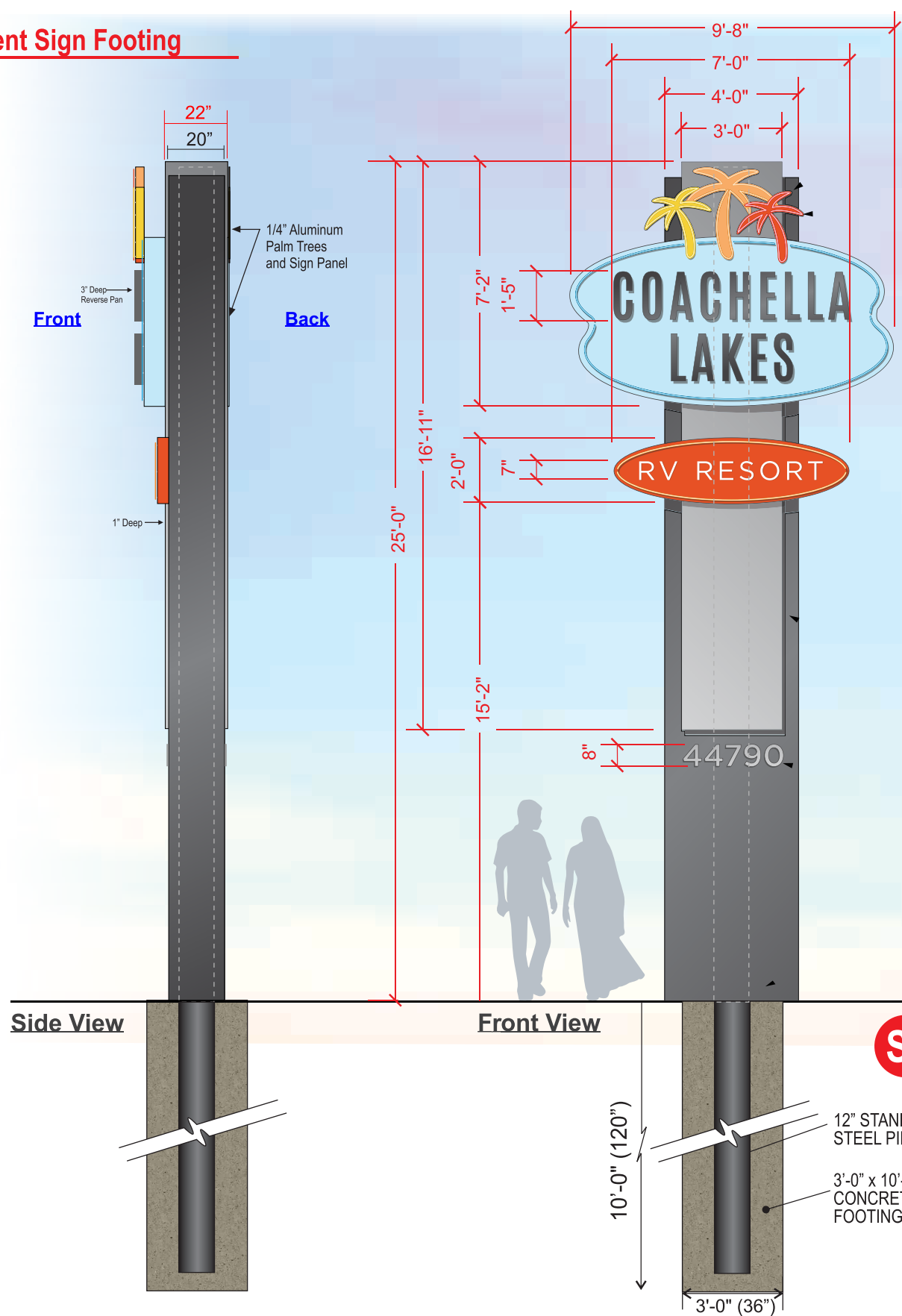
 Scale: As Noted

(Customer Signature) _____ (Date) _____
 Approved and accepted with changes to drawing
 Approved and accepted with changes with hand notes added
 Approved and accepted with no changes to drawing

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S1a Primary Monument Sign Footing

Item 4.



PAINT COLORS	
	PAINTED FINISH - CHARCOAL BLACK
	PAINTED FINISH - BRUSHED ALUMINUM
	PAINTED FINISH - RED #_____
	PAINTED FINISH - LIGHT ORANGE#_____
	PAINTED FINISH - YELLOW #_____
	PAINTED FINISH - LIGHT BLUE #_____
	PAINTED FINISH - ORANGE-RED #_____

Side View

Front View

S1 Manufacture and Install One D/F Illuminated Primary Entry Monument Sign
NTS

12" STANDARD STEEL PIPE
3'-0" x 10'-0" CONCRETE FOOTING

IMAGE SERVICES, INC.
Signs • Lighting • Electrical
855-754-6243
CALIFORNIA CONTRACTOR LICENSE NUMBER 978517

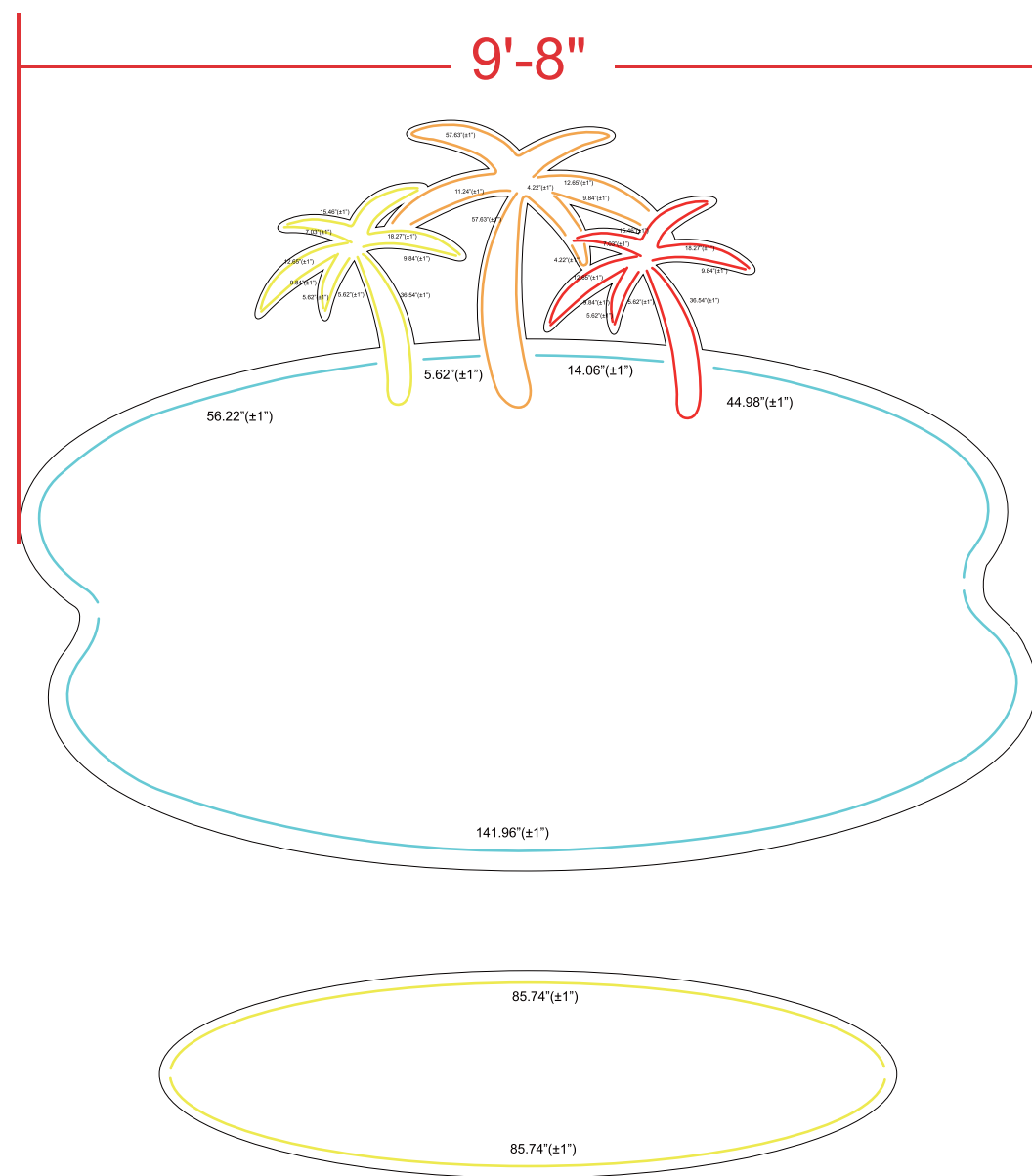
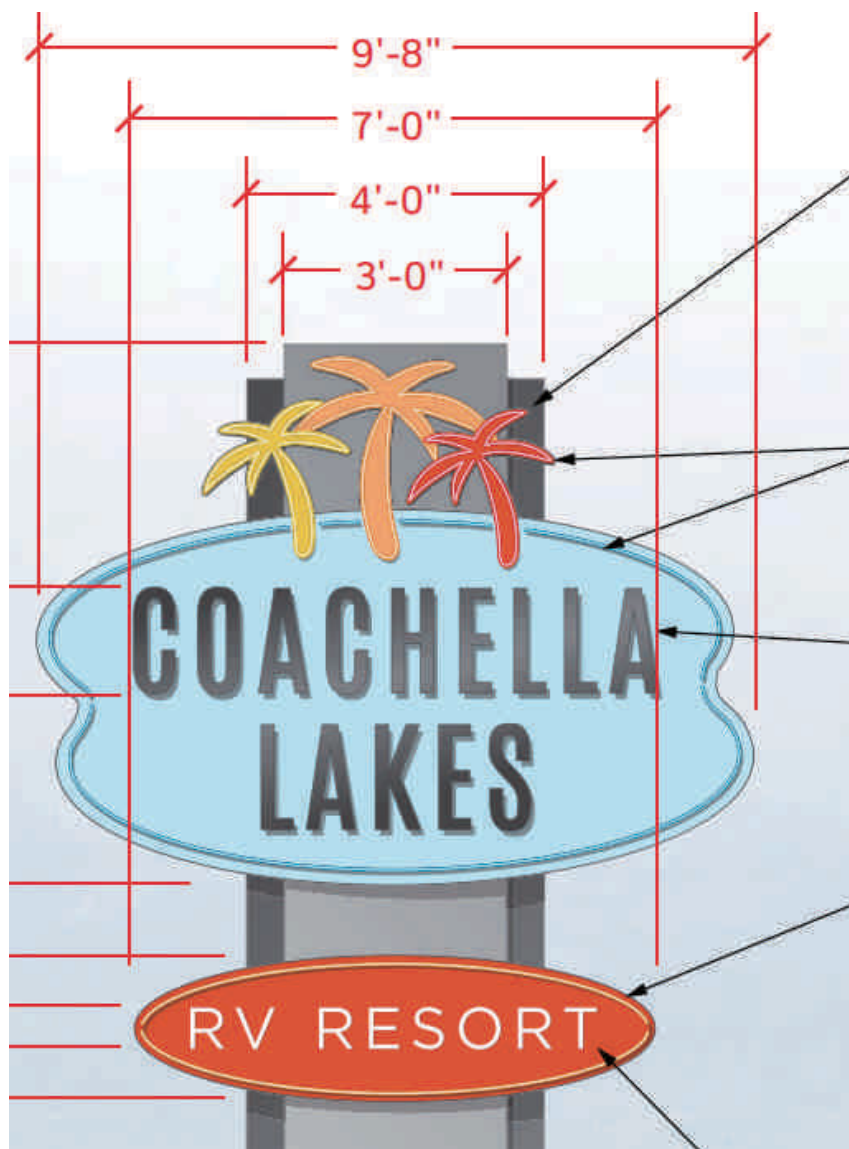
Project Name: Coachella Lakes RV Resort
Address: 44-800 Dillon Rd.
Coachella, CA 92236
Drawing #: _____ Date: R2 06/30/2022

Revision Sequence:

Scale: As Noted

(Customer Signature) _____ (Date) _____
 Approved and accepted with changes to drawing
 Approved and accepted with changes with hand notes added
 Approved and accepted with no changes to drawing

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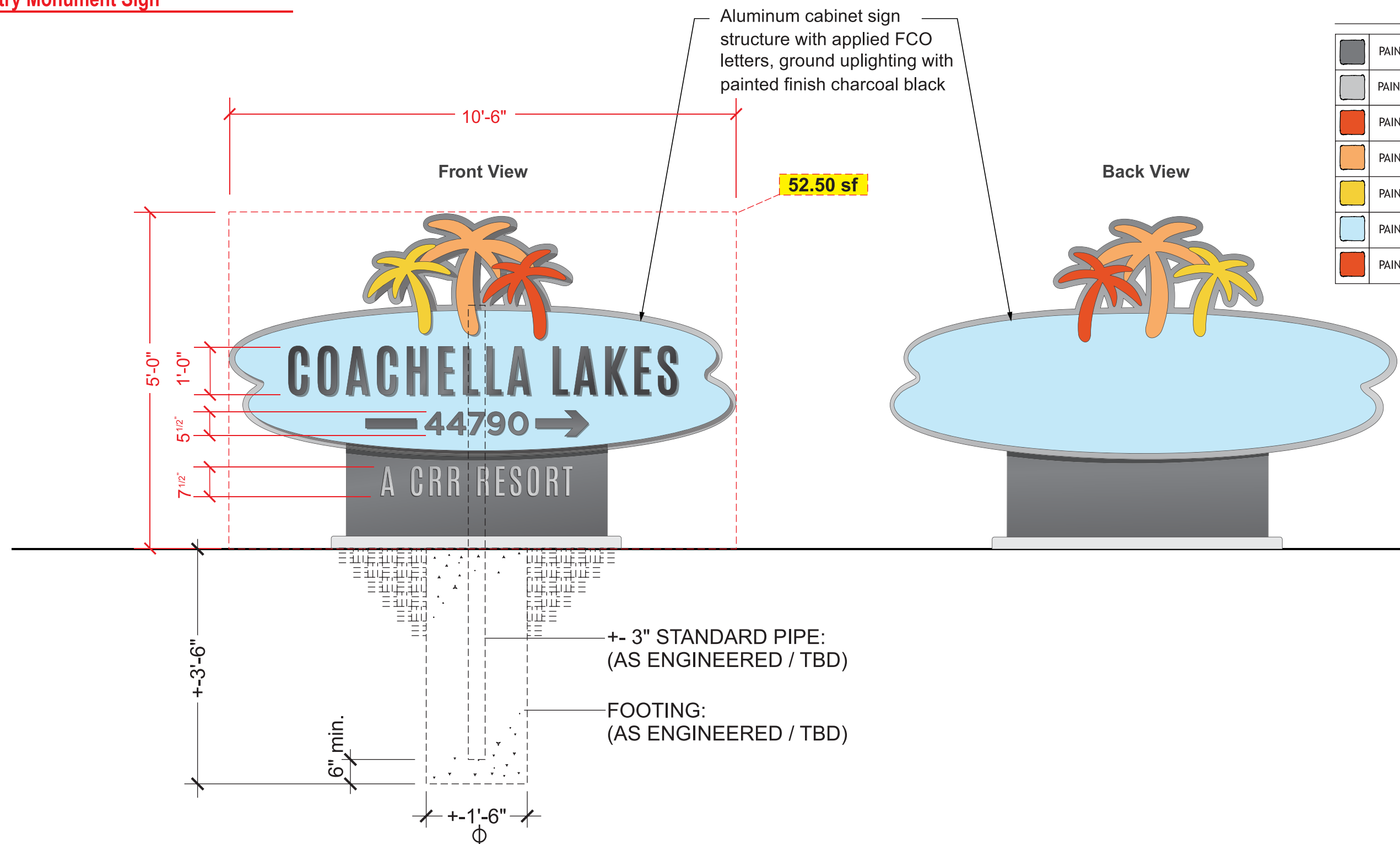
Illuminated Primary Entry Monument Sign - LED Tube Neon

NTS

SELECTION CRITERIA		NEON LIGHTS	Project: _____ Date: <u>03/04/2022</u> Approved By: _____	AF2004 Aurora Flex Mini Ice Blue: 266.6in 62.2w AF2005 Aurora Flex Mini Red: 128.84in 27.37w AF2006 Aurora Flex Mini Yellow: 301.98in 70.46w AF2008 Aurora Flex Mini Orange: 164.19in 38.31w AFA2002 Aurora Flex MINI Fast Dry Glue: 1pcs AFA1019 Aurora Flex Silicone Squeezing Accessory: 1PCS AFA1018 Aurora Flex Silicone: 4pcs AFA2004 Aurora Flex Mini Crystal Mounting Clips: 430pcs AFA2005 Aurora Flex Mini Mounting accessories: 430pcs AFA200301 Aurora Flex MINI End Cap with Wires: 32pcs AFA2001 Aurora Flex Mini End Cap: 32pcs AFA2010 Aurora Flex Mini Boots: 64PCS QISPS1001 G2G-QIS -60W-12VDC: 4pcs
Color	Ice Blue,Red,Yellow,Orange	<input type="checkbox"/> Aurora Flex <input checked="" type="checkbox"/> Aurora Flex MINI <input type="checkbox"/> Aurora Flex Micro <input type="checkbox"/> Aurora Flex Curve		
Total Quantity	861.6 [IN]			
Total Power Consumption	201.0 [Watts]			
Power Supplies 60w 12v	4 [PCS]			
MOUNTING BRACKETS	[PCS]			

S2 Entry Monument Sign

Item 4.



S2 Manufacture and Install One (1) D/F Entry Monument
NTS

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CALIFORNIA CONTRACTOR LICENSE NUMBER 978517

Project Name: Coachella Lakes RV Resort
Address: 44-800 Dillon Rd.
Coachella, CA 92236
Drawing #: _____ Date: R2 06/30/2022

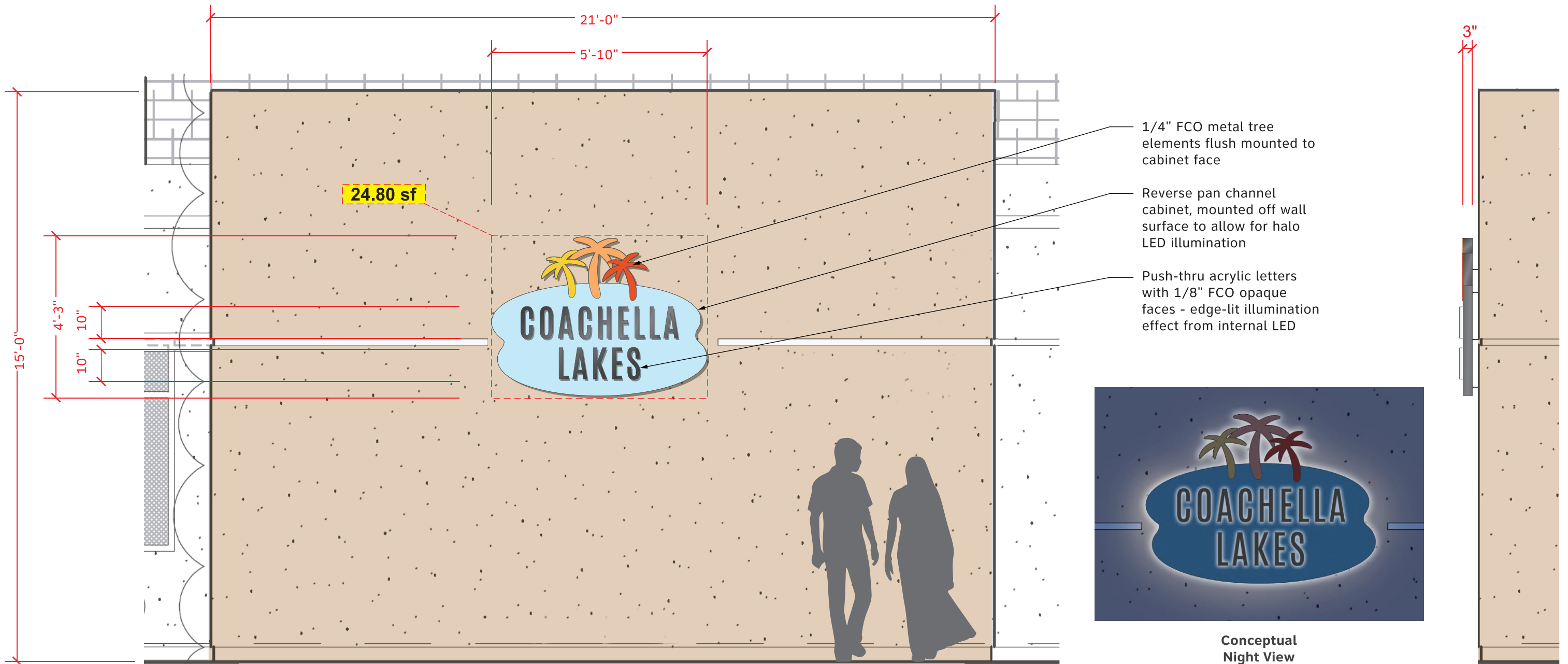
Revision Sequence:

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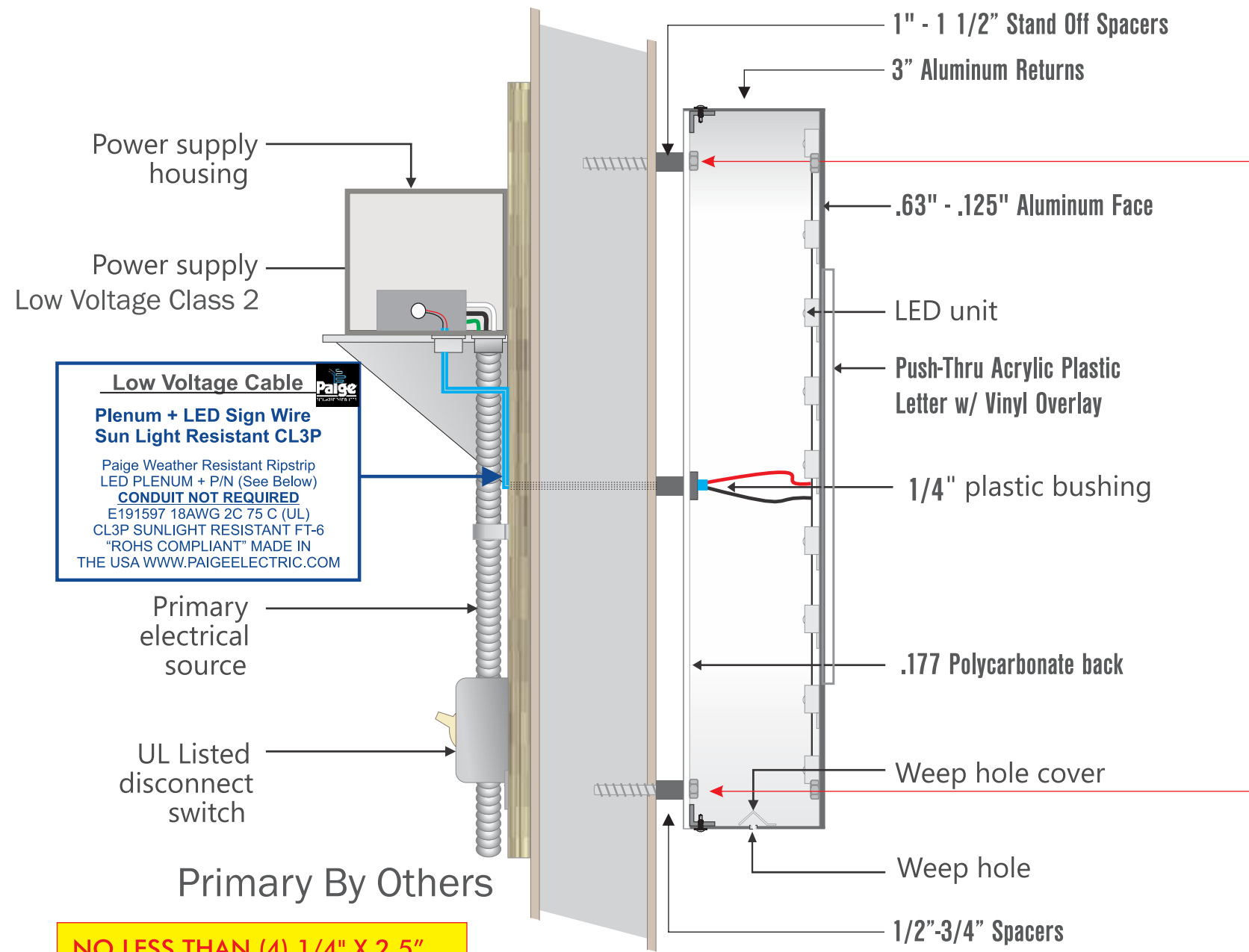
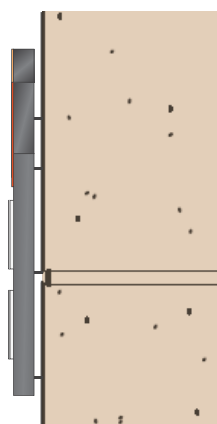
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S3 Coachella Lakes - Office Wall Sign
NTS

Stucco with Wood Backer



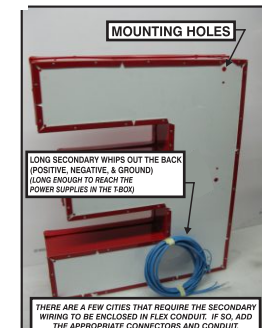
NO LESS THAN (4) 1/4" X 2.5" HEX HEAD SCREWS PER UNIT MOUNTED IN NYLON ANCHOR WITH MINIMUM 2" EMBEDMENT

ALL ELECTRICAL COMPONENTS



APPROVED

ELECTRIC SIGN SECTION



VERY IMPORTANT!
 WHEN INSTALLING LETTERS, USE CLEAR SILICONE SEALANT TO SEAL AROUND SECONDARY HOLES, ALL BUILDING PENETRATIONS, & RUN A BEAD ACROSS THE TOPS OF ALL LETTERS, IN ORDER TO PREVENT LEAKS.

SIGN TO BE U.L. LISTED AND SHALL MEET N.E.C. STANDARDS
 Underwriters Laboratories Inc.
ELECTRIC SIGN

ALL ELECTRICAL COMPONENTS TO BE U.L. LISTED, APPROVED AND MARKED PER N.E.C. 600-4. ALL TO BE ELECTRICALLY GROUNDED PER N.E.C. 250. ALL POWER SUPPLIES TO BE FUSED PER U. L. 48, 28.2.1 GROUNDING AND BONDING PER N.E.C. 250-90, -92, -94, -96. SIGN TO MEET N.E.C. AND U. L. 48 STANDARDS FOR ELECTRICAL SIGNS. GROUNDING & BONDING AS PER N.E.C. 250-90 & NEC 600-7

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 855-754-6243
 CALIFORNIA CONTRACTOR LICENSE NUMBER 978517

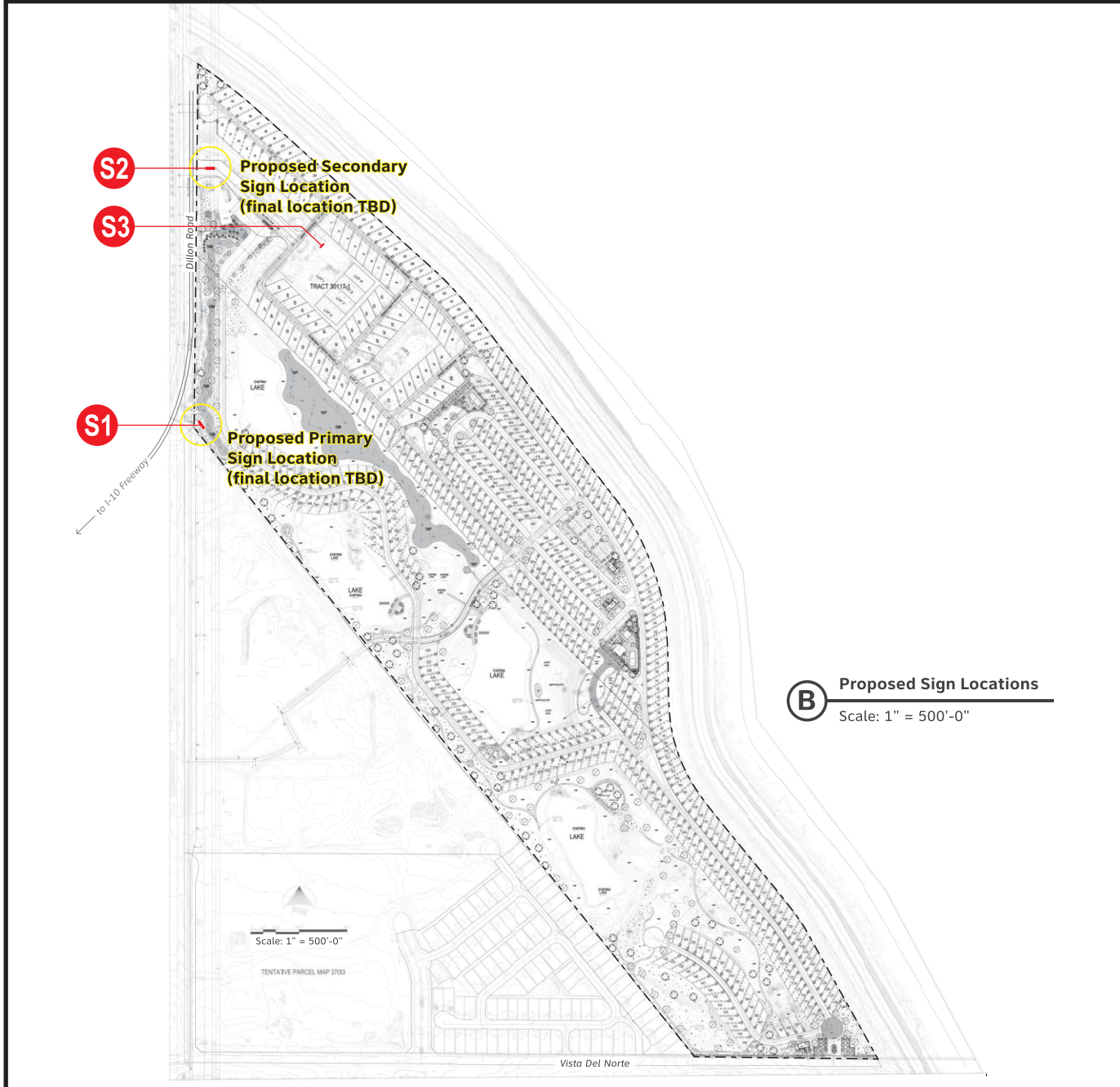
Project Name: Coachella Lakes RV Resort
 Address: 44-800 Dillon Rd.
Coachella, CA 92236
 Drawing #: _____ Date: R2 06/30/2022

Revision Sequence:

 Scale: As Noted

(Customer Signature) _____ (Date) _____
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 Approved and accepted with changes with hand notes added
 Approved and accepted with no changes to drawing

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SITE PLAN: Sign Locations NTS 
NORTH

VICINITY MAP NTS 
NORTH

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 855-754-6243
 CALIFORNIA CONTRACTOR LICENSE NUMBER 978517

Project Name: Coachella Lakes RV Resort
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STAFF REPORT
7/6/2022

TO: Planning Commission Chair and Commissioners

FROM: Nikki Gomez, Associate Planner

SUBJECT: Coachella Lakes RV Resort – Type 41 Alcohol Sales

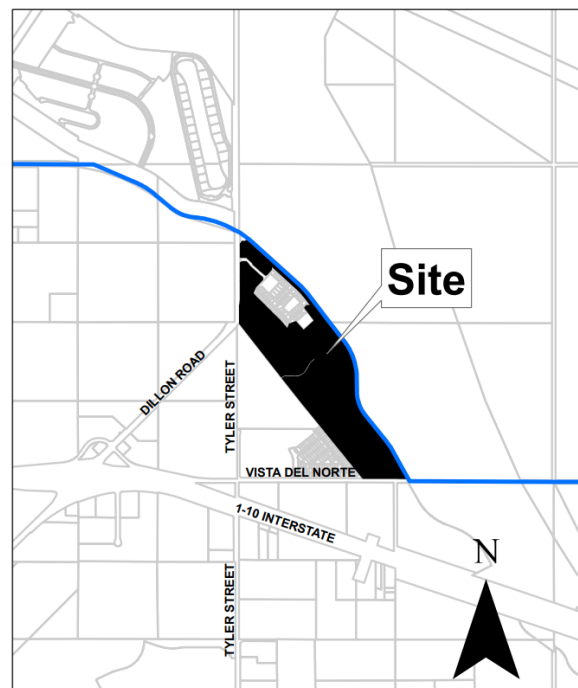
SPECIFICS: Conditional Use Permit No. 353 to allow liquor sales to allow for a Type 41 Alcohol Beverage Control license for “On-Sale Beer & Wine – Eating Place” for an existing facility registration office and a new clubhouse located at 44790 & 44800 Dillon Road and 86401 Sonoma Creek in the C-T, PD (Commercial Tourist Planned Development Overlay) . Coach RV LLC (Applicant) Continued from June 15, 2022 Planning Commission meeting.

STAFF RECOMMENDATION:

Staff recommends that the Planning Commission adopt Resolution No. PC 2022-19 approving Conditional Use permit No. 353 to allow a liquor sales license (Type 41 On-Sale Beer & Wine – Eating Place) for “registration office” and “clubhouse” located at 44790 & 44800 Dillon Road and 86401 Sonoma Creek within the Coachella Lakes RV Resort pursuant to the findings and conditions of approval contained in the attached resolution.

BACKGROUND:

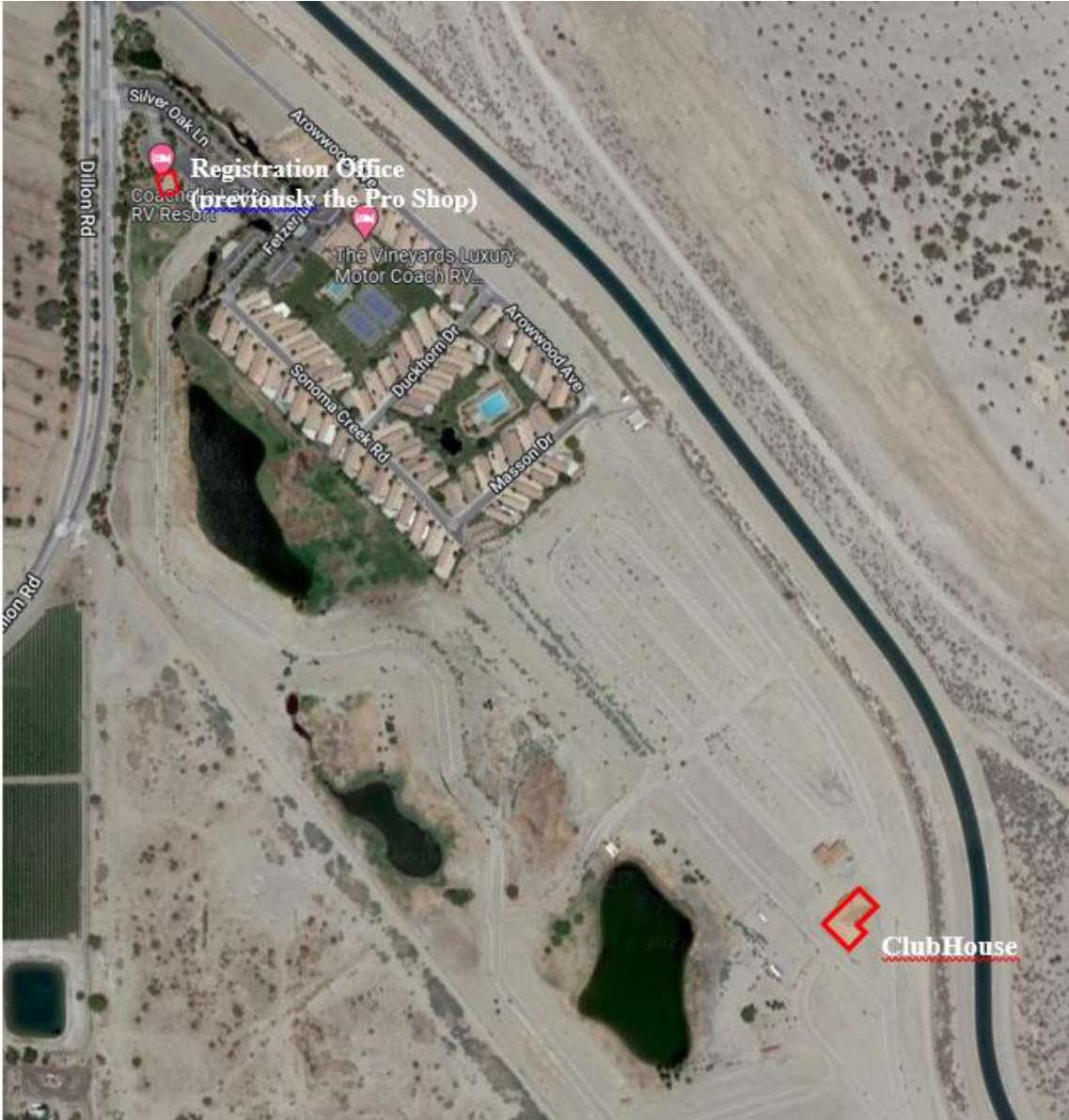
On March 6, 2019, the Planning Commission approved the Red Moon RV Park project on 78.3 acres of vacant land located within the existing Vineyards Luxury Motorcoach Resort at 44-790 Dillon Road. This project is a deviation from the existing Vineyards Luxury Motorcoach Resort that consisted of subdivided lots for both RV parking and “Casitas”. The Red Moon RV Park project was approved with Environmental Assessment (EA 18-03), Conditional Use Permit (CUP 308), and Architecture Review (AR 18-18) for a Recreational Vehicle (RV) Campground consisting of 469 RV parking rental spaces with no structures, clubhouse, laundry/restroom buildings with private streets and common area amenities.



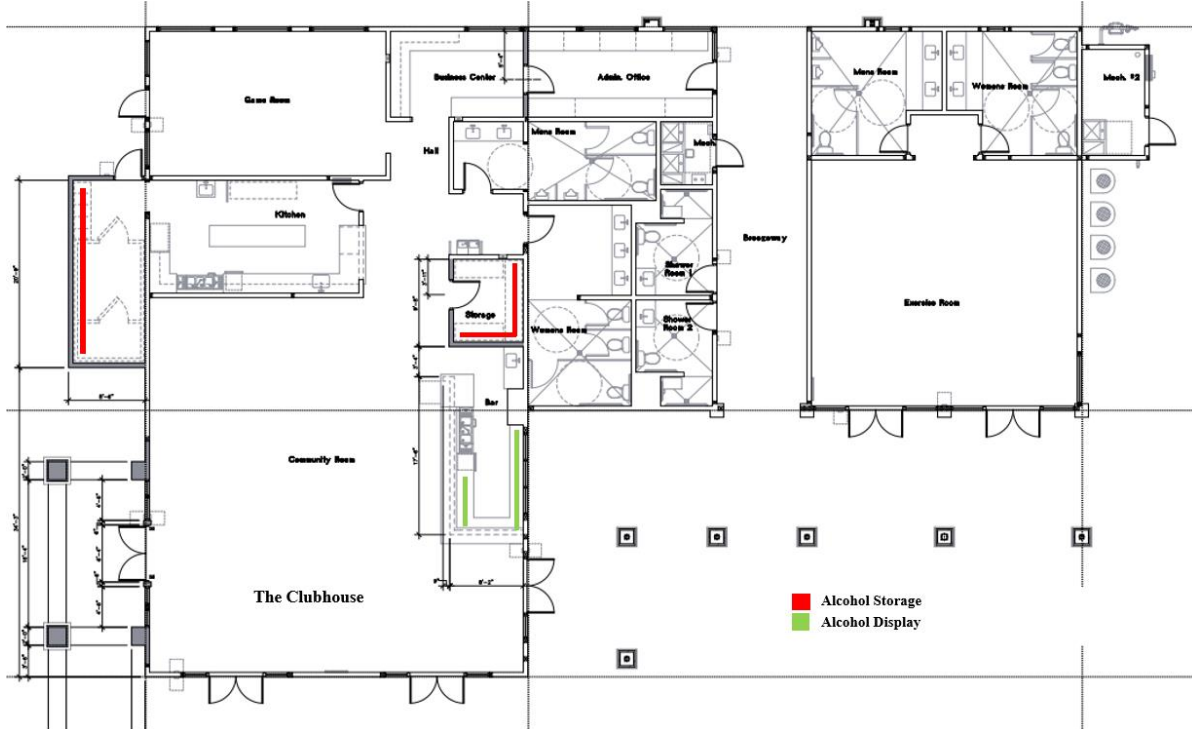
The development name “Red Moon RV Park” was recently renamed to “Coachella Lakes RV Resort” and is under construction. The Type 41 (On-Sale Beer & Wine – Eating Place) alcohol license will be utilized for the existing facility the Registration Office which was previously referred to as the “Pro Shop” and the Clubhouse. The aerial map below shows the Registration office at the northwest area of the development and the Clubhouse is centrally located with the development. The applicant requested for the proposed project to be continued on date certain of July 6, 2022 as the applicant needed more time to review and evaluate the conditions of approval.

DISCUSSION/ANALYSIS:

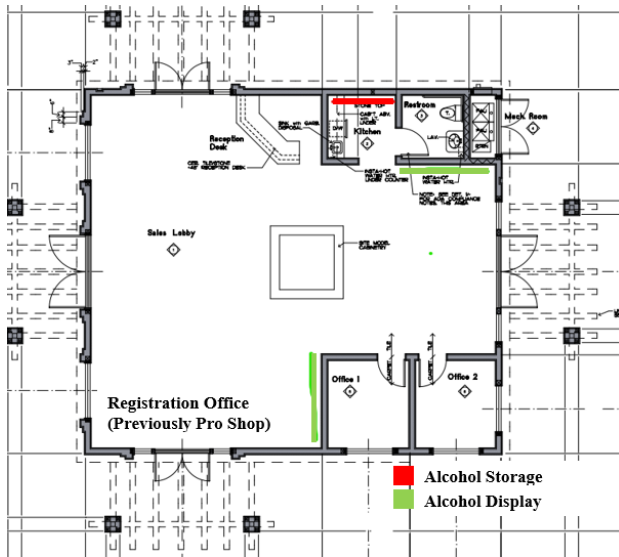
The applicant, Coach RV LLC, has submitted a request for a Conditional Use Permit (CUP) to allow Type 41 On-Sale Beer & Wine – Eating Place. The approval of a Type 41 Alcohol license *authorizes the sale of beer and wine for consumption on or off the premises where sold. Distilled spirits may not be on the premises (except brandy, rum, or liqueurs for use solely for cooking*



purposes). The establishment must operate and maintain the licensed premises as a bona fide eating place. Must maintain suitable kitchen facilities, and must make actual and substantial sales of meals for consumption on the premises. Minors are allowed on the premises. According to the Alcohol and Beverage Control (ABC), if an establishment has a bona fide restaurant, the applicant has the privilege to sell alcohol for offsite consumption. The clubhouse shown below, will have a



bona fide restaurant, therefore the applicant will have the authorization to sell alcohol for offsite consumption. ABC considers a bona fide eating-place as *Licensed premises that are maintained in good faith and used for the regular service of meals to patrons. The premises must have suitable kitchen facilities and supply an assortment of foods commonly ordered at various hours of the day.* The Clubhouse floorplan shows the kitchen, dining/community room, bar, game room, office, restrooms with a breezeway separating the exercise room. The areas highlighted in red will be location of the alcohol storage and the area highlighted in green is the alcohol bar display.



The Registration Office, previously the Pro Shop, is undergoing minor renovation to incorporate two offices and interior and exterior enhancements. The registration office shows the reception area, offices, and bathroom. It also identifies the alcohol storage and display. Renters and visitors may purchase alcohol for the display areas.

The zoning designation for the subject property is CT-PD (Commercial Tourist Planned Development Overlay) and allows liquor sales with approval of a CUP. In December of 2016, the City adopted an ordinance requiring a conditional use permit for any off-sale and on-sale alcohol sales establishment, with additional

land use regulations contained in Section 17.74.015 of the Zoning Code. The Conditional Use Permit findings are required to be made by the Planning Commission.

Section 17.74.015(C)(2) of the Municipal Code establishes that on-sale establishments must comply with the following requirements:

- a. **The establishment does not fall within seven hundred (700) feet, measured from property line to property line, of a use for religious worship, school (public or private), park, playground, residential or any similar sensitive use; and**

The establishment does fall within a residential community Villas at the Vineyards abutting at the southwest side of the project.

- b. **The establishment does not fall within seven hundred (700) feet, measured from property line to property line, of an existing liquor, on-sale use; and**

The establishment is not within 700 feet of the any existing liquor on-sale use.

- i. **The planning commission may find that the public benefit outweighs the distance restrictions of subsections (a) and/or (b) above, upon additional findings that:**

Staff recommends that the Planning Commission makes the finding that the public convenience or necessity is justified to issue the on or off-sale beer, wine (Type 41 Alcohol License) for the clubhouse which offers services such as a restaurant in the nearby neighborhood.

- (A) **The establishment provides a substantial benefit to the immediate neighborhood not provided by another existing establishment within the restricted distance; and**

The establishment provides benefit to the immediate neighborhood, as the clubhouse one of the facilities will have a restaurant on the most northern part of the city north of the I-10 Freeway.

- (B) **Not greater than five percent of the sales floor area or one thousand (1,000) square feet, whichever is less, shall be used for the display or sale of alcoholic beverages;**

The Registration Office and Clubhouse shows the location of the display and storage of the beer and wine. The Registration Office beer and wine storage is 3.4 percent which does not exceed the five percent or the 1,000 sf for the display and storage of alcoholic beverages. The Clubhouse beer and wine storage is 4.3 percent, which does not exceed the five percent or the 1,000 sf for the display and storage of alcoholic beverages.

- c. **That the requested establishment at the proposed location will not adversely affect the economic welfare of the nearby community; and**

The establishment will not adversely affect the economic welfare of the nearby community since the alcohol sales will be associated with a new facility (Clubhouse) which will provide amenities such as restaurant, pool area and exercise room conducive to renters and visitor resort experience in the Coachella Lakes RV Resort. The establishment is within an RV Resort where visitors and renters can access the facility with ease promoting walkability within the development. The Resort is located almost 2 miles from the nearest licensed on-premises establishment, so the ability to purchase alcoholic beverages at the Resort without having to leave to drive to another licensed establishment will provide a tremendous convenience for the visitors and renters. Furthermore, the ability to sell alcoholic beverages adds to the viability of the Resort's business model, as the public expects that a luxury RV park of this nature will offer alcoholic beverages. However, staff added conditions to the project to address the landscaping along the perimeter of the project and median along Dillon Rad. One of the condition is for the applicant to form a Lighting and Landscaping Maintenance District to maintain the median island and street lighting. The perimeter landscaping and perimeter walls shall be included in the property owner's association for maintenance, repair and replacement. In the meantime, the applicant will provide landscape maintenance along the perimeter of the project along Dillon Road and Vista Del Norte.

- d. **That the exterior appearance of the structure of the proposed establishment will not be inconsistent with the exterior appearance of commercial structures already constructed or under construction within the immediate neighborhood so as to cause blight or deterioration, or substantially diminish or impair the property values within the neighborhood.**

The exterior appearance of the structure of the proposed establishments will be consisted with the exterior of the appearance of the commercial structures already constructed. The

Registration Office is undergoing minor renovation, which will enhance interior layout and exterior facade. The Clubhouse is under construction. The Clubhouse will have a Spanish Mediterranean design with stucco finish, s-tile roofing, tower elements, decorative quatrefoils, and decorative window treatments to match the existing building at the Vineyards Luxury Motorcoach Resort.

- D. Additionally, if the California Department of Alcoholic Beverage Control (ABC) notifies the City that the proposed establishment is either (1) subject to the State law moratorium/limit on the number of liquor licenses that may be issued within the city under California Business and Professions Code Section 23817.7 or, (2) is in an area of "undue concentration" of liquor licenses, as defined in California Business and Professions Code Section 23958.4; the planning commission must additionally find that the public convenience or necessity nonetheless justifies the issuance of the liquor license to the establishment.**

The subject site is located within Census Tract 452.22, where ABC concentration standards allow a maximum of 2 on-sale licenses, where 4 exists as shown on Table 1. When it is determined by ABC that there is an undue concentration of on-sale licenses, the Planning Commission must make findings that the public convenience or necessity justifies the issuance of the liquor license to the establishment.

Table 1 – On-Sale Alcohol Licenses

Off-Sale Alcohol License within Census Tract 452.22			
	Business Name	Address	License Type
1	Fantasy Springs Casino	84245 Indio Springs, Indio, CA 92203	68 (Portable Bar License)
2	Fantasy Springs Casino	84245 Indio Springs, Indio, CA 92203	47 (On Sale General – Eating Place)
3	Fantasy Springs Casino	84245 Indio Springs, Indio, CA 92203	58 (Caterer’s Permit)
4	La Paloma Restaurant	43990 Golf Center Ste B1, Indio CA 92203	41 (On-Sale Beer and Wine – Eating Place)

Staff contacted Lieutenant Vasquez with the Riverside County Sheriff regarding any concerns or complaints for the Type 41 Liquor License for the Registration Office and Clubhouse within the Coachella Lakes RV Resort and he did not identify any concerns with the applicant for the Coachella Lakes RV Resort Alcohol License and does not recommend any conditions of approval.

Hours of Operation:

The operating hours for the Registration Office and the Clubhouse is from 8:00 a.m. to 10:00 p.m. daily.

Environmental Setting:

The subject site is within an existing development, the Registration Office is located at 44790 & 44800 Dillon Road and the Clubhouse located at 86401 Sonoma Creek. The project site is substantially surrounded by open space and some development, with adjoining zoning and land uses as follows:

North: All-American Canal/Unincorporated County – Landfill (N-A County Zone)

South: Vacant Land (MS Manufacturing Service/ CG General Commercial Zone)

East: All-American Canal/Eastside Dike (N-A County Zone)

West: Vacant Land (CT – Commercial Tourist/RMH – Residential Mobile Home)

Site Plan / Parking and Circulation:

The project site main entrance is located on Dillon Road with secondary access from Vista Del Norte. The project site main entrance is located on Dillon Road with secondary access from Vista Del Norte. Both of the Registration Office and Clubhouse will be open to the public, therefore the Restaurant and Office Parking Standard under the Municipal Code will apply. Under 17.51.010 Restaurants requires *one space for each forty-five (45) square feet of customer area, plus one space for each two hundred (200) square feet of noncustomer area*. The restaurant floorplan shows 1,511 square feet of customer area with 1,216 square feet of non-customer area resulting in 40 required parking spaces. The Registration Office will require *one parking space per (250) square feet*. The registration office floorplan shows 1,745 square feet resulting in 14 parking spaces. The combined required parking spaces within the property is 53 parking spaces. The Coachella Lakes RV Resort has a combined parking area including golf cart parking throughout the development is 92 parking spaces. Since a large part of the customers that will utilize these facilities are within the Resort, other on-site parking is used to satisfy the parking requirement considering that RV renters may utilize alternative means such as walking and use of golf carts to access the facilities.

ENVIRONMENTAL REVIEW:

Staff has determined that the proposed project is categorically exempt from environmental review pursuant to the guidelines of the California Environmental Quality Act as “Existing Facilities” (CEQA Guidelines, Section 15301). The proposed project consists of the operation, licensing and minor alteration of an existing private commercial structure involving no expansion of existing or former commercial use on the property. The subject site has been used for commercial retail establishments and no expansions of floor area are proposed.

CONCLUSIONS AND RECOMMENDATIONS

1. Based on the analysis contained herein and the findings listed below, staff is recommending that the Planning Commission approve Conditional Use Permit No. 353 with the findings and conditions listed in Resolution No. PC 2022-19, and in the attached resolution.

2. Deny the proposed project.
3. Continue this item and provide staff and the applicant with direction.

Attachments:

1. Resolution No. PC 2022-19
2. Exhibit A to Resolution No. PC 2022-19
3. Project Plans
4. Vicinity Map
5. Existing Conditions

RESOLUTION NO. PC 2022-19

A RESOLUTION OF THE PLANNING COMMISSION OF THE CITY OF COACHELLA, CALIFORNIA APPROVING CONDITIONAL USE PERMIT NO. 353 TO ALLOW ON OR OFF SALE OF BEER SPIRITS FOR THE REGISTRATION OFFICE AND CLUBHOUSE (ABC LICENSE TYPE 41-ON-SALE BEER & WINE – EATING PLACE) WITHIN THE COACHELLA LAKES RV RESORT LOCATED AT 44790 & 44800 DILLON ROAD AND 86401 SONOMA CREEK. (APN 697-330-001, 005, 006, 007, 008, 009, 010, 011); COACH RV LLC, APPLICANT.

WHEREAS, Coach RV LLC filed an application for Conditional Use Permit No. 353 (CUP 353) to allow on or off sales of beer and wine (ABC License Type 41 – On-Sale Beer & Wine – Eating Place) in an the Registration Office and Clubhouse within the Coachella Lakes RV Resort located at 44790 & 44800 Dillon Road and 86401 Sonoma Creek.; Assessor’s Parcel No. 697-330-001, 005, 006, 007, 008, 009, 010, 011 (“Project”); and,

WHEREAS, the Planning Commission conducted a duly noticed public hearing on CUP No. 353 on June 15, 2022, the project was continued to a date certain of July 6, 2022 at the 1515 Sixth Street Coachella, California regarding the proposed Project; and,

WHEREAS, at the Planning Commission hearing, the Applicant and members of the public were present and were afforded an opportunity to testify regarding the Project; and,

WHEREAS, the proposed use is necessary or desirable for the development of the community, is consistent with the objectives of the City’s General Plan, and is not detrimental to the existing uses or the uses specifically permitted in the zone in which the proposed use is to be located; and,

WHEREAS, the proposed establishments is within the Coachella Lakes RV Resort and proposed to utilize an ABC Type 41 On-Sale Beer & Wine – Eating Place; and,

WHEREAS, the site for proposed use relates properly to streets which are designed to carry the type and quantity of traffic to be generated by the proposed use; and,

WHEREAS, the proposed project is exempt from the California Environmental Quality Act, as amended; and, The City of Coachella has determined that the proposed project is categorically exempt from environmental review pursuant to the guidelines of the California Environmental Quality Act as an “Existing Facilities” project (CEQA Guidelines, Section 15301a) as the proposed on sale service of liquor will occur in conjunction with an establishment in an approved RV Resort development.

WHEREAS, the conditions as stipulated by the City are necessary to protect the public health, safety and welfare of the community.

NOW, THEREFORE, BE IT RESOLVED, that the Planning Commission of the City of

Coachella, California does hereby resolve as follows:

Section 1. Incorporation of Recitals

The Planning Commission hereby finds that all of the facts in the Recitals are true and correct and are incorporated and adopted as findings of the Planning Commission as fully set forth in this resolution.

Section 2. CEQA Findings

The City of Coachella has determined that the proposed project is categorically exempt from environmental review pursuant to the guidelines of the California Environmental Quality Act as an “Existing Facilities” project (CEQA Guidelines, Section 15301a) as the proposed on sale service of liquor will occur in conjunction with an existing business in an existing commercial building.

Section 3. Conditional Use Permit

With respect to the Conditional Use Permit (CUP) 353, the Planning Commission finds as follows for the proposed liquor license sales:

1. The proposed use will not be in conflict with, but will be in harmony with and in accordance with the objectives of the Coachella General Plan 2035. The site has a General Neighborhood and Open Space land use designation with Sub-Area No.12 (North Dillon) policies which encourage a final designation buildout with mixture of uses that includes 25% of the sub-area for Resort uses that allows gated communities. In addition, the Sub-Area No. 12 identifies a policy direction to “*capitalize on the access to the freeway and seek and encourage freeway oriented uses ...regional entertainment and recreation uses that benefit from easy regional access*”. The proposed Type 41- On-Sale Beer & Wine – Eating Place liquor license support the Coachella Lakes RV Resort development by enhancing the visitor and renter’s experience, while utilizing on site amenities such as the Clubhouse.
2. The proposed use will be located, designed, constructed, operated and maintained so as to be compatible with the existing character of the general vicinity and shall not change the essential character of the same area because the proposed use is within a zoning designation of C-T, PD (Commercial Tourist Planned Development Overlay) which permits facilities such as clubhouse and registration office subject to obtaining a conditional use permit to sell alcoholic beverages. The proposed use in the C-T, PD (Commercial Tourist Planned Development Overlay) and is compatible with the adjoining commercial uses and the conditional use permit can be revoked if any of the conditions of approval are violated.
3. Consideration has been given to harmony in scale, bulk, coverage and density, to the availability of public facilities and utilities, to harmful effect, if any, upon desirable neighborhood character, to the generation of traffic and the capacity of surrounding

streets, and to any other relevant impact of development, because the Coachella Lakes RV Resort is currently being constructed incorporating all infrastructure improvements to support the amenities in the development. The Clubhouse and Registration Office in which the alcohol license will apply to was part of a previously approved entitlements by Planning Commission as Environmental Assessment (EA 18-03), Conditional Use Permit (CUP 308), and Architecture Review (AR 18-18).

- 4. Where the proposed use may be potentially hazardous or disturbing to existing or reasonably expected neighboring uses, it must be justified by the common public interest as a benefit to the community as a whole. As conditioned, the proposed use will provide for the sale of alcoholic beverages. The Riverside County Sheriff indicated that there no concerns about the operations of this establishment including service of on-sale beer. The Sherriff’s Department not recommend conditions related to public safety concerns. As such, there will be no hazardous or disturbing effects to the existing and neighboring uses.
- 5. The proposed use provides vehicular approaches to the property designed for reasonable interference with traffic on surrounding public streets or roads. Existing vehicular approaches from Dillon Road and Vista Del Norte will be utilized for the project. Existing on-site vehicular parking is sufficient.
- 6. The City of Coachella has determined that the proposed project is categorically exempt from environmental review pursuant to the guidelines of the California Environmental Quality Act as an “Existing Facilities” project (CEQA Guidelines, Section 15301a) as the proposed on sale service of liquor will occur in conjunction with an existing business in an existing commercial building.

Section 4. Planning Commission Approval

Based on the forgoing recitals and findings above, and the written and oral comments, facts and evidence presented, the City of Coachella Planning Commission approve Conditional Use Permit (CUP No. 353) for the ABC License Type 41 – On-Sale Beer & Wine – Eating Place for the Registration Office and Clubhouse with the Coachella Lakes RV Resort and subject to the Conditions and Approval as set forth in “Exhibit A”:

PASSED APPROVED and ADOPTED this 6th day of July 2022 by the following vote:

AYES:

NOES:

ABSENT:

ABSTAIN:

Stephanie Virgen
Planning Commission Chairperson

ATTEST:

Gabriel Perez
Planning Commission Secretary

APPROVED AS TO FORM:

Carlos Campos
City Attorney

STATE OF CALIFORNIA)
COUNTY OF RIVERSIDE) ss.
CITY OF COACHELLA)

I HEREBY CERTIFY that the foregoing Resolution No. PC2022-19 was duly adopted by the Planning Commission of the City of Coachella at a regular meeting thereof, held on this 6th day of July 2022 by the following vote of the Planning Commission:

AYES:

NOES:

ABSENT:

ABSTAIN:

Gabriel Perez
Planning Commission Secretary

Resolution No. PC 2022-19

EXHIBIT A

CONDITIONS OF APPROVAL FOR CONDITIONAL USE PERMIT NO. 353:

1. Conditional Use Permit No. 353 is an approval for the On-Sale Beer & Wine – Eating Place Type 41 Liquor License existing and new commercial facility with in the Vineyards RV Resort. This approval is based on the floor plan submitted for the proposed project. Violation of any of the conditions of approval shall be cause for revocation of the Conditional Use Permit.
2. Alcohol sales must be commenced within 12 months of the effective date of this Conditional Use Permit unless a request for an extension of time is reviewed by the Planning Commission. A request for time extension shall be filed in a timely manner with applicable fees.
3. ~~The applicant shall agree to the formation of a Lighting and Landscaping Maintenance District (LLMD) to maintain the median island landscaping, irrigation and street lighting along Dillon Road consistent with Mitigation Measure 14 for Tentative Tract Map 30117. The owner shall prepare the improvement plans, Engineer's Report, Estimated Cost, and submit to the City Engineer as required for the formation of the LLMD. The funds to be deposited shall be a minimum of \$1,000. Costs over \$1,000 shall be billed by the City to the owner for payment. The Applicant shall complete the formation LLMD of prior to issuance of Certificate of Occupancy of the Registration Office.~~
4. The Applicant/Property Owner shall abide by the Conditions of Approval 2d. under the Conditional Use Permit 308 and Architecture 18-03, Resolution PC 2019-07, stating that a Covenants, Conditions and Restrictions (CC&Rs) to establish laws and a new management association for maintenance of all common area improvements and perimeter walls. The CC&R's shall include provisions for owner participation in the maintenance of the street center medial along Dillon Road. The Applicant shall create and record the CC&Rs prior to issuance of Certificate of Occupancy of any structures within the project or at the projected completion date of October 1, 2022.
5. The Applicant/Property Owner shall enter into Landscape Maintenance Agreement with the City Coachella identifying maintenance responsibilities for project perimeter landscaping, common areas and parkways in a first class condition.
6. The Applicant/Property Owner shall of on-site missing, dead or decaying landscaping along the ~~center median~~ and perimeter the project site along Dillon Road the satisfaction of the Code Enforcement Manager and Development Services Director. The applicant shall arrange a field inspection with the Code Enforcement Manager and Development Services Director to affirm that this conditional approval was addressed. The Development Services Director will confirm that this condition has been satisfied at the projected completion date of October 1, 2022.
7. The site landscaping shall be maintained in good condition at all times, and the owner or operator of the facility shall be responsible for replacing any damaged, dead or decayed landscaping as soon as practicable, and in accordance with the approved landscape plan.

8. The applicant will agree to defend and indemnify the City of Coachella against all claims, actions, damages, and losses, including attorney fees and costs, in the event that anyone files legal challenges to the approval of this project on the basis of the California Environmental Quality Act (CEQA). Prior to the issuance of building permits, the applicant shall execute a standard indemnification agreement subject to review by the City Attorney.
9. The use shall meet the standards within the limits established by the Coachella Municipal Code as related to emissions of noise, odor, dust, vibration, wastes, fumes, or any public nuisances arising or occurring incidental to the establishment or operation.
10. The operating hours for the Registration Office and the Clubhouse is from 8:00 a.m. to 10:00 p.m. daily.
11. The applicant shall comply with all requirements imposed by the State Department of Alcoholic Beverage Control and a review of this conditional use permit will be required if the business results in an increase in floor area.

WALL SYMBOL LEGEND

- 2 x 6 stud wall construction (2 x 6 at 16" o.c.) with 5/8" gyp. board ea side. Exterior walls to have exterior plaster finish. All framed walls to have R-21 Fiber Glass Batt Insulation. Insulation may be omitted at interior walls with permission of owner.
-
-
-
-

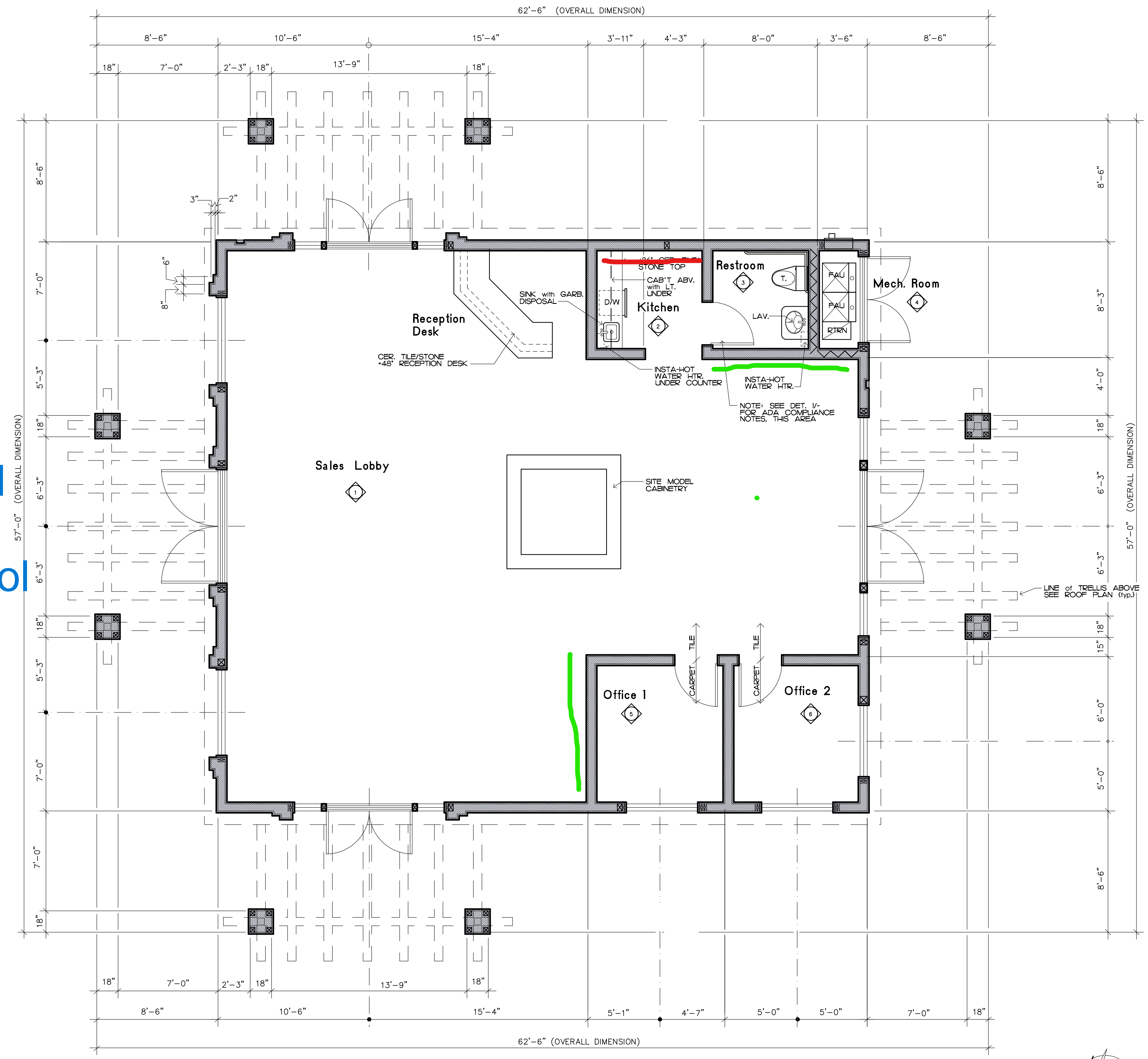
BUILDING INSULATION PROGRAM

WALLS:
All Stud and Knee Walls shall be insulated with: R-23 Encapsulated Batt Insulation.

CEILING:
All ceilings shall be insulated with R-38 Fiberglass Batt Insulation. The insulation shall be installed in Direct Contact with the Underside of the Roof Sheathing to avoid Condensation. Per. CBC, 1203.3 (Unvented Attic and Unvented Enclosed Rafter Assemblies)

ROOF FRAMING:
All vertical attic surfaces and gable end shall have a radiant barrier installed.
All foam plastic insulation shall have a flame spread index of not more than 75 and a smoke developed index of not more than 450.

The pro shop drawing shows green lines where coolers will display alcohol for sale. the red lines are where alcohol will be stored.



SYMBOLS LEGEND

- Wood Posts - See Structural Drawings for Sizes and Locations (Typical)
- Steel Tube Columns - See Structural Drawings for Sizes and Locations (Typical)
- Steel Wide Flanges - See Structural Drawings for Sizes and Locations (Typical)

NOTE: ALL APPLIANCES MUST BE ENERGY STAR LABELED FINISH MATERIAL SHALL COMPLY WITH CGBC 4504.2 YOUR CONTRACT LIMITS for ARCHITECTURAL COATING (PAINTS) SHALL COMPLY WITH CGBC TABLE 4504.3

NOTE: HOT-MOPPED SHOWER PAN SHALL BE INSPECTED UPON COMPLETION OF HOT-MOPPING and SHALL BE FILLED WITH WATER FOR INSPECTION

NOTE: WALL and CEILING FINISHES SHALL have a FLAME SPREAD INDEX not GREATER than 200 and SMOKE-DEVELOPED INDEX not GREATER than 450.

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and
Residences

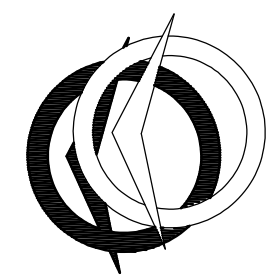
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Coachella, California
Coachella Lakes
RV Resort

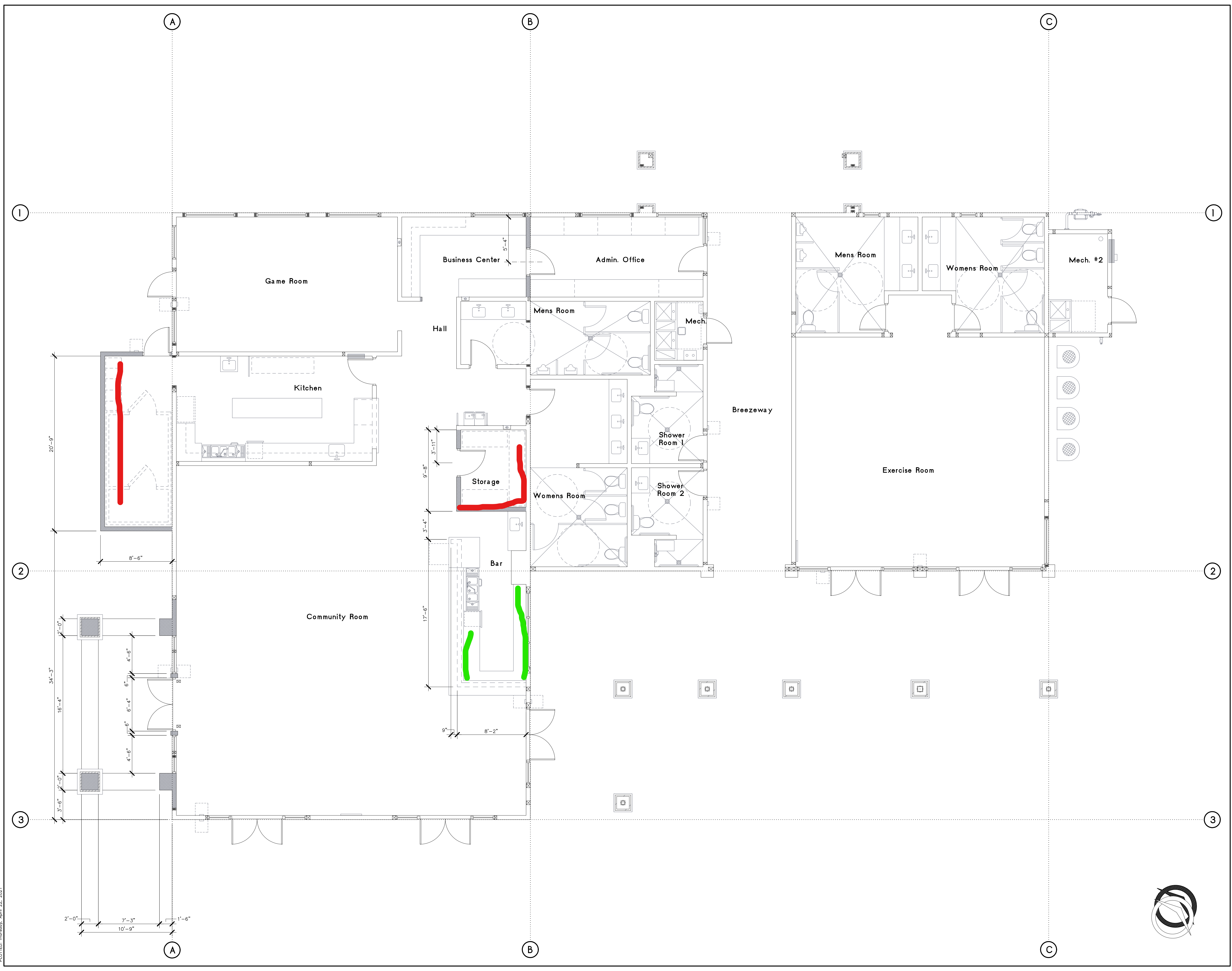
A.P.N. 697-320-070
Lot 1
Tract Map No. 30117-1
Per MB 331/071

Coachella Lakes RV Resort
Check In Building

NOTATION FLOOR PLAN

DRAWN	RA
1st Submittal
Thursday, July 29, 2021	
SCALE	1/4"=1'-0"
JOB NO.	2102
SHEET NO.	
A2.1	





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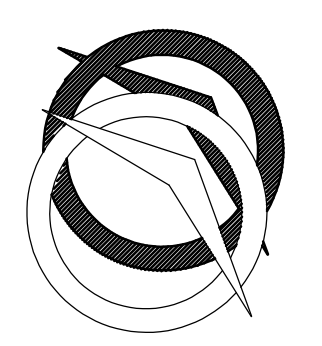
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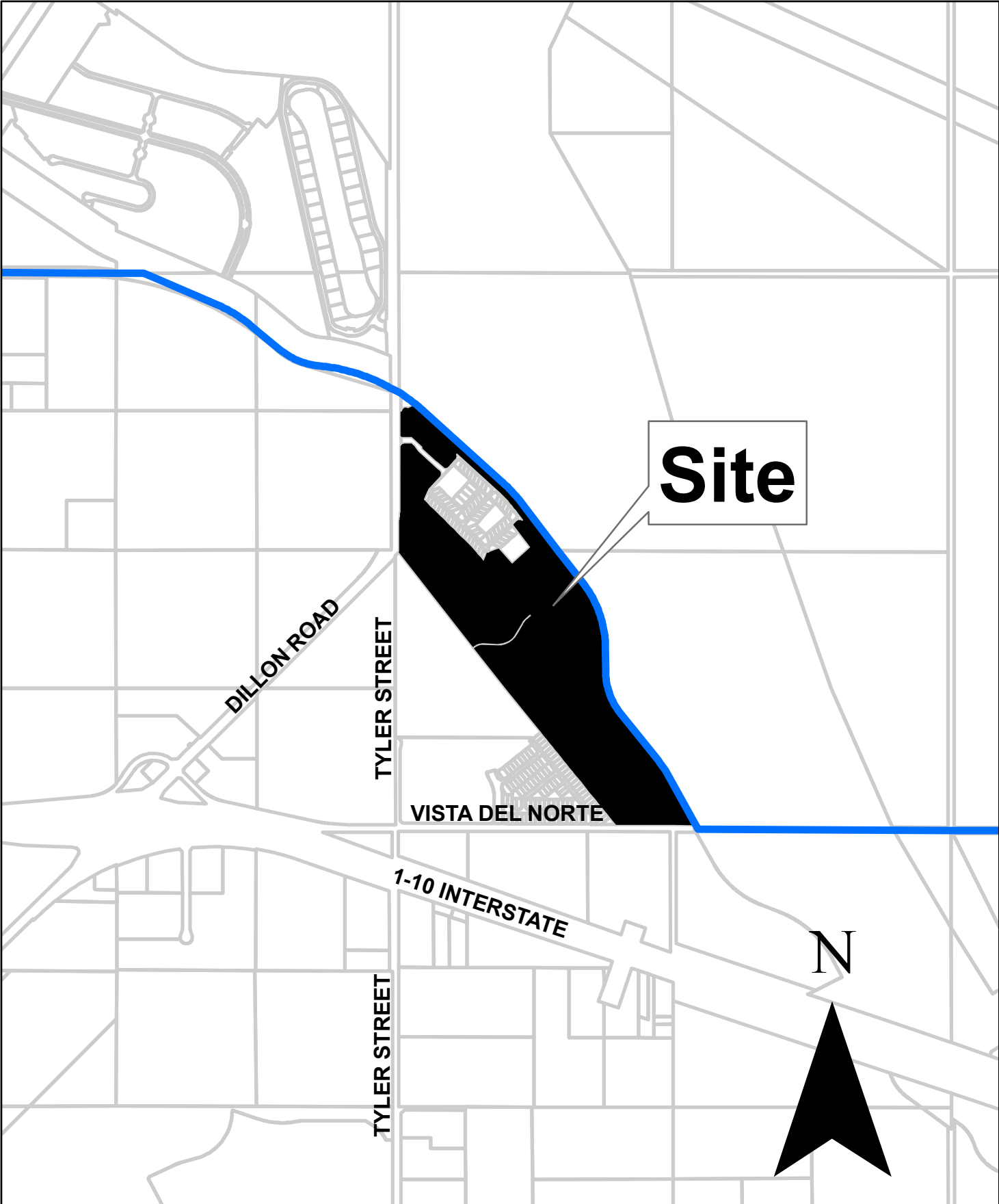
A Motor Coach Resort for:
 Contemporary Resorts
 &
 Residences
 44-800 Dillon Road
 Coachella, California
 Coachella Lakes
 RV Resort
 A.P.N. 697-320-070
 Lot 1
 Tract Map No. 30117-1
 Per MB 331/071

The Vineyards

Club House

DRAWN	RA
1st Submittal
Thursday, April 22, 2021	
SCALE	1/4" = 1'-0"
JOB NO.	1914
SHEET NO.	
A2.2	





Site

DILLON ROAD

TYLER STREET

VISTA DEL NORTE

1-10 INTERSTATE

TYLER STREET

N



Existing Conditions Photos

Coachella Lakes RV Resort

44970 & 44800 Dillon Road & 86401 Sonoma Creek

Along Dillon Road



Registration Office



Clubhouse

